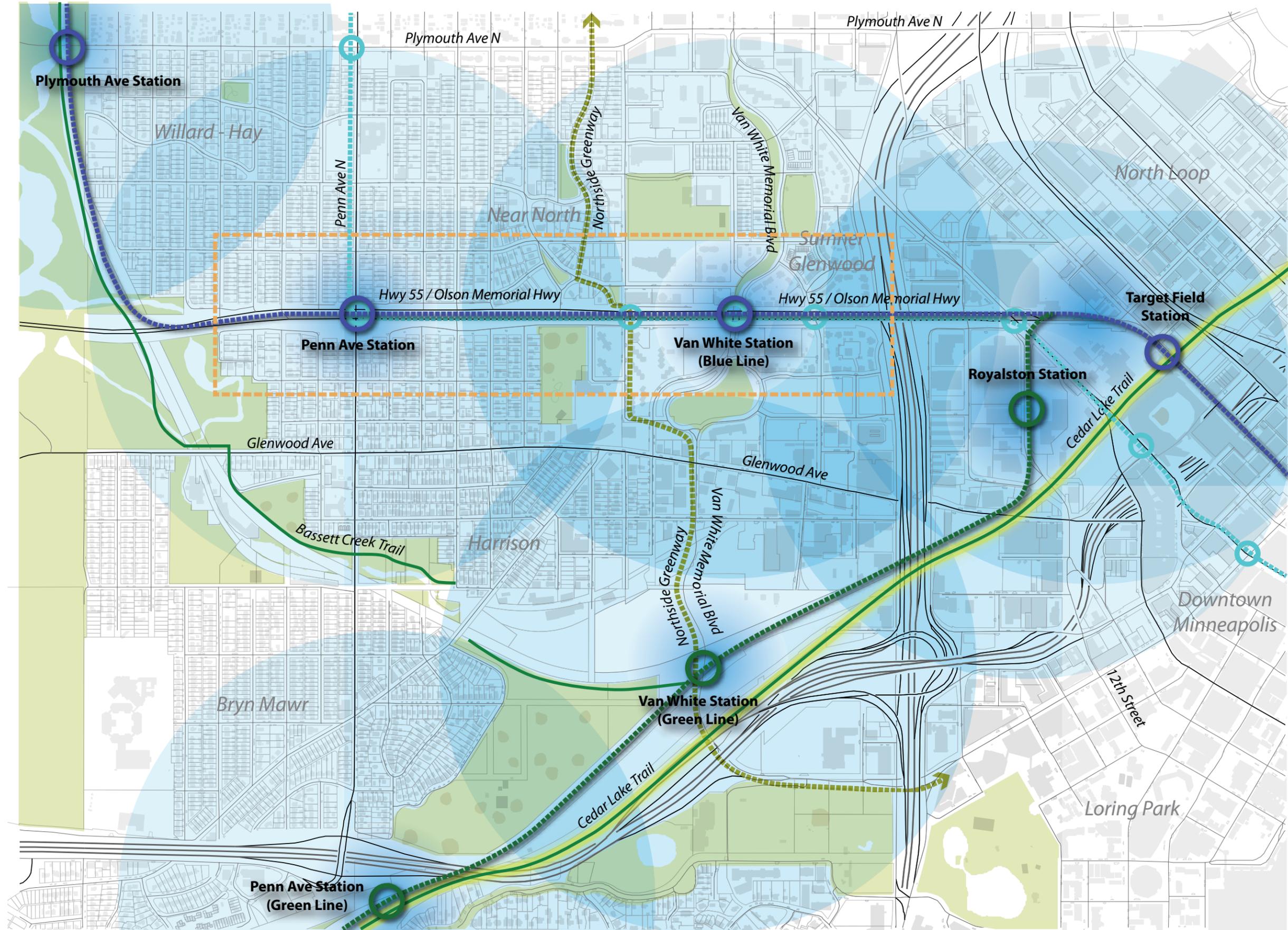


Bottineau LRT / Olson Memorial Highway Context - Station Areas

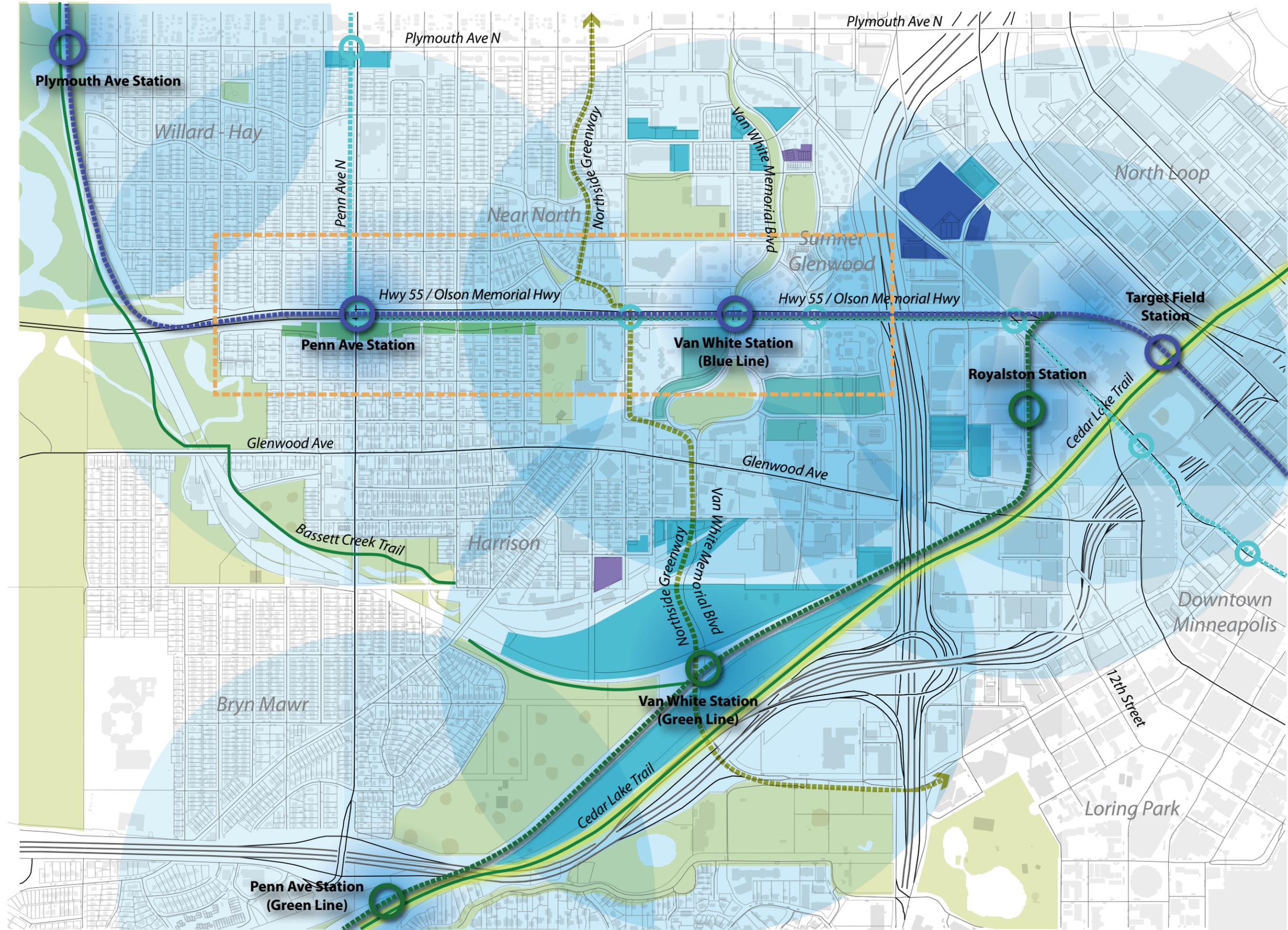


Context

The area comprising the lower Near North Side which includes the Bassett Creek Valley, Sumner-Glenwood and Harrison neighborhoods as well as the western portion of the North Loop are poised to see a massive amount of investment in transit infrastructure. The concentration of this investment in a relatively small geographic area creates a unique opportunity to coordinate planning efforts towards achieving many of Minneapolis goals at the same time for a sector of the city that has been historically underserved and disinvested.

