



CPED STAFF REPORT

Prepared for the Heritage Preservation Commission
 HPC Agenda Item #4
 May 17, 2016
 BZH-29104

HERITAGE PRESERVATION APPLICATION SUMMARY

Property Location: 201 2nd Avenue Southeast
Project Name: St. Anthony Ramp Exterior Renovation
Prepared By: Janelle Widmeier, Senior City Planner, (612) 673-3156
Applicant: St. Anthony Ramp LLC
Project Contact: Steven Faber, St. Anthony Ramp LLC
Ward: 3
Neighborhood: Marcy Holmes
Request: To allow exterior alterations to a parking ramp, including a canopy addition and signage.

Required Applications:

Certificate of Appropriateness	To allow exterior alterations to a parking ramp, including a canopy addition and signage.
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HISTORIC PROPERTY INFORMATION

Current Name	St. Anthony Main Ramp
Historic Name	Not applicable
Historic Address	Not applicable
Original Construction Date	1980
Original Architect	Unknown
Original Builder	Tri-Stat
Original Engineer	Unknown
Historic Use	Not applicable
Current Use	Parking ramp
Proposed Use	Parking ramp

Date Application Deemed Complete	April 25, 2016	Date Extension Letter Sent	Not applicable
End of 60-Day Decision Period	June 24, 2016	End of 120-Day Decision Period	Not applicable

CLASSIFICATION

Local Historic District	St. Anthony Falls Historic District
Period of Significance	1848-1941
Criteria of Significance	<i>Criteria 1:</i> The property is associated with significant events or with periods that exemplify broad patterns of cultural, political, economic or social history. <i>Criteria 4:</i> The property embodies the distinctive characteristics of an architectural or engineering type or style, or method of construction.
Date of Local Designation	1971
Date of National Register Listing	1971
Applicable Design Guidelines	<i>St. Anthony Falls Historic District Design Guidelines (2012)</i>

SUMMARY

BACKGROUND. The subject property is located in the St. Anthony Falls Historic District. The St. Anthony Falls National Register Historic District nomination states that the “Falls of St. Anthony were instrumental in the development of Minnesota’s largest city in all its stages of growth” and the “area’s configuration primarily reflects the historic patterns of waterpower development.” In addition to its original natural beauty, the falls furnished direct power to the lumber and flour industries and electrical power for industrial and residential use. Centered around this influential landmark, the St. Anthony Falls Historic District reveals the origins and early history of Minneapolis.

The existing parking ramp was originally constructed as a municipal parking facility and was owned by the City of Minneapolis. It includes a pedestrian tunnel that extends under 2nd Street Southeast to a secondary pedestrian entrance. The parking ramp was constructed outside the period of significance of the historic district and is not a contributing structure.

APPLICANT’S PROPOSAL. The applicant is proposing various alterations at the property of 201 2nd Avenue Southeast to enhance the exterior of the existing parking ramp. The exterior alterations include constructing a canopy over the main pedestrian entrance at the corner of 2nd Street and 2nd Avenue. The canopy would be constructed with polycarbonate and an aluminum framing system. Bands of the polycarbonate would also extend up the sides of the building above the canopy. The existing entrance is non-descript and tucked under the building with limited visibility. The proposed entrance alterations are intended to emphasize the main pedestrian entrance location. The railings of each parking level would be painted blue. Landscape improvements are proposed on each street facing side of the ramp. Existing exterior signage is proposed to be updated. The existing, non-illuminated wall signs above the vehicle entrances and exits are to be replaced with signage of the same size. Two backlit freestanding signs sit on concrete half-walls adjacent to these access points. The walls would be removed and the existing freestanding signs would be placed on the ground. A certificate of appropriateness is required for any new additions to a building in a historic district. Although the other exterior changes are minor and could be reviewed administratively on their own, they are included in this review.

PUBLIC COMMENTS. As of the writing of this staff report, comments have not been received from the neighborhood group. Any correspondence received prior to the public meeting will be forwarded on to the Heritage Preservation Commission for consideration.

ANALYSIS

CERTIFICATE OF APPROPRIATENESS

The Department of Community Planning and Economic Development has analyzed the application to allow exterior alterations to a parking ramp, including a canopy addition and signage based on the following findings:

1. *The alteration is compatible with the designation of the landmark or historic district, including the period and criteria of significance.*

The St. Anthony Falls National Register Historic District nomination states that the “Falls of St. Anthony were instrumental in the development of Minnesota’s largest city in all its stages of growth” and the “area’s configuration primarily reflects the historic patterns of waterpower development.” In addition to its original natural beauty, the falls furnished direct power to the lumber and flour industries and electrical power for industrial and residential use. Centered around this influential landmark, the St. Anthony Falls Historic District reveals the origins and early history of Minneapolis.

The proposed alternations include a new canopy addition, replacing wall signage, removing concrete walls and relocating two freestanding signs, and landscaping updates including seating. The existing structure is noncontributing and is not compatible with the designation of the district. As discussed in finding #3, the proposed canopy would be a contemporary addition, the intent of which is to emphasize the location of the main pedestrian entrance as well as recall the river and falls. The landscaping would be traditional, which is appropriate in this location. The concrete, half-walls, one on each side of the vehicle access driveway on 2nd Avenue, were constructed outside the period of significance. The signage would not increase in size. Because backlit signs are not appropriate in historic districts, staff is recommending that the illumination of the freestanding signs be changed to internal or external illumination. With the adoption of the staff recommendation, the proposed alterations would be compatible with the designation of the subject property within the St. Anthony Falls Historic District.

2. *The alteration will ensure the continued integrity of the landmark or historic district.*

The existing structure was built in 1980, outside of the period of significance for the district. The structure does not contribute to the significance of the district. In the immediate area, most of the historic fabric has been lost. The canopy addition and materials would be a contemporary interpretation of the falls. Creating a faux historic entrance would not be appropriate. The landscaping would be traditional, appropriate to the remaining setting. The demolition of the concrete, half-walls would have no impact on integrity. The alterations to the signage would be minor and would not detract from integrity.

3. *The alteration is consistent with the applicable design guidelines adopted by the commission.*

The *St. Anthony Falls Historic District Design Guidelines* were adopted in 2012. The guidelines provide specific requirements for both new construction and alterations of contributing properties, as well as general guidance for changes in the historic district. It also identifies smaller character areas. The subject site is located in the Water Power Character Area. Within this character area, there are four relatively distinct concentrations of buildings and related development patterns. The subject site is located in the University Avenue Transition Area. The applicable design guidelines are analyzed below:

CHARACTER AREAS

The Water Power Character Area

University Avenue Transition Area

This subarea transitions from industrial and commercial development along Main Street to a former eclectic mix of single- and two-family dwellings, apartments, factories, laboratories and other industrial uses that faced University Avenue Southeast. The buildings ranged in height from three stories to one and a half stories, which provided a transition from the height of the milling and industrial buildings along Main Street.

This area has experienced significant changes and most of its historic fabric has been lost. Buildings along 6th and 5th Avenues Southeast and University Avenue Southeast are indicative of some of the development types of this subarea.

The University Avenue Transition Area is bounded by 2nd Street South, Central Avenue Northeast, University Avenue Southeast and 6th Avenue Southeast.

Intent

Enhanced landscapes and streetscapes in this character area are encouraged. They should not impede one's ability to understand the historical function and character of the context. Guidance offered in Chapter 6 for landscapes, streetscapes, and open spaces in historic industrial areas should be applied in the West Side and East Side Milling Areas and the Main Street Area. The University Avenue Transition Area was a historic commercial mixed use area; traditional landscapes and streetscapes are more appropriate in this location.

GENERAL GUIDELINES

Streetscape Design

Requirements

6.4 New or replacement street furnishings, such as street lights and street furniture, shall be compatible with the context of the individual character areas.

- a. In historic industrial areas, simple contemporary utilitarian designs are appropriate for street furnishings.
- b. In historic residential and commercial areas, contemporary styles, such as metal with a painted finish, are appropriate for designs for street furnishings.
- c. Designs that create a false sense of history, such as highly ornate historic styles, are not appropriate.

6.6 Streetscape plantings should be compatible with the context of the individual character areas.

- a. In historic industrial areas, street trees should be clustered and have irregular spacing to evoke a sense of the volunteer nature of vegetation of these industrial settings.
- b. Boulevard plantings will be considered in historic industrial areas.
- c. In historic commercial and residential areas, traditional regular spacing and placement of trees is appropriate.

- d. Boulevard plantings are appropriate in historic commercial and residential areas
- e. Street trees shall not be located directly in front of entrances.

Staff Comment:

The subject site was historically residential and industrial. The surrounding area was historically residential, commercial and industrial. The proposed plantings would be consistent with traditional spacing.

NEW INFILL BUILDING GUIDELINES

Building Placement and Orientation

9.3 Maintain the traditional orientation pattern of buildings facing the street.

- a. Locate the primary entrance to face the street and design it to be clearly identifiable.

Staff Comment:

The proposed entrance alterations would clearly identify the location of the primary pedestrian entrance.

Architectural Character and Detail

9.4 Design a new building to reflect its time while respecting key features of its context.

- a. In those character areas with a high concentration of historic structures, relating to the context is especially important. In other areas where new construction is more predominant, respecting broader traditional development patterns that shaped the area historically is important.

9.5 A contemporary interpretation of traditional designs is appropriate.

- a. The design should be compatible with the relevant character area.
- b. Contemporary interpretations of architectural details are appropriate.
- c. Incorporate contemporary details to create interest while expressing a new, compatible design.
- d. Use designs for window moldings and door surrounds to provide visual interest while helping to convey that a building is new.

9.7 Incorporate traditional facade articulation techniques in a new design.

- a. Use these methods:
 - A tall first floor
 - Vertically proportioned upper story windows
 - Window sills and frames that provide detail
 - Horizontal expression elements, such as canopies, moldings and cornices
 - Vertical expression features, such as columns and pilasters
 - A similar ratio of solid wall to window area

Staff Comment:

A contemporary entrance design is proposed. The existing entrance is non-descript and tucked under the building. The canopy and façade alterations would establish vertical elements. At the ground level, the polycarbonate canopy panels could be interpreted as storefront windows.

Primary Entrances

9.19 Design a building entrance to appear similar in character to those used traditionally.

- a. Clearly define the primary entrance.
- b. Use a contemporary interpretation of a traditional building entry, which is similar in scale and overall character to those seen historically.

Staff Comment:

The proposed entrance alterations would clearly identify the location of the primary pedestrian entrance. Because the ramp does not have a traditional building entry, the canopy would provide a contemporary interpretation.

Materials

9.21 Contemporary materials that are similar in character to traditional ones will be considered.

- d. Contemporary, alternative materials should appear similar in scale, durability and proportion to those used traditionally.

9.22 Use high quality, durable materials.

- a. Materials should be proven to be durable in the local Minneapolis climate.
- b. The material should maintain an intended finish over time, or acquire a patina, which is understood to be a likely outcome.
- c. Materials at the ground level should withstand ongoing contact with the public, sustaining impacts without compromising the appearance.

Staff Comment:

The proposed materials are polycarbonate and aluminum. Polycarbonate is not a traditional material found in the district. Blue is the proposed color. The applicant has indicated that the intent of the material and its color is to recall water and the falls of the Mississippi River in the design of the new entrance. The applicant has also indicated that the proposed polycarbonate is a durable material and can withstand wind, snow, rain and on-going impact with the public.

Canopies/Awnings

9.26 A canopy/awning should be in character with the building.

- a. Mount a canopy/awning to accentuate character defining features.
- b. A canopy/awning should remain a subordinate feature on the building.

Staff Comment:

The canopy would be a subordinate feature on the building.

In the Design Guidelines for On-Premise Signs and Awnings, backlit signs are not appropriate for historic districts. The freestanding signs that would be relocated on the ground after the half-walls are removed are backlit. Because the sign cabinets would be reused, staff is recommending that the

signs be changed to internal or external illumination. The replacement walls signs would be nonilluminated and would not increase in size.

4. *The alteration is consistent with the applicable recommendations contained in The Secretary of the Interior's Standards for the Treatment of Historic Properties.*

The following standards for rehabilitation are most applicable to this proposal:

- New additions, exterior alterations, or related new construction shall not destroy historic materials that characterize the property. The new work shall be differentiated from the old and shall be compatible with the massing, size, scale, and architectural features to protect the historic integrity of the property and its environment.

No contributing structures are located on the subject site. The proposed alterations would not impact any contributing structures. They would be compatible with the massing, size, scale, and architectural features of the property and its environment.

5. *The alteration is consistent with the spirit and intent of the preservation ordinance, the applicable policies of the comprehensive plan, and the applicable preservation policies in small area plans adopted by the city council.*

The following policies of the *Minneapolis Plan for Sustainable Growth* are applicable to the proposal:

Heritage Preservation Policy 8.1: Preserve, maintain, and designate districts, landmarks, and historic resources which serve as reminders of the city's architecture, history, and culture.

- 8.1.2 Require new construction in historic districts to be compatible with the historic fabric.

Urban Design Policy 10.10: Support urban design standards that emphasize a traditional urban form in commercial areas.

- 10.10.1 Enhance the city's commercial districts by encouraging appropriate building forms and designs, historic preservation objectives, site plans that enhance the pedestrian environment, and by maintaining high quality four season public spaces and infrastructure.
- 10.10.3 Enhance pedestrian and transit-oriented commercial districts with street furniture, street plantings, plazas, water features, public art and improved transit and pedestrian and bicycle amenities.

The Marcy-Holmes Neighborhood Master Plan was adopted by the City Council in 2014. In the plan, the site falls in the Riverfront Character Area. The goal of the plan for this area is to “Expand and improve riverfront parks, improve connectivity, balance local and regional access and use, create bike- and walk-friendly environments on 2nd Street Southeast, and embrace diversity of building uses and eras.”

The existing parking ramp is not compatible with the character of the historic district. As discussed in finding #3 above, the contemporary canopy addition would be compatible with the historic context and would not impact any contributing structures. The site is located in the East Hennepin Activity Center. The design of the ramp is heavily auto-oriented. The proposed entrance canopy would emphasize the location of the main pedestrian entrance, which is currently non-descript. It would also help to reinforce the street wall. Bike parking would be maintained next to the main entrance as well. These changes and the proposed landscaping and seating would enhance the pedestrian environment. The proposed alterations conform to the applicable regulations of the

preservation ordinance, are consistent with the above policies of the comprehensive plan, and the applicable preservation policies in the adopted small area plan.