- - - - BRT
- - - - LRT
- Park Trail
- - - - Tentative Northside Greenway
- Area of Study
- Transit Station Area

Bottineau LRT / Olson Memorial Highway Context - Publicly Owned Land

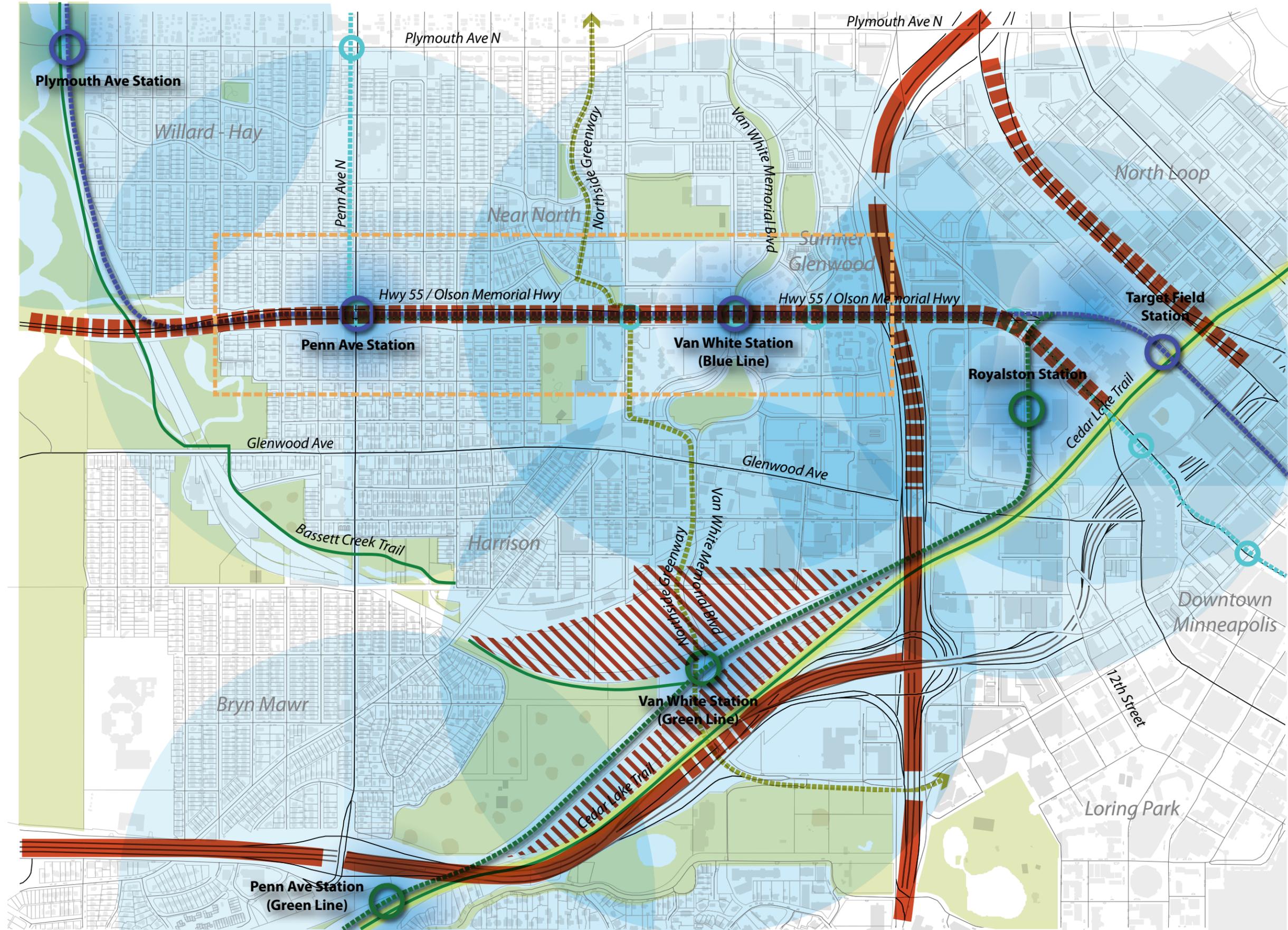


Context

Many key redevelopment sites around the 55 and Van White station area are publicly owned allowing for increased public stewardship of the development process and expedited delivery of desired development outcomes. Many of these parcel are already vacant or underdeveloped making for a unique opportunity within the city borders to add to our growth and density goals on a major transit corridor.

- - - - BRT
- LRT
- - - - Tentative Northside Greenway
- — — — Park Trail
- Hennepin County Land
- City of Minneapolis
- MPHA
- Met Council
- MN Dot
- Area of Study
- Transit Station Area

Bottineau LRT / Olson Memorial Highway Context



Context

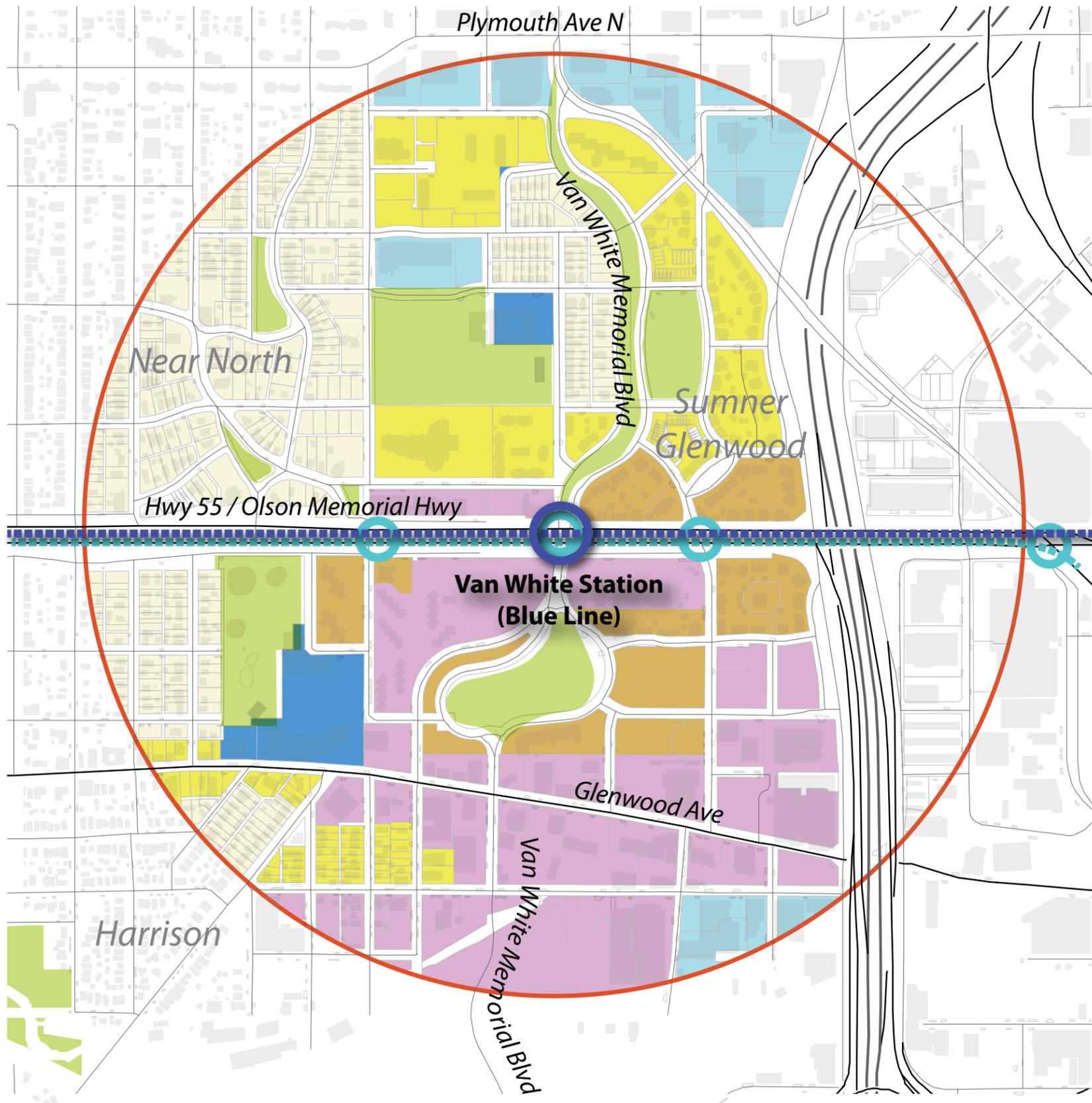
Several barriers exist to the comfortable and safe movement of residents through the neighborhoods of the Near Northside. The main barriers that exist are the Highway Interstates of 94 and 394 which create major blockages for all modes of transit to the East and South of the study area and impede movements between the Downtown Business district and the neighborhoods in question. Additionally, Hwy 55 is seen by the community as a barrier between neighborhoods. Several schools are located on or around Highway 55 and the safety of children needing to cross the roadway on their way to and from school is a major concern.