RECOMMENDATIONS

The Department of Community Planning and Economic Development recommends that the Heritage Preservation Commission adopt staff findings for the application by St. Anthony Ramp LLC for the property located at 201 2nd Avenue Southeast in the St. Anthony Falls Historic District:

A. Certificate of Appropriateness.

Recommended motion: **Approve** the certificate of appropriateness to allow exterior alterations to a parking ramp, including a canopy addition and signage, subject to the following conditions:

1. The freestanding signs shall not be backlit.
2. By ordinance, approvals are valid for a period of two years from the date of the decision unless required permits are obtained and the action approved is substantially begun and proceeds in a continuous basis toward completion. Upon written request and for good cause, the planning director may grant up to a one year extension if the request is made in writing no later than May 17, 2018.
3. By ordinance, all approvals granted in this certificate of appropriateness shall remain in effect as long as all of the conditions and guarantees of such approvals are observed. Failure to comply with such conditions and guarantees shall constitute a violation of this Certificate of Appropriateness and may result in termination of the approval.

ATTACHMENTS

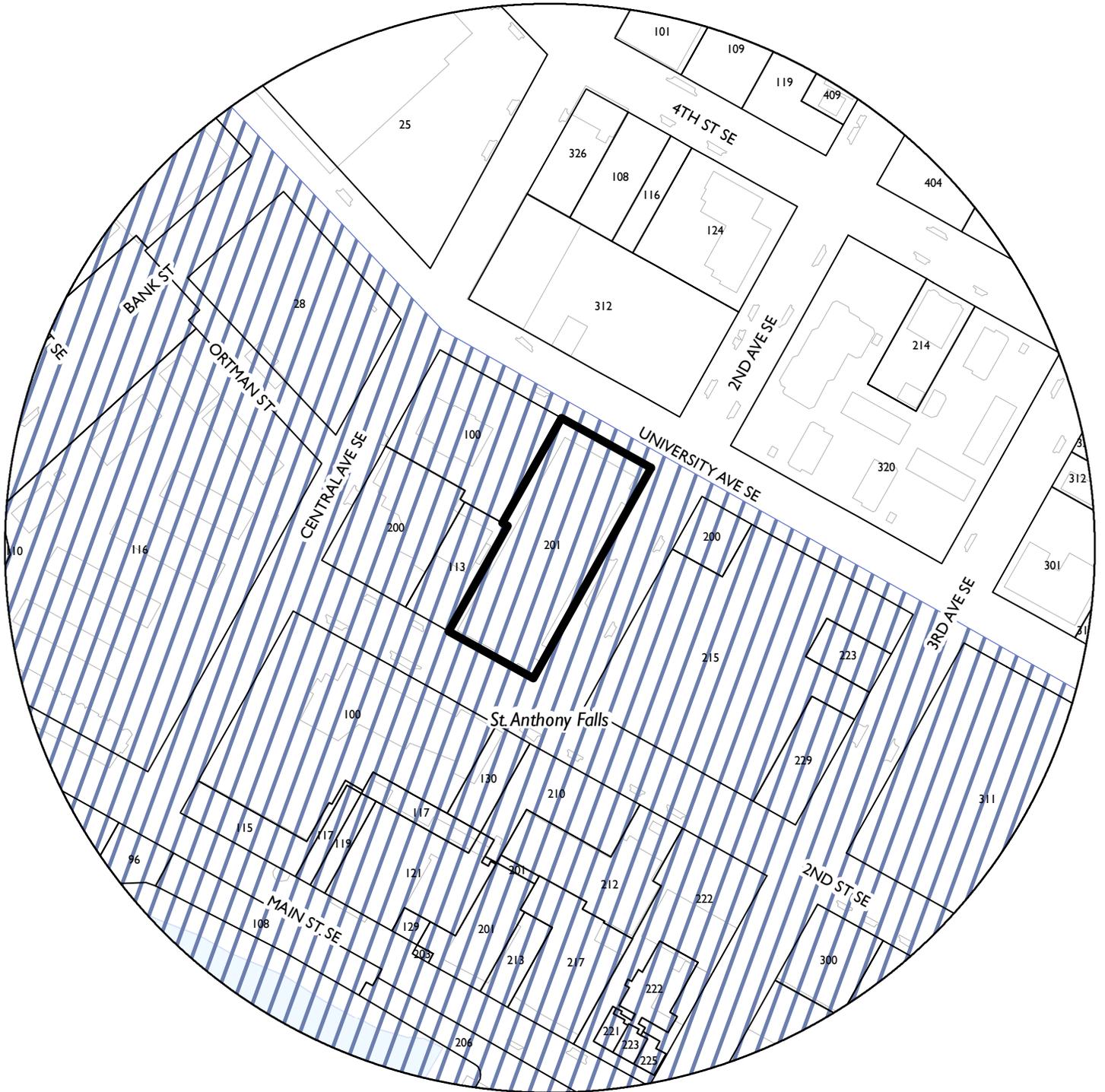
1. BZH Map
2. Written description and findings submitted by applicant
3. Photos
4. Site survey
5. Site plan
6. Plans
7. Building elevations
8. Renderings
9. Signage
10. Photo of proposed materials
11. Material specifications

St. Anthony Ramp LLC

3rd

NAME OF APPLICANT

WARD



PROPERTY ADDRESS

201 2nd Avenue Southeast

FILE NUMBER

BZH-29104

St. Anthony Ramp - 2nd Ave SE, Minneapolis, MN 55414

Application for Certificate of Appropriateness for New Construction + Enhancements

Statement of Proposed Use, Project Description, and Findings

April 11, 2016

The proposed use for the site is remaining unchanged – a 9-story parking ramp constructed in 1980. The project intent is to fulfill the ramp purchase agreement requirements from the City of Minneapolis that were recently amended and require exterior enhancements to the ramp.

The project intent is to create cohesive, high-quality, focused, and durable enhancements that will extend the life of the ramp; and be an attractive addition to the City's built environment. This project will substantially enhance the pedestrian realm at the base of the ramp; as well as offering visual continuity across the façade.

Design Intent

The design intent calls for no changes or replacements to important architectural details of the building. Improved items include structural enhancements to the garage floor slabs where necessary, extensive painting (a majority to the interior of the ramp: ramp drive surfaces, stair towers, elevator lobbies, floor numbers, tunnel – and minimal new color highlights to the exterior railings), new entry canopy, removal of chain link along ground floor, trimming and lighting of existing trees, and additional plantings.

Demolition of knee walls at garage vehicle entrance is being done for pedestrian safety reasons, as the walls currently block driver views of the sidewalk, creating the potential for pedestrian/vehicular accidents. With removal of these walls, the pedestrian realm will flow better across the driveway apron, and driver views to the sidewalk will be much clearer, lessening any accident potential. There is no current economic value or usefulness of the existing walls, they are, in fact a detriment to current function of ramp. The "P" parking signs that currently sit atop the walls will be removed from wall and placed at grade.

New signage replicating the existing signage in size will be replaced above the garage entry/exits. The new signage package will offer a continuity in style that currently does not exist. New signage and railings will be Pantone Blue 285C. (see submittal)

A new canopy and planting area will be added to the southeast corner of 2nd Avenue SE and 2nd Street SE. This canopy is designed to add visual interest to the corner, tying into the overall blue color theme of the exterior highlights. The intention was to recall water, as the Mississippi is mere steps away, with the blue color. The canopy is constructed of polycarbonate within an aluminum framing system. The system is designed for high strength, outdoor hardiness, snow and wind loads, and to offer shelter from rain and snow.

New perennial plantings will replace grass on the 2nd Street SE, 2nd Avenue SE, and University Avenue SE faces – substantially reducing mowing and emissions and adding a vibrant plant mix to the area. Limestone benches will be placed adjacent to the new canopy and adjacent to the bus stop along University. Existing bike racks will be relocated to a newly paved area adjacent to the stair tower at 2nd and 2nd SE.

CERTIFICATE OF APPROPRIATENESS FINDINGS

- (1) *The alteration is compatible with the designation of the landmark or historic district, including the period and criteria of significance.*
- (2) *The alteration will ensure the continued integrity of the landmark or historic district.*

The exterior canopy, planting bed, and bench being proposed is compatible with the structure of the St. Anthony Ramp to which it will be attached. The Ramp itself is an anomaly within the historic district, built in the early to mid-1980s long after the local and national designations were put in place in 1971.

It is our intent to create an enhancement that works in concert with the ramp, thus the clean and minimal canopy structure – designed to protect users from the weather upon entering/exiting the elevator or stair tower.

Attempting to recreate a historic enhancement, we feel, would stand out as artifice and be less compatible with the immediate context than the current proposal.

- (3) *The alteration is consistent with the applicable design guidelines adopted by the commission.*

The alteration is consistent with the general design characteristics outlined by the design guidelines.

- **Authenticity:** The proposed enhancements to the St. Anthony Ramp are authentic in that they reflect the time/style in which the ramp was built and are compatible with the ramp they are enhancing.
- **Simplicity:** The Ramp is simple in its design. The same can be said for the enhancement – a simple canopy connecting the building to the site.
- **Durability:** The materials chosen – aluminum, polycarbonate, stone – are highly durable and will have a long lifespan.
- **Functionality:** This enhancement clearly expresses its functionality of shielding users from the elements as they emerge or enter the ramp structure.
- **Integrity of Design:** Simple, industrial lines – horizontals and verticals of machined aluminum speak to an industrial past. Limestone benches recall the materiality of the river gorge.
- **Innovation:** This design speaks to the innovative spirit of the district – creating an enhancement solution that is simple, cost-effective, and of its time.
- **Connectivity:** The blue panels of the proposed canopy were chosen to recall the flow of water in the river beyond. The limestone benches speak to the river gorge. Both elements act to tie the design to its context.

The landscape design also addresses the following policy requirements:

6.2 Design new landscapes to be in harmony with the overall historic character of the district.

b. A new landscape design should not convey a false sense of history. Designs that reflect their own time, while helping to convey the history of a site, are appropriate.

(4) *The alteration is consistent with the applicable recommendations contained in The Secretary of the Interior's Standards for the Treatment of Historic Properties.*

The Secretary's Standards focus on the following: identifying, retaining, preserving, stabilizing protecting, maintaining, and repairing buildings/features that reinforce/define the historic character of the area.

The existing ramp is not important in defining the overall historic character of the district, so enhancements will not diminish the overall character any further. They will improve the immediate surroundings. The exterior canopy, planting bed, and bench being proposed will ensure the continued integrity of the St. Anthony Ramp with the surrounding block.

(5) *The alteration is consistent with the spirit and intent of the preservation ordinance, the applicable policies of the comprehensive plan, and the applicable preservation policies in small area plans adopted by the city council.*

The enhancements being proposed are designed to work with the spirit and intent of the ordinance, in that they are minimalist in nature, designed to enhance the pedestrian realm and experience – helping to draw users towards the historic riverfront beyond. Material choices help reinforce the notion of river and gorge in a complimentary blend of industrial aluminum and natural stone. Color choices reinforce the current palette and act to enhance the visuals of the ramp itself.

The St. Anthony Design Guidelines outline that this area, the University Avenue Transition Area, has *“experienced significant changes and most of it historic fabric has been lost.”*

“Enhanced landscapes and streetscapes in this character area are encouraged. They should not impede one's ability to understand the historical function and character of the context. The University Avenue Transition Area was a historic commercial mixed use area; traditional landscapes and streetscapes are more appropriate in this location.” p. 130

Since most of the historic fabric has been lost in the area where the ramp sits and our interventions will occur, our proposal focuses on enhancing the landscape and streetscape here in a more traditional manner, as encouraged. The design ideas will not impede the understanding of the historic function/character of the area.





Backlit P Sign (2): 4'-0" x 4'-0"

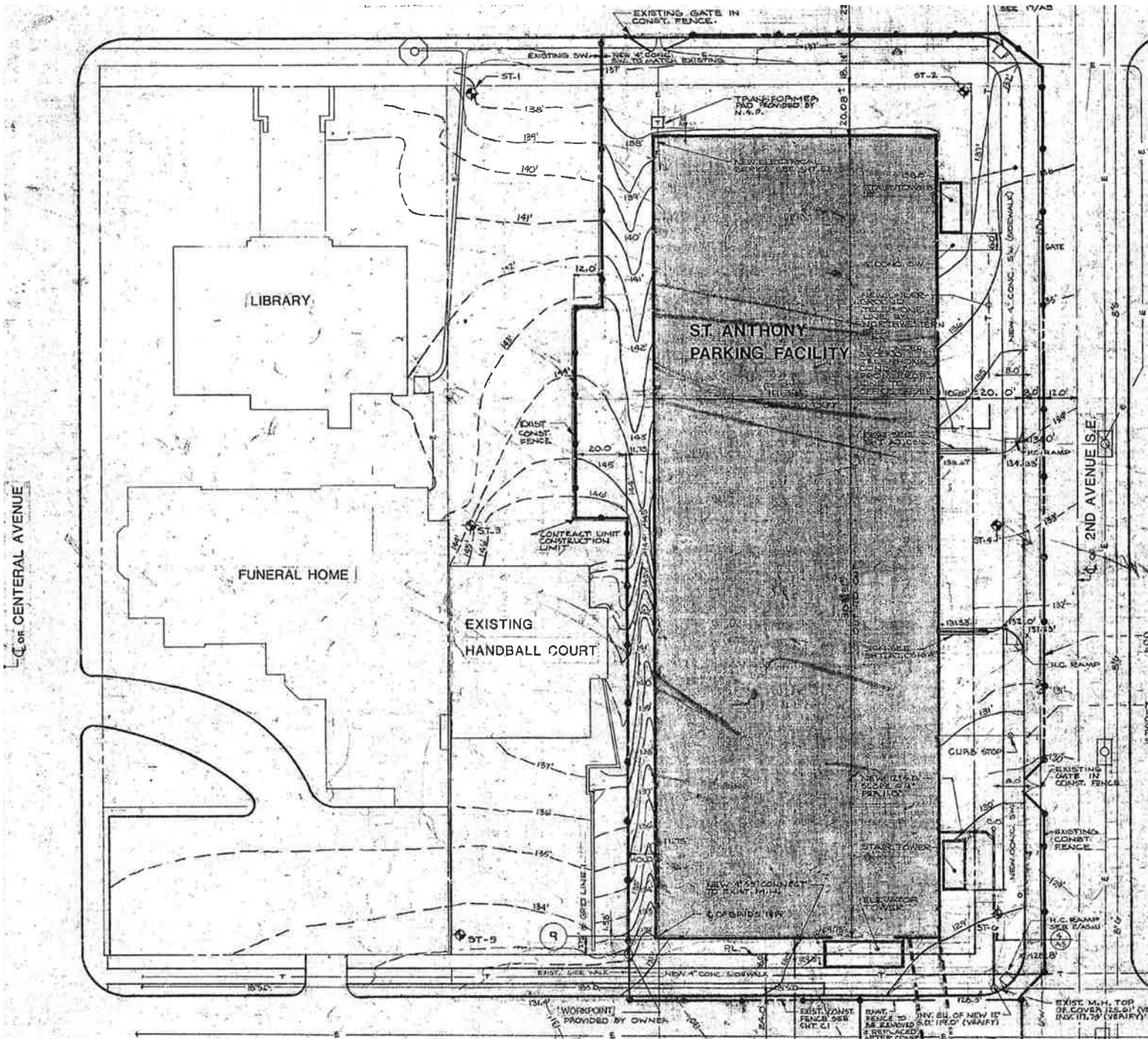


Entrance Signage to Ramp Across In/Out-Going Lanes: 5'-3" x 72'-0"