-  BRT
-  LRT
-  Tentative Northside Greenway
-  Park Trail
-  Area of Study
-  Transit Station Area

- *The Van White and Penn Ave Station Area Plans were prepared as a partnership between Hennepin County and the City of Minneapolis*
- *The draft final plans were presented at an open house on June 4, 2015, with a placeholder for additional planning work for the Olson Memorial corridor*
- *The road design and development scenarios for the corridor presented tonight will be incorporated into the plans for adoption into the comprehensive plan*

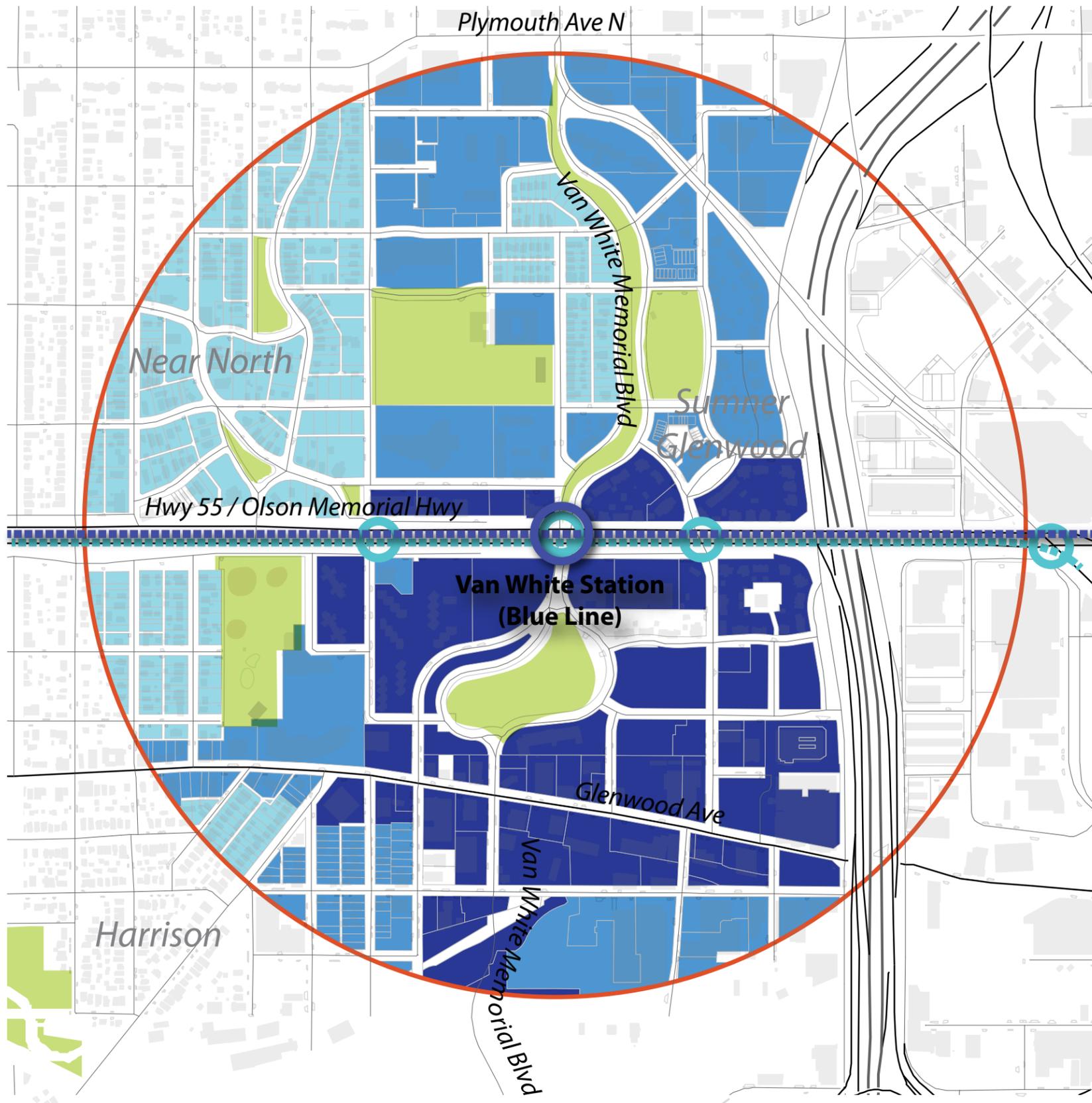
- *Van White Station land use map shows the area north of Olson Memorial reflecting current adopted city land use guidance*
- *South of Olson Memorial Highway land use guidance has been modified to allow more flexibility by changing categories to mixed-use, which would allow commercial, office, and residential*
- *Van White Station development intensity map shows intensities or scales:*
 - *Neighborhood Scale – existing single-family pattern*
 - *Urban Scale – buildings up to 4 stories*
 - *Transit Scale – buildings 5 stories or greater*

Van White Station Area - Land Use Plan



-  **LRT Station**
-  **BRT Station**
-  **1/2 Mile Walkshed**
-  **BRT Line**
-  **LRT Line**
-  **Low Density Residential**
-  **Medium Density Residential**
-  **Medium to High Density Residential**
-  **Mixed Use Medium to High Density Residential**
-  **Parks and Open Space**
-  **Public Institutional**
-  **Transitional Industrial**

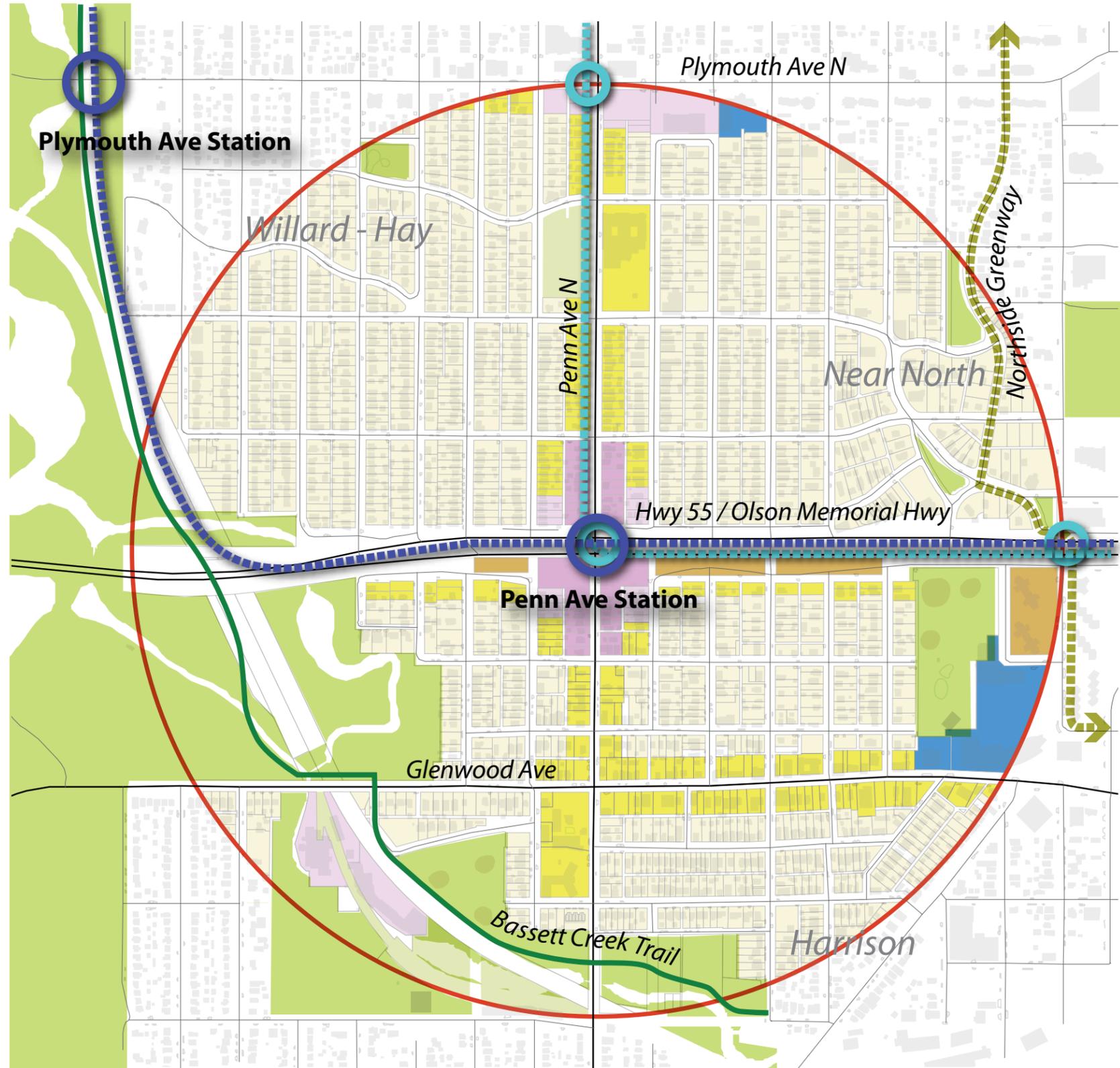
Van White Station Area - Development Intensity Plan



-  **LRT Station**
-  **BRT Station**
-  **1/2 Mile Walkshed**
-  **BRT Line**
-  **LRT Line**
-  **Neighborhood Scale**
Single Family Homes, townhomes, and two to three story small apartment / condominium buildings
-  **Urban Scale**
Townhomes, and apartment/condominium buildings up to four stories
-  **Transit Scale**
Apartment / condominium building heights of 5 or more stories
-  **Parks and Open Space**

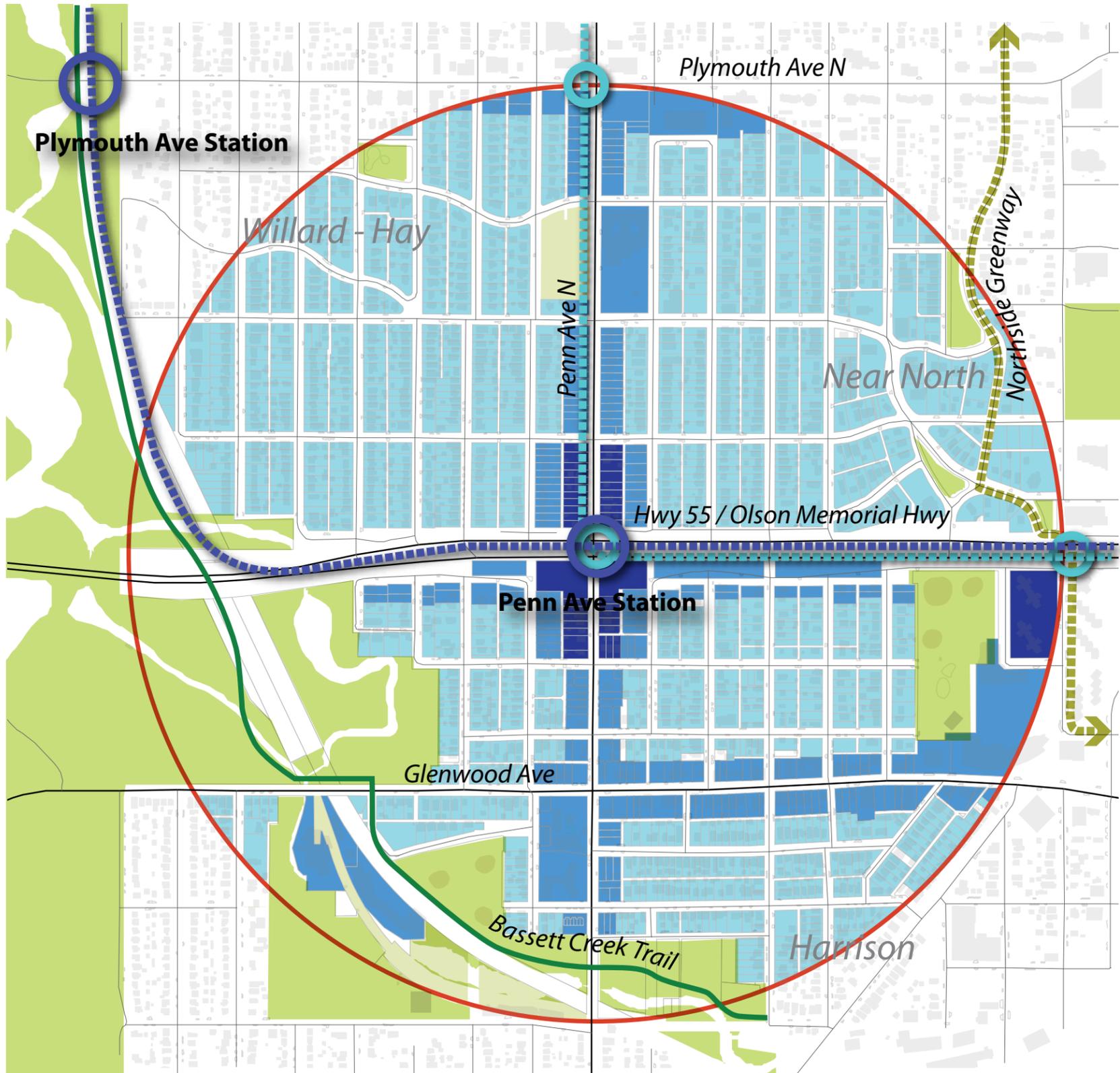
- *Penn Avenue Station land use reflects current adopted city land use guidance for most of the station area*
- *At the Penn Avenue/Olson Memorial intersection the guidance has been modified to allow more flexibility by changing the category to mixed-use, which would allow commercial, office, and residential*
- *Penn Avenue Station development intensity map shows intensities or scales:*
 - *Neighborhood Scale – existing single-family pattern*
 - *Urban Scale – buildings up to 4 stories*
 - *Transit Scale – buildings up to 5 stories*
- *Olson Memorial corridor between the stations proposed to have a residential designation*
- *Olson Memorial corridor development intensity will stay at neighborhood scale on the north side and will be urban or transit scale on the south side*

Penn Ave Station Area - Land Use Plan



-  LRT Station
-  BRT Station
-  1/2 Mile Walkshed
-  BRT Line
-  LRT Line
-  Low Density Residential
-  Medium Density Residential
-  Medium to High Density Residential
-  Mixed Use Medium to High Density Residential
-  Parks and Open Space
-  Public Institutional
-  Transitional Industrial

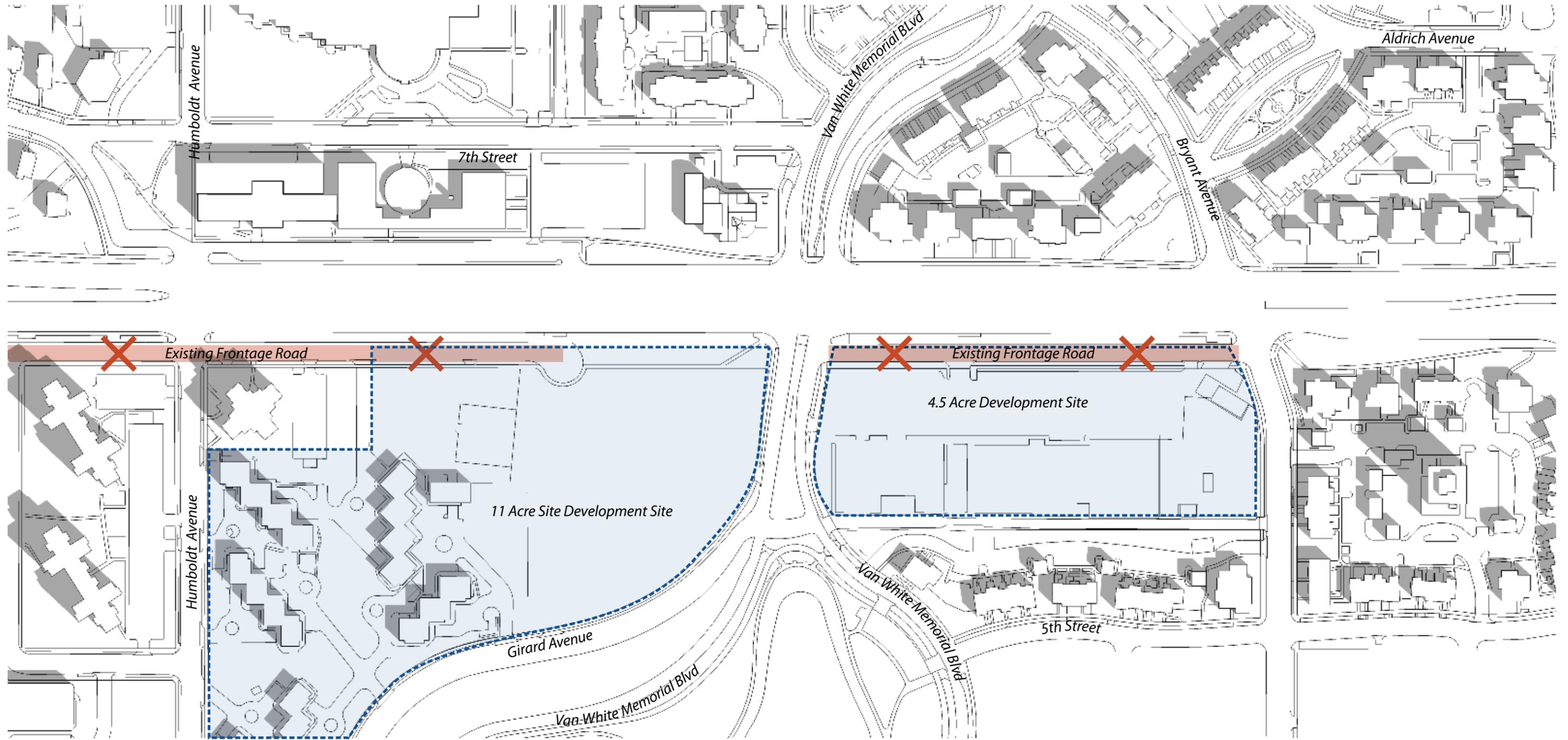
Penn Ave Station Area - Development Intensity Plan