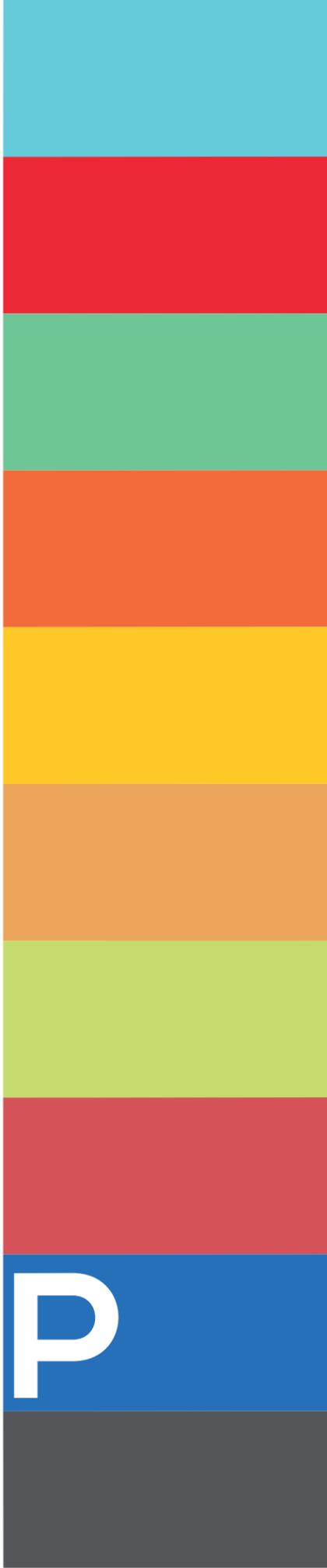


Cloth Ramp Sign: 8'-0" x 2'-0"





ST. ANTHONY RAMP





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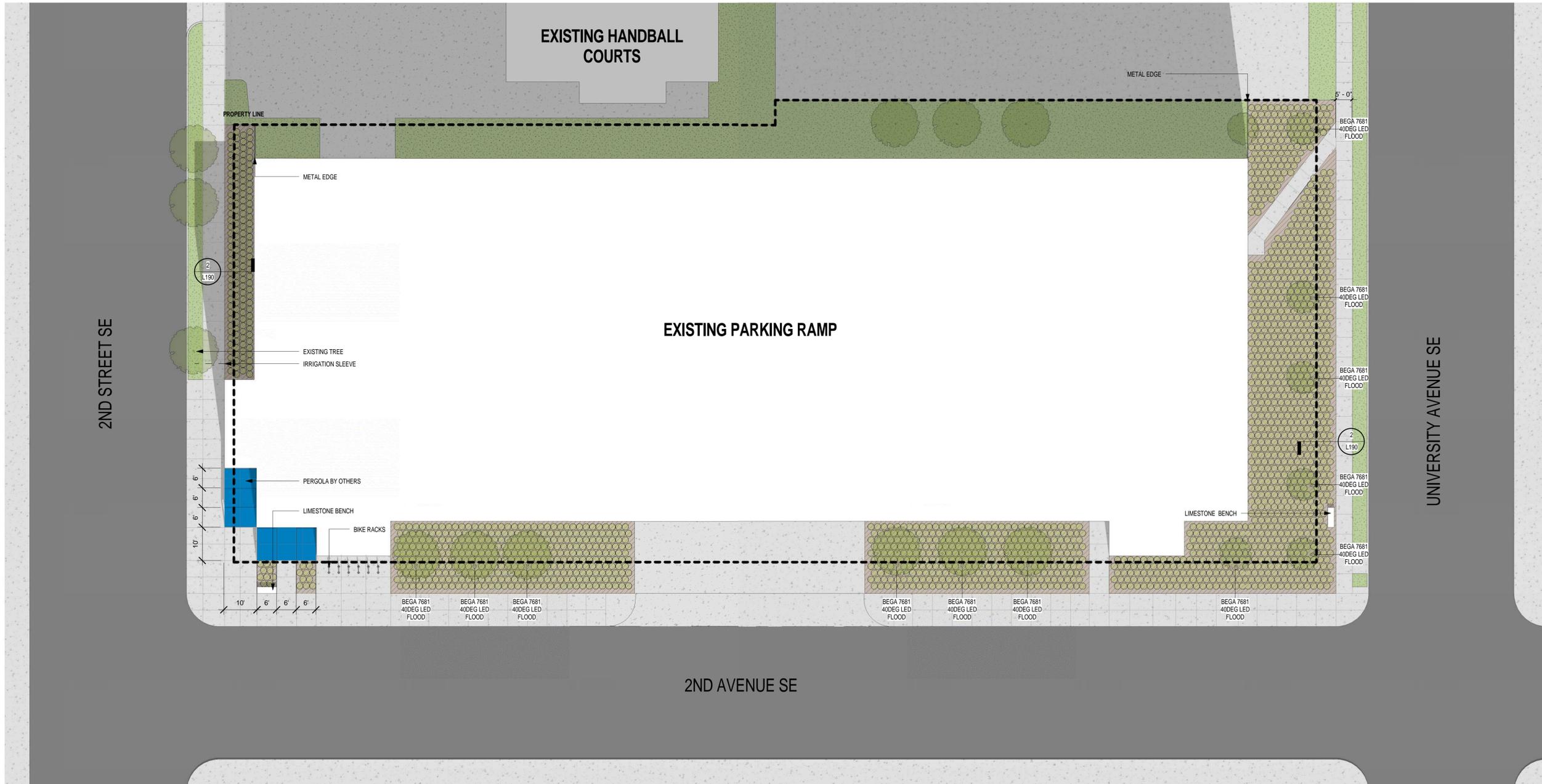
Revisions

No.	Date	Description

**PRELIMINARY
NOT FOR
CONSTRUCTION**

Date: February 19, 2016
PIC / AIC: JC
Drawn By: JH
Checked By: DM
Document Phase:
Comm. No.: 15-0439

Project Title
**St. Anthony Parking
Ramp**



1 SITE - LANDSCAPE PLAN - OPTION B
L100 1/16" = 1'-0"



PLANTING SCHEDULE						
	COUNT	COMMON NAME	BOTANICAL NAME	HEIGHT	WIDTH	COMMENTS
GROUND COVER	1841	VINCA	VINCA MINOR 'DART'S BLUE'	6"	1' - 6"	

LIGHTING FIXTURE SCHEDULE	
	COUNT
BEGA 7681 40DEG LED FLOOD	12

LANDSCAPE LEGEND

	CONCRETE
	MULCH
	TURF
	PROPERTY LINE
	EXISTING TREE

Sheet Title
Landscape Plan

Sheet Number

L100



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Revisions

No.	Date	Description

PRELIMINARY NOT FOR CONSTRUCTION

Date: JANUARY 29, 2016

PIC / AIC: JC

Drawn By: JH

Checked By: DM

Document Phase:

Comm. No.: 08-0039

Project Title

St. Anthony Parking Ramp

Sheet Title

Elevation

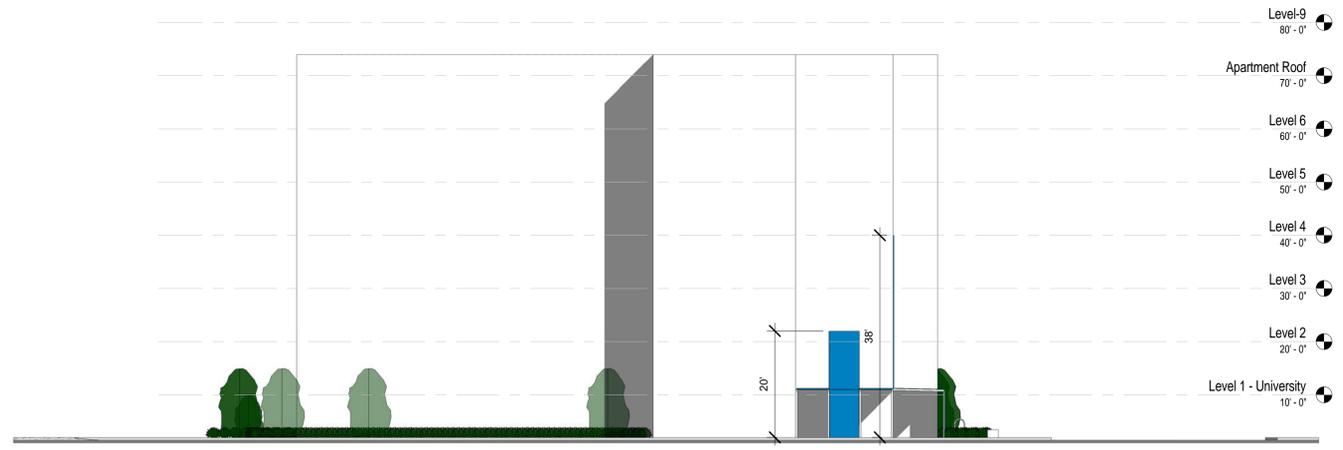
Sheet Number

L200

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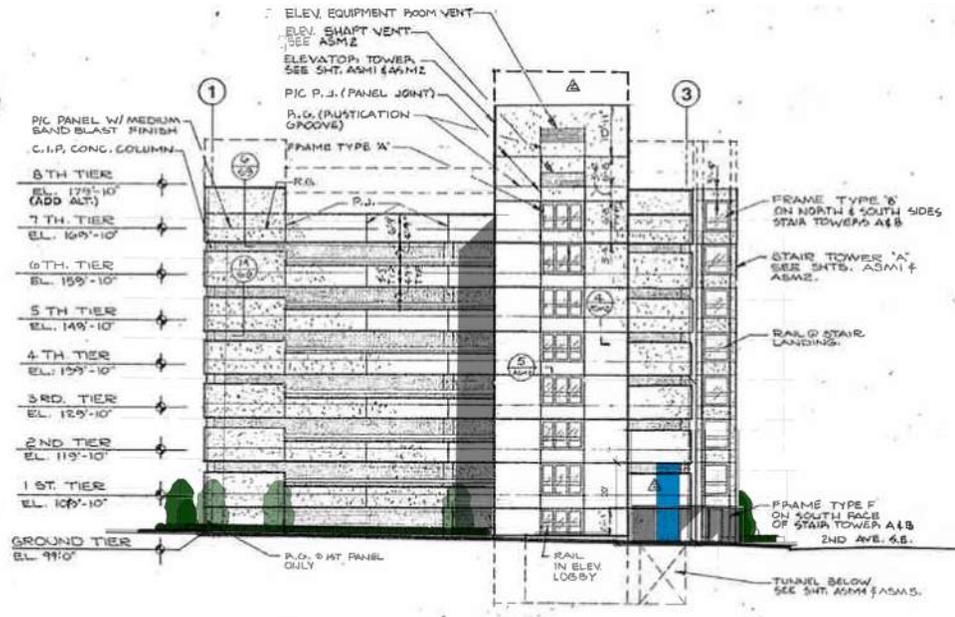
1 ELEVATION - SOUTH
L200 1/16" = 1'-0"



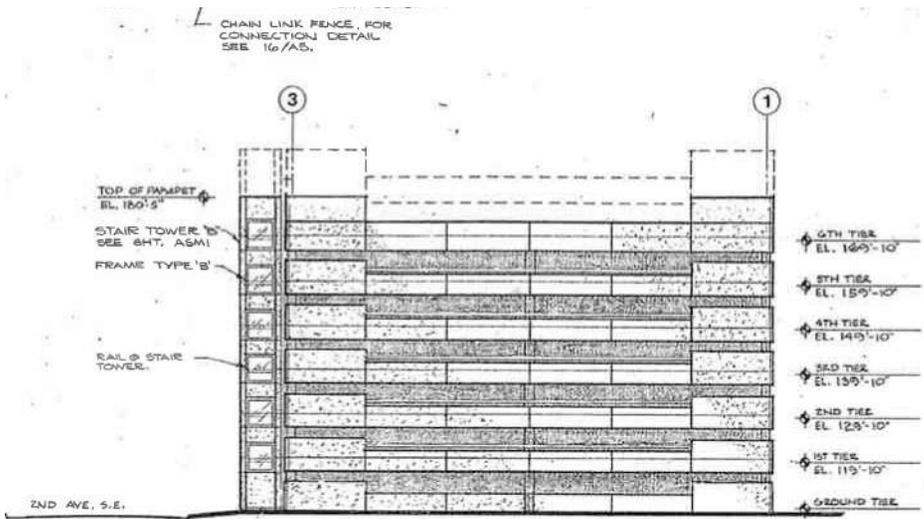
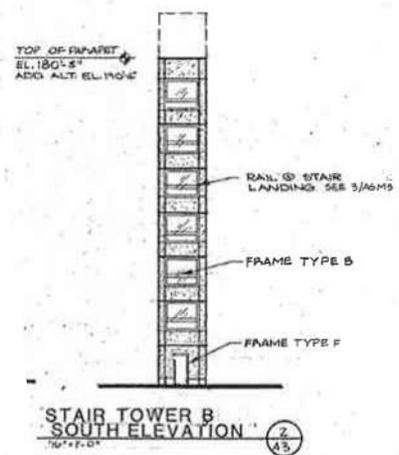
2 ELEVATION - WEST
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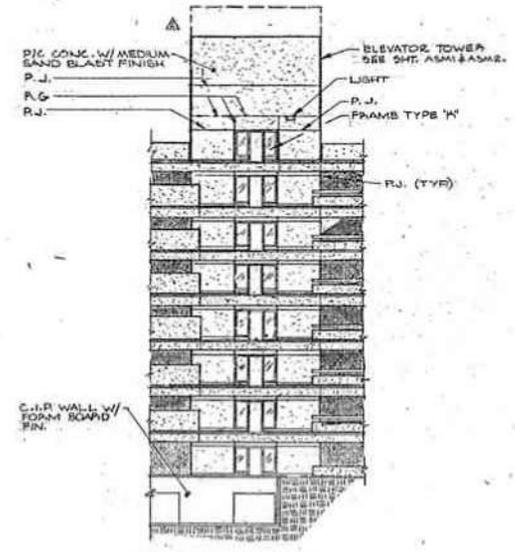
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SOUTH ELEVATION (5) (A5)