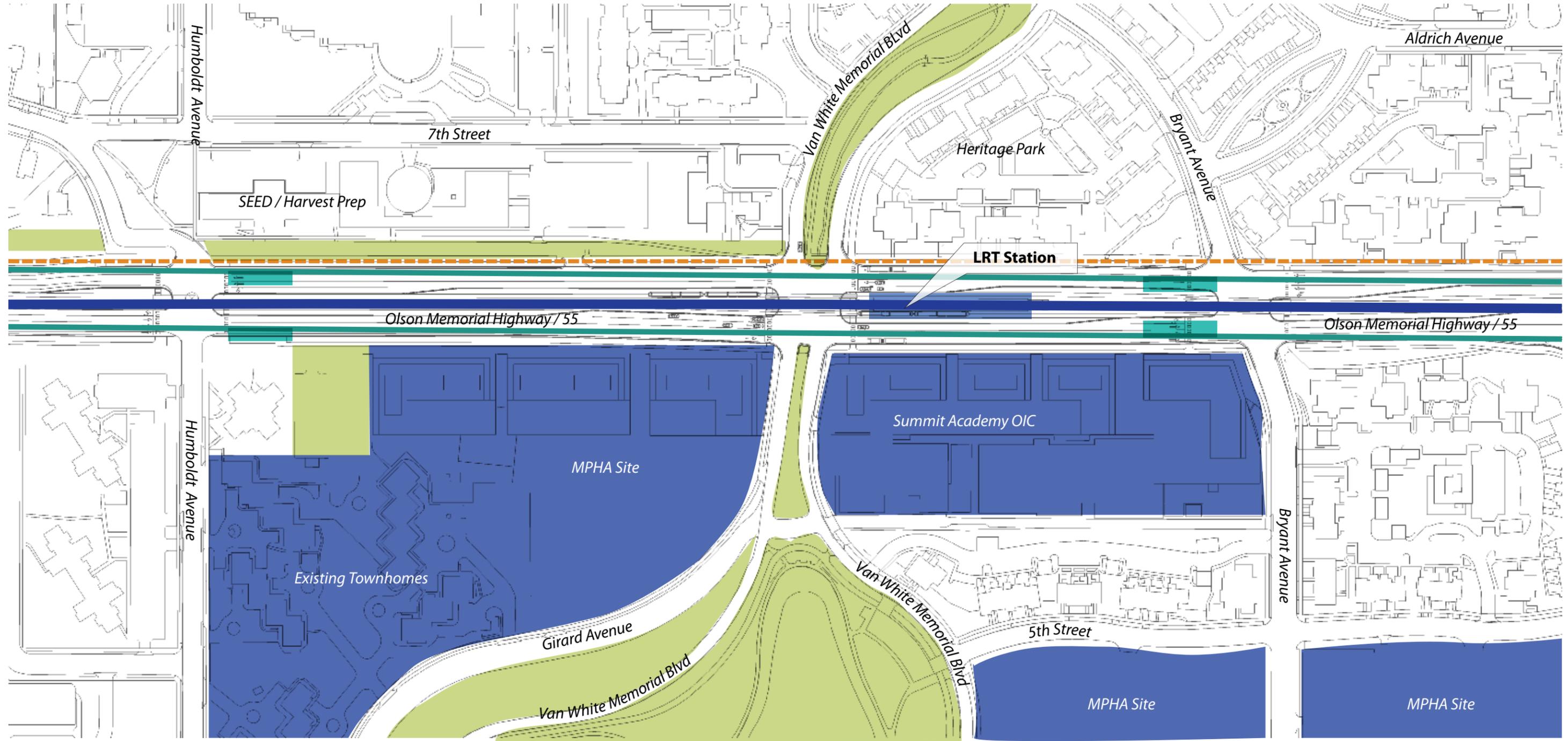
-  **LRT Station**
-  **BRT Station**
-  **1/2 Mile Walkshed**
-  **BRT Line**
-  **LRT Line**
-  **Neighborhood Scale**
Single Family Homes, townhomes, and two to three story small apartment / condominium buildings
-  **Urban Scale**
Townhomes, and apartment/condominium buildings up to four stories
-  **Transit Scale**
Apartment / condominium buildings heights of up to 5 stories
-  **Parks and Open Space**

- Capitalize on **development opportunity** created by LRT investment, especially on the large tracts of **publically owned land** available in area
- Create **mixed-income and mixed-use neighborhoods** where people have the option of using transit
- Provide a **range of density** and housing types that support **neighborhood-serving retail**
- **Create opportunity for new residents** to live in the area, while preserving ability of **existing residents** to stay in neighborhood
- Enhance **pedestrian and bicycle friendly** neighborhoods by incorporating green space, improving safety, and providing **connectivity** to stations