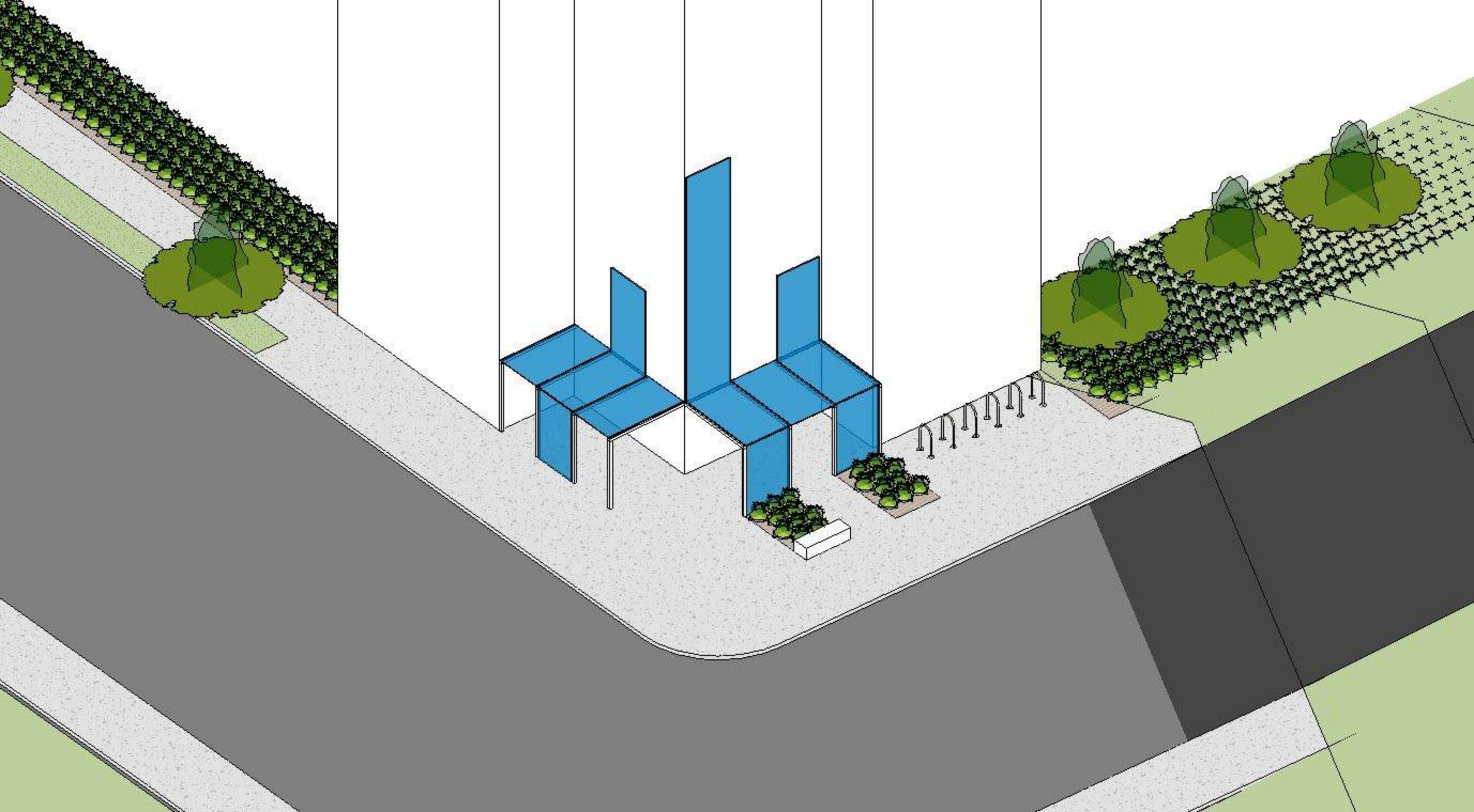


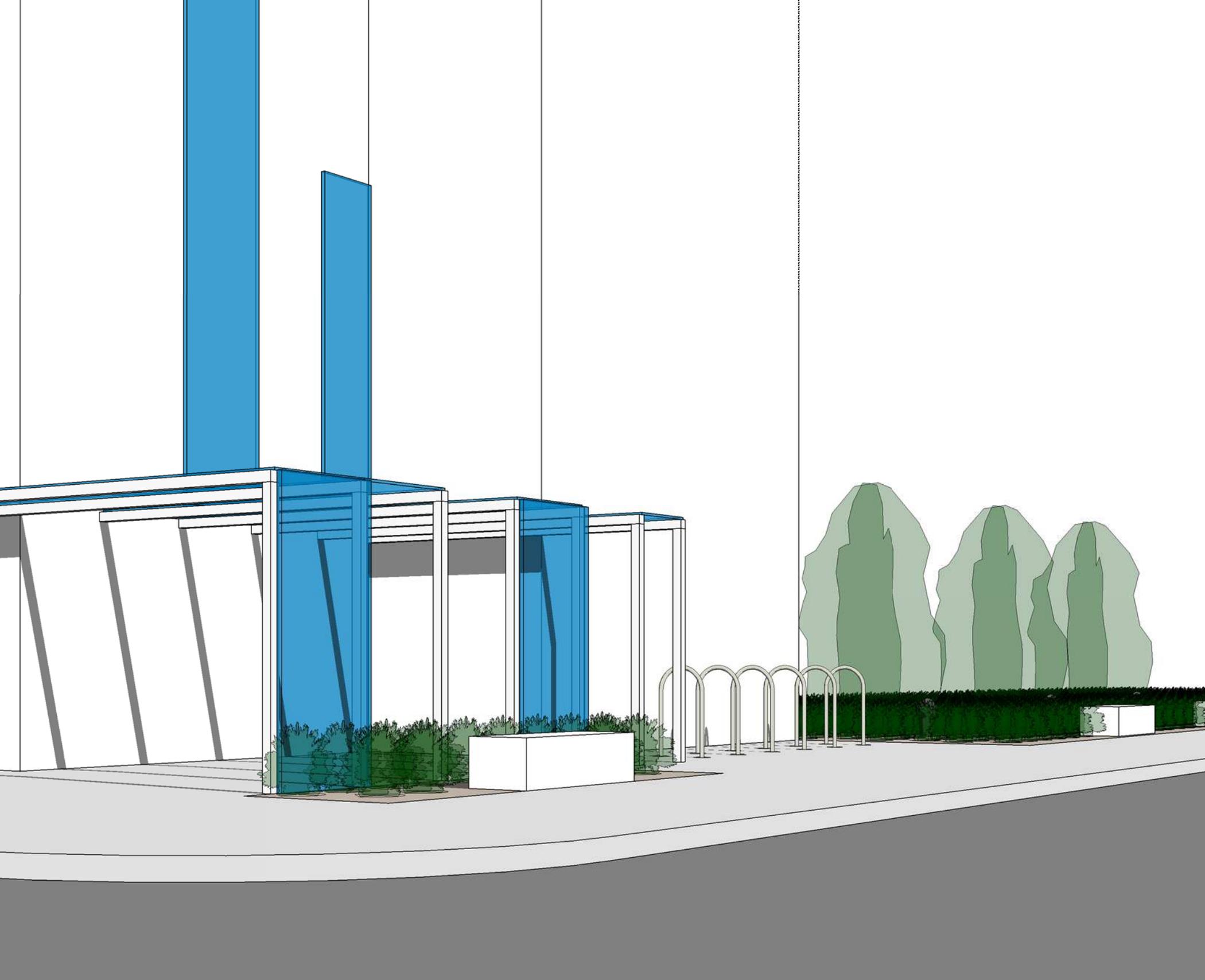
NORTH ELEVATION (6) (A5)



ELEVATOR TOWER NORTH ELEVATION (4) (A5)







All exterior railing painted PANTONE 285 C



Existing Conditions



Remove existing signage

All exterior railing painted PANTONE 285 C

Signage painted PANTONE 285 C size is 5'3" x 72'