Van White Station Area Existing Conditions

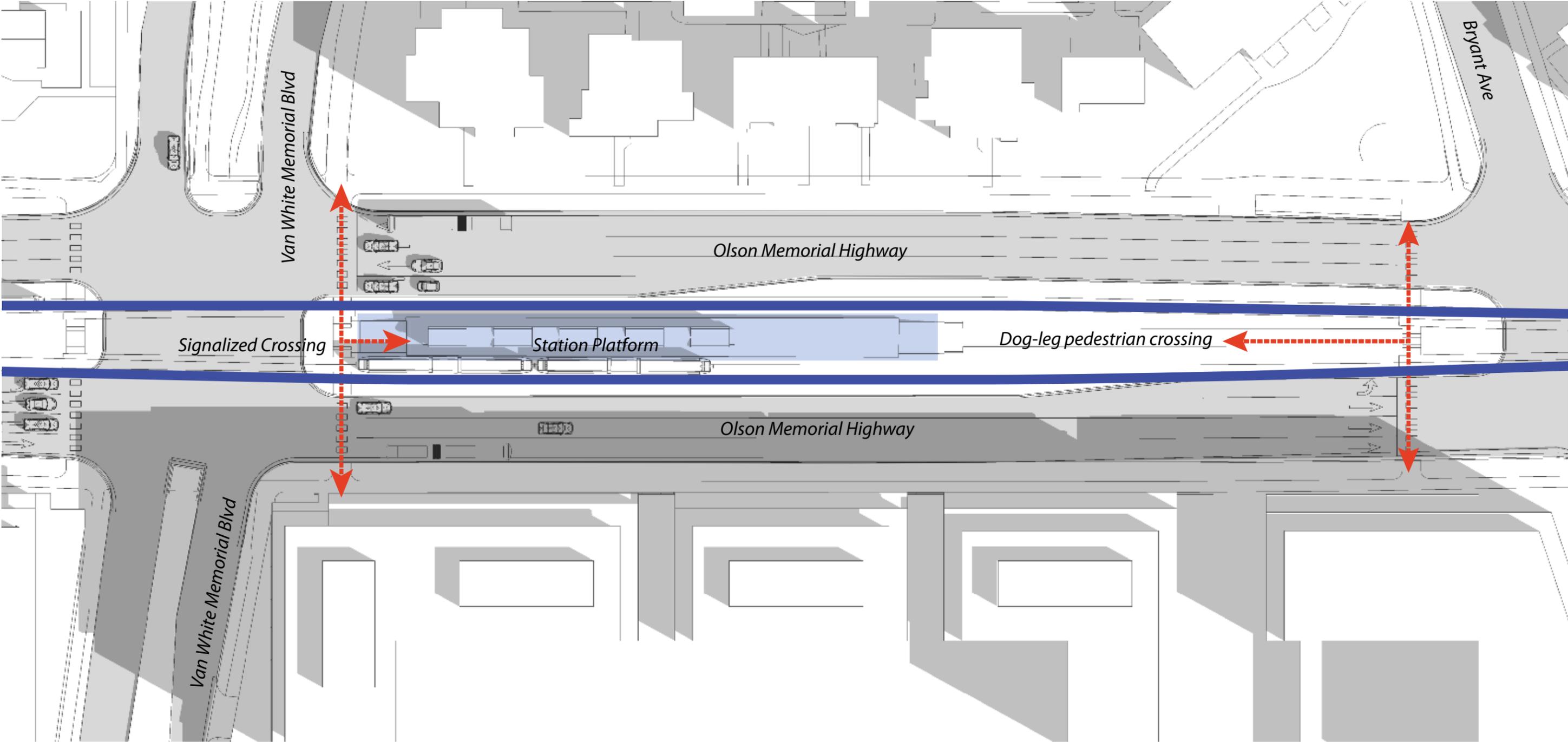


Van White Station Area Development Plan

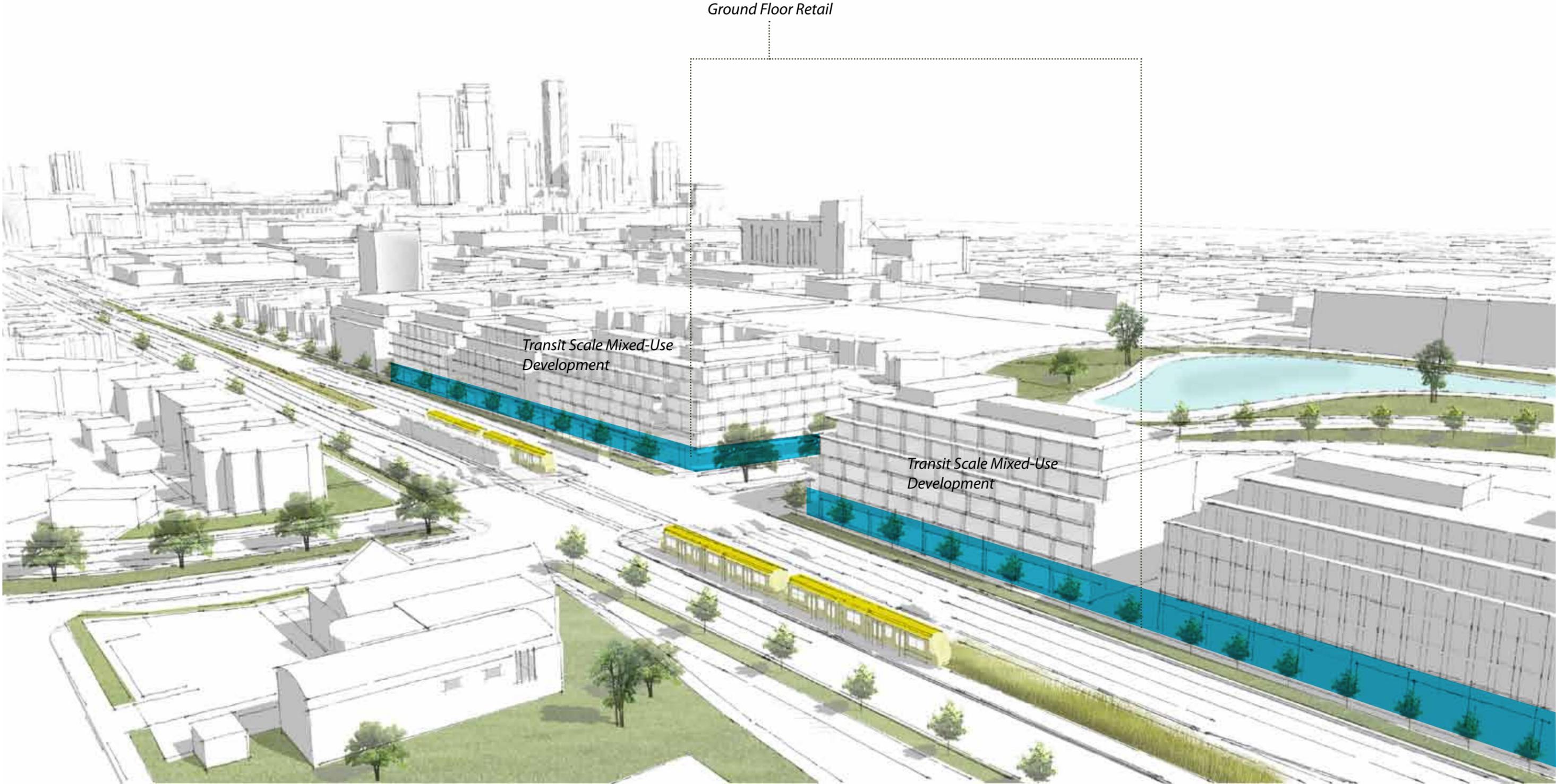


- | | | | |
|---|---|---|-----------------------------|
|  | Transit Scale Development
Apartment / condominium building heights of five stories or more. |  | Parks and Open Space |
|  | Urban Scale Development
Townhomes and apartment/condominium buildings up to four stories. |  | BRT Station |
| | |  | LRT Station |
| | |  | Future Bike Path |
| | |  | BRT |
| | |  | LRT |

Van White Pedestrian Infrastructure



Van White Station Area - Transit Oriented Development Vision

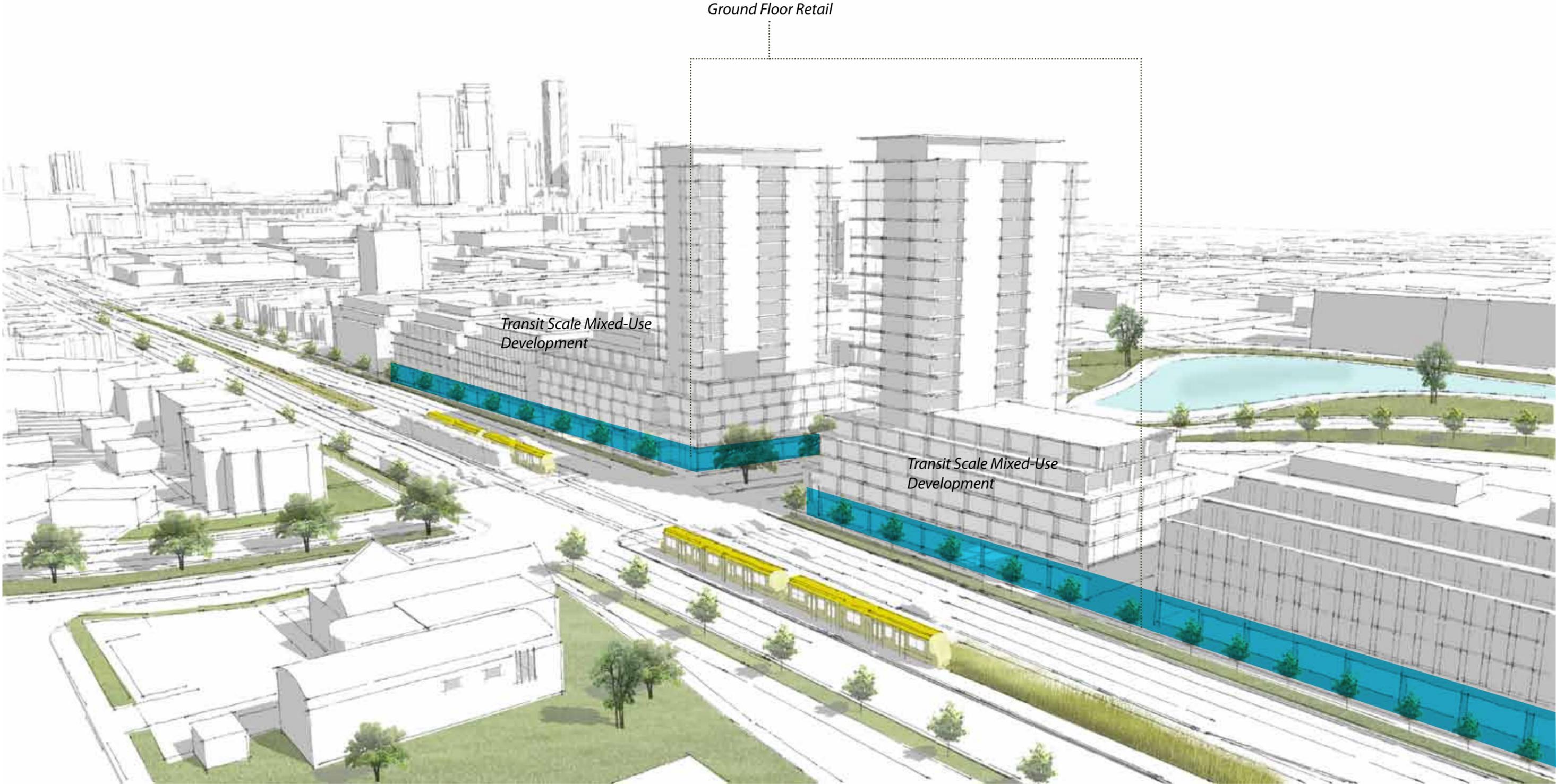


Ground Floor Retail

Transit Scale Mixed-Use Development

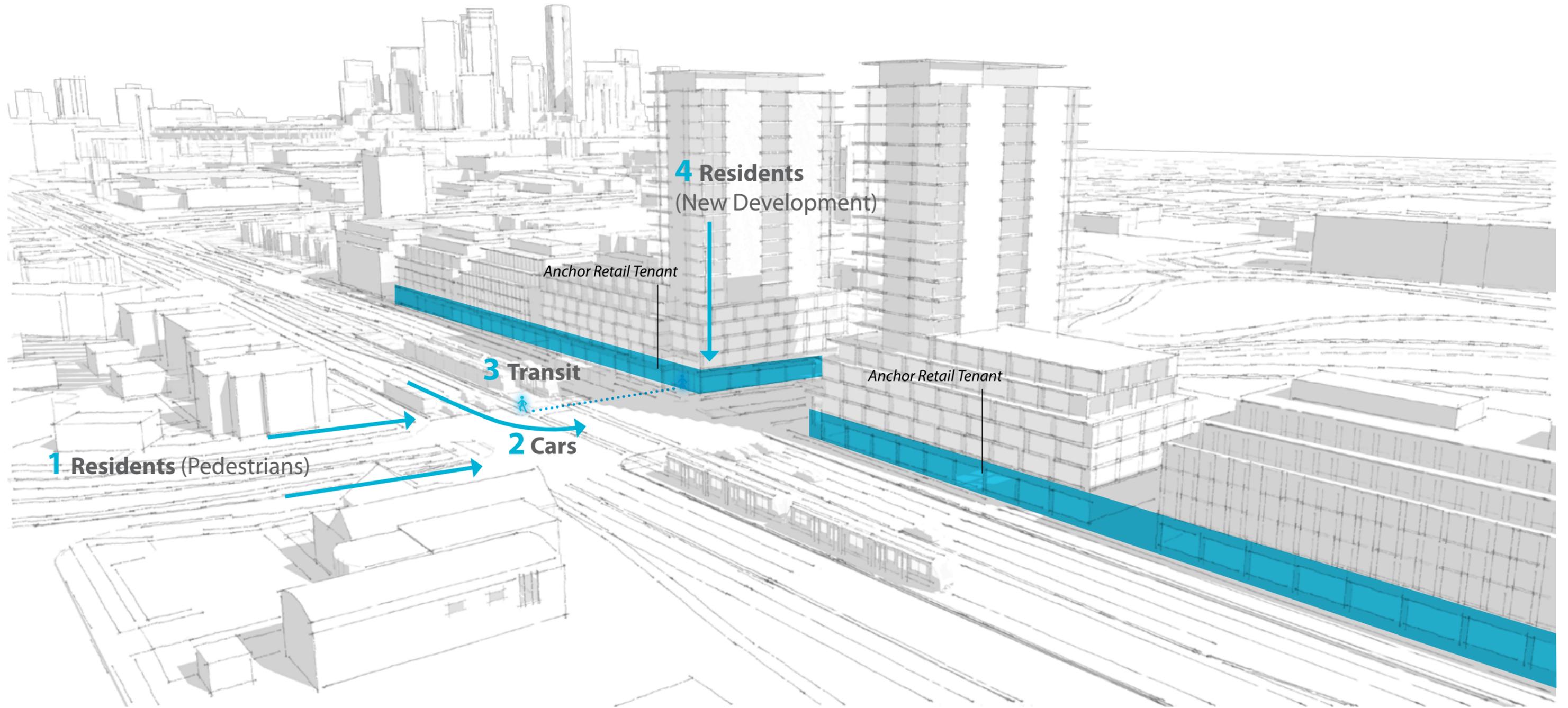
Transit Scale Mixed-Use Development

Van White Station Area - Transit Oriented Development Vision



Making Retail Work - Customer Base

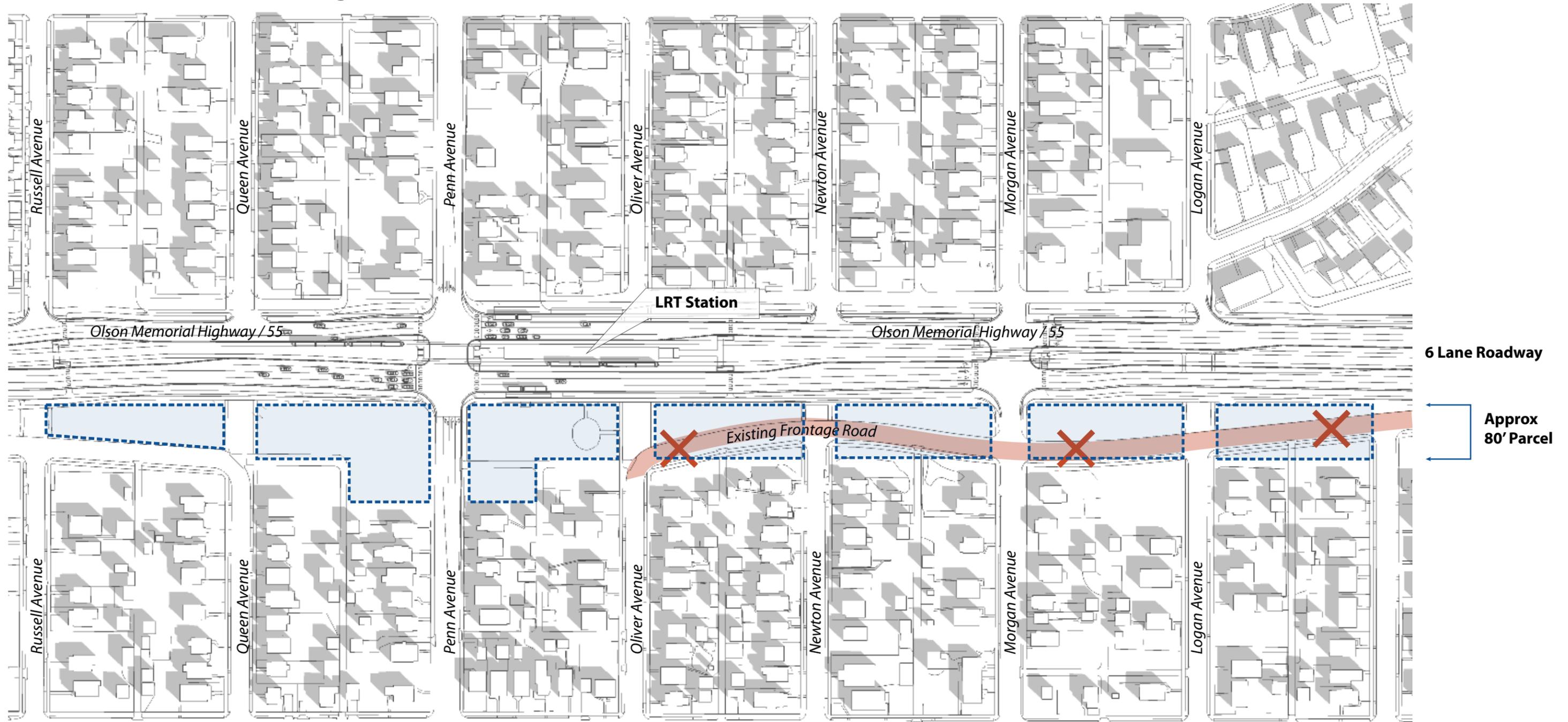
Successful retail in a mixed use development will be dependent upon several populations of people and modes of transit in order to work. These include pedestrians traveling from nearby neighborhoods, people driving their cars and commuting from work, LRT passengers and new residents of mixed use development at the station area.