Existing Conditions



Remove existing walls but keep the back-lit Parking Signs and mount on the ground at equivalent positions (2) sizes at 4' x 4'



Existing signage 8' x 2'



Pentaglas 10mm
Blue



U-LITE™
Blue 4mm Danpalon

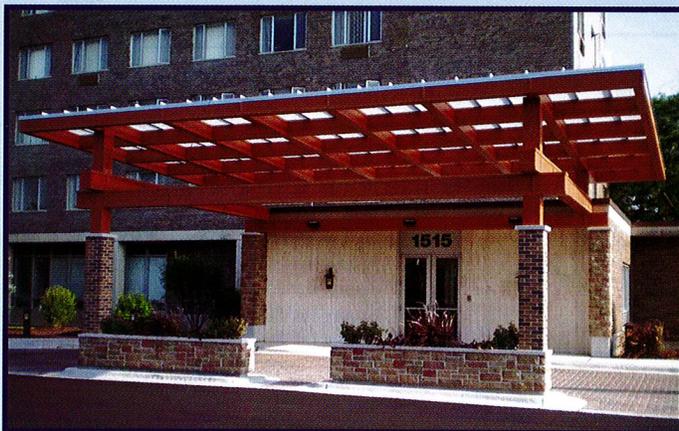
U-LITE™

*Monolithic Polycarbonate Standing Seam Panel
for Low Slope Canopies*



- Low Slope Design
- Modular Panel System
- Long Span
- Exposed Edge Design
- Simple Installation

- Optional Simple On-site Fabrication to Expedite Scheduling
- U.V. Stable Polycarbonate
- High Impact Resistance
- Translucent Glazing Provides Pleasant Daylight Transmission



28662 N. Ballard Drive, Lake Forest, IL 60045
PH: 800-759-6985 FX: 847-816-0425
EMAIL: cpi@cpidaylighting.com
WEB: www.cpidaylighting.com

Minimum Pitch 1/2" : 12"
Maximum panel length 19.5' up slope

2" x 3" x 1/8" ANGLE ● SILL
 ATTACHED W/ #14 FASTENER

.060" ALUM END PLATE
 WELDED TO BATTEN
 4mm U-LITE W/
 EXPOSED EDGE

CPI-1291 2" ALUM BATTEN
 ● 2'-0" O.C. W/ #14 FASTENER
 ● 12" O.C.

.060" ALUM END PLATE
 WELDED TO BATTEN
 4mm U-LITE W/
 EXPOSED EDGE

(A) SECTION ● SILL

(B) SECTION ● PURLIN

(C) SECTION ● HEAD

CONTACT CPI W/LOADING
 FOR MAX OVERHANGE

CONTACT CPI W/LOADING
 FOR PURLIN SPACING

CONTACT CPI W/LOADING
 FOR MAX OVERHANGE

WALL BY OTHERS

REGLET BY OTHERS

COUNTER FLASHING BY OTHERS

.060" ALUM END PLATE WELDED TO BATTEN
 ALUM POP RIVET ATTACH FLASHING
 TO BATTEN ● 24" O.C.

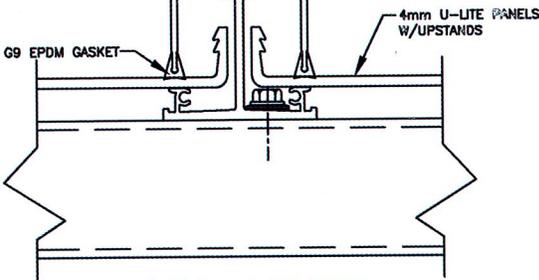
(2) 1" x 1" BAFFLE BETWEEN BATTENS,
 W/PSA SIDE TO PANEL

#14 FASTENER ● 12" O.C.

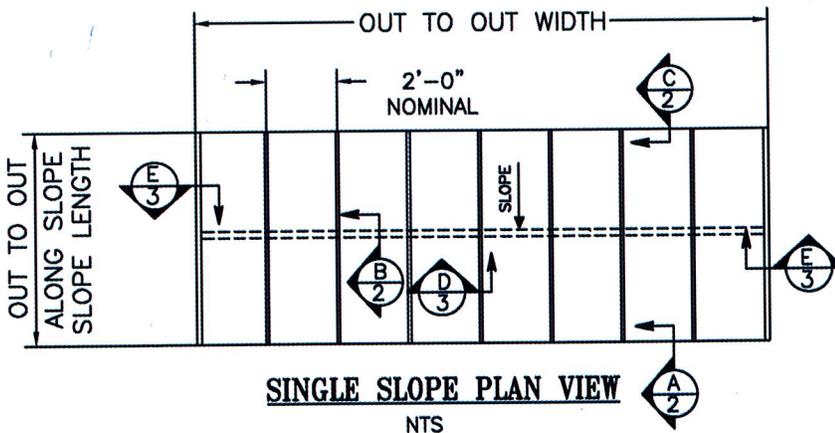
PURLINS BY OTHERS

(G) SECTION HEAD ● WALL

CPI-1291 2" ALUM BATTEN
 ● 2'-0" O.C. W/ #14 FASTENER
 ● 12" O.C.



(D) SECTION ● TYP BATTEN



Guidelines for Spans Between Supports with Combined Loads:

- Up to 10' Spans for 18 PSF live/20 PSF wind loads
- Up to 9' Spans for 25 PSF live/20 PSF wind loads
- Up to 8' Spans for 35 PSF live/25 PSF wind loads
- Up to 7' Spans for 50 PSF live/30 PSF wind loads
- Up to 6' Spans for 80 PSF live/30 PSF wind loads

NOTE: Live loads include drift loads

Please contact CPI for project specific span performance data

U-LITE™
 Monolithic Polycarbonate
 Standing Seam Panel

CPI
DAYLIGHTING
Quality Comes to Light®

28662 N. Ballard Drive
 Lake Forest, IL 60045
 PH 847-816-1060
 FX 847-816-0425
 WWW.CPIDAYLIGHTING.COM

U-Lite™ – 4mm Danpalon Performance Summary

RV 03/15

Test Description	Test Procedure	Results & Comments
FLAMMABILITY Self-Ignition Smoke Density Burn Extent Interior Flame Spread	ASTM 1929-3 ASTM D2843 ASTM D635 ASTM E-84	1120° F 54% CC1 Rating Class A

PANEL PROPERTIES AND DIMENSIONS

Standard Size: Glazing Panel = 23 5/8" (592 mm) wide x up to 19'5" long
 System Module = 23 3/4" wide (c/c with spacer) by up to 19'5" long

Weight of glazing panel: 0.98 LB / SF

Weight of entire panel system: 1.5 – 2 lbs / SF depending on system configuration

Thermal Expansion: Linear thermal expansion = 0.0000375 In/In/°F

U Value: Thermal insulation factor = 0.94 Btu/(hr)(ft²)(°F)

OPTICAL PROPERTIES

Color	STANDARD COLORS			CUSTOM COLORS			
	IW	White	Reflective	Green	Clear	Bronze	Blue
Visible Light Transmission	55%	40%	20%	75%	89%	38%	64%
Solar Transmission	58%	44%	18%	69%	80%	41%	73%
Solar Heat Gain Coefficient	0.61	0.48	0.28	0.72	0.81	0.50	0.75

LT - % of visible light transmission (400-700 nm)

ST- %of total solar radiation transmission (300-2800 nm)

SHGC – Solar Heat Gail Coefficient, total solar energy transmitted through the panel = %ST +0.2x[1-(%st+%sr)]

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