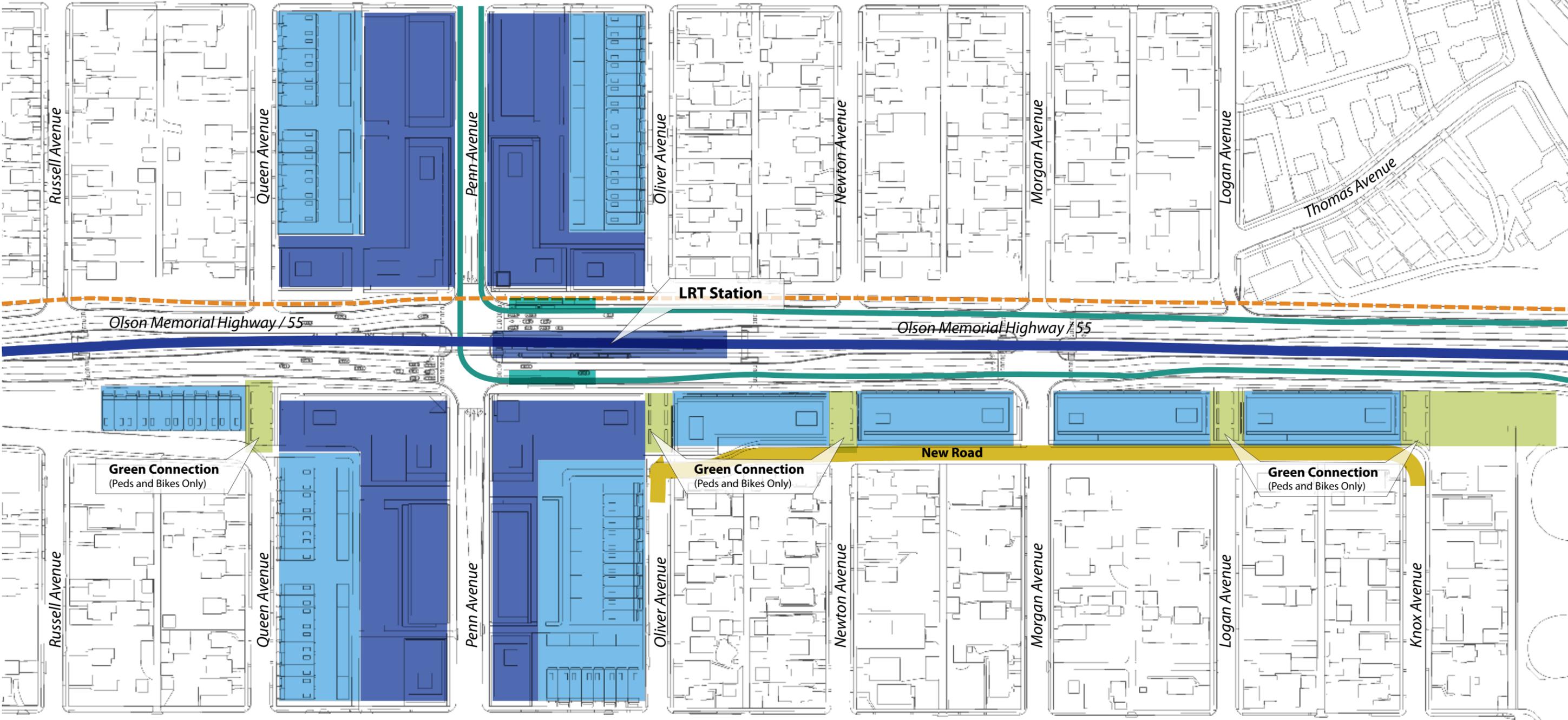
Van White Street Perspective Looking South

Penn Ave Station Area Existing Conditions



-  Potential Development Parcel
-  Existing Frontage Road

Penn Ave Station Area Development Plan



- Transit Scale Development**
Apartment / condominium
building heights up to 5 stories

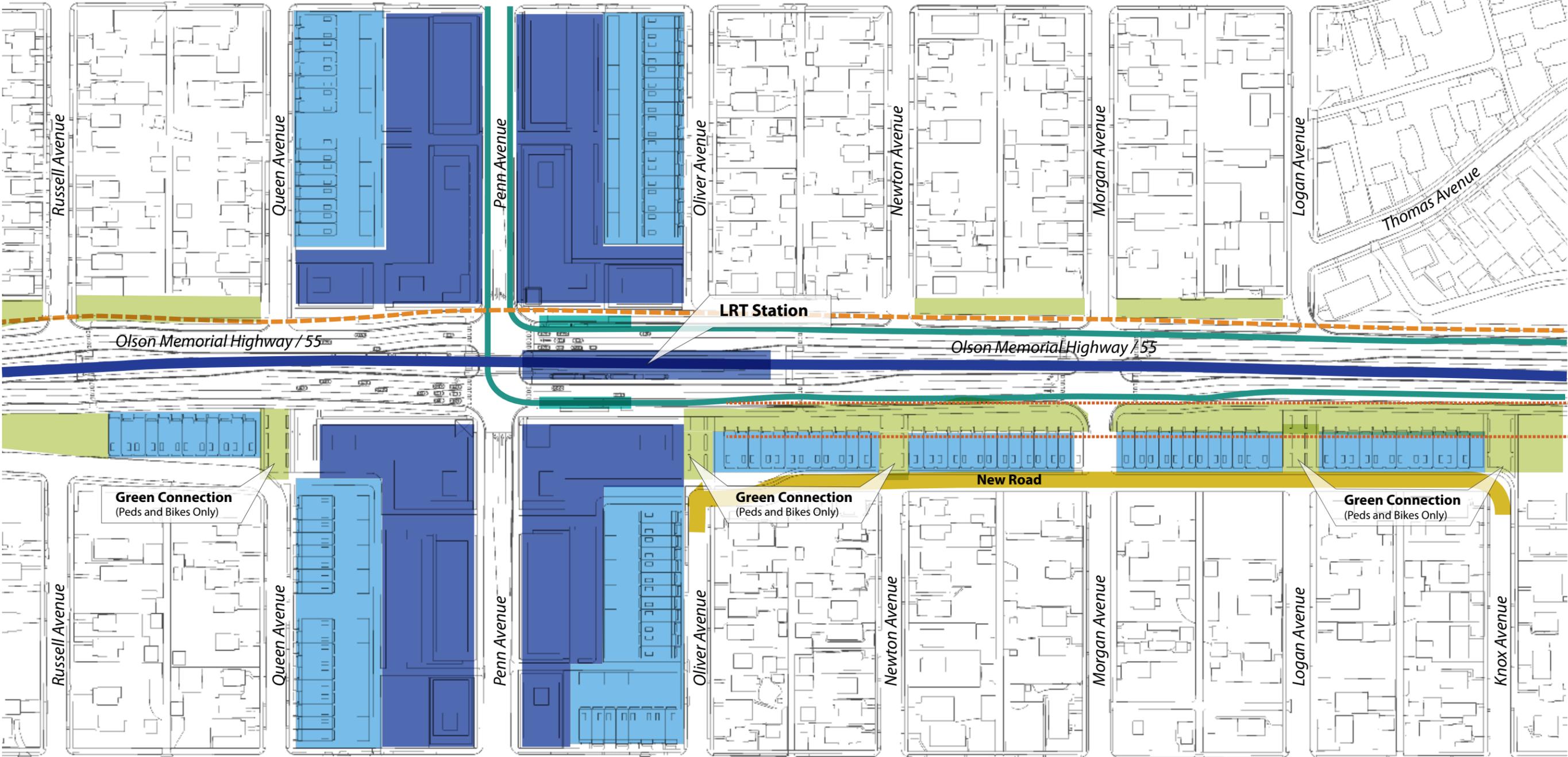
Parks and Open Space
- Urban Scale Development**
Townhomes and
apartment/condominium buildings
up to four stories.

BRT Station
- LRT Station**

Future Bike Path
- BRT**

LRT
- New Road**

Penn Ave Station Area Development Plan



30 ft Green Buffer

Green Connection
(Peds and Bikes Only)

Green Connection
(Peds and Bikes Only)

Green Connection
(Peds and Bikes Only)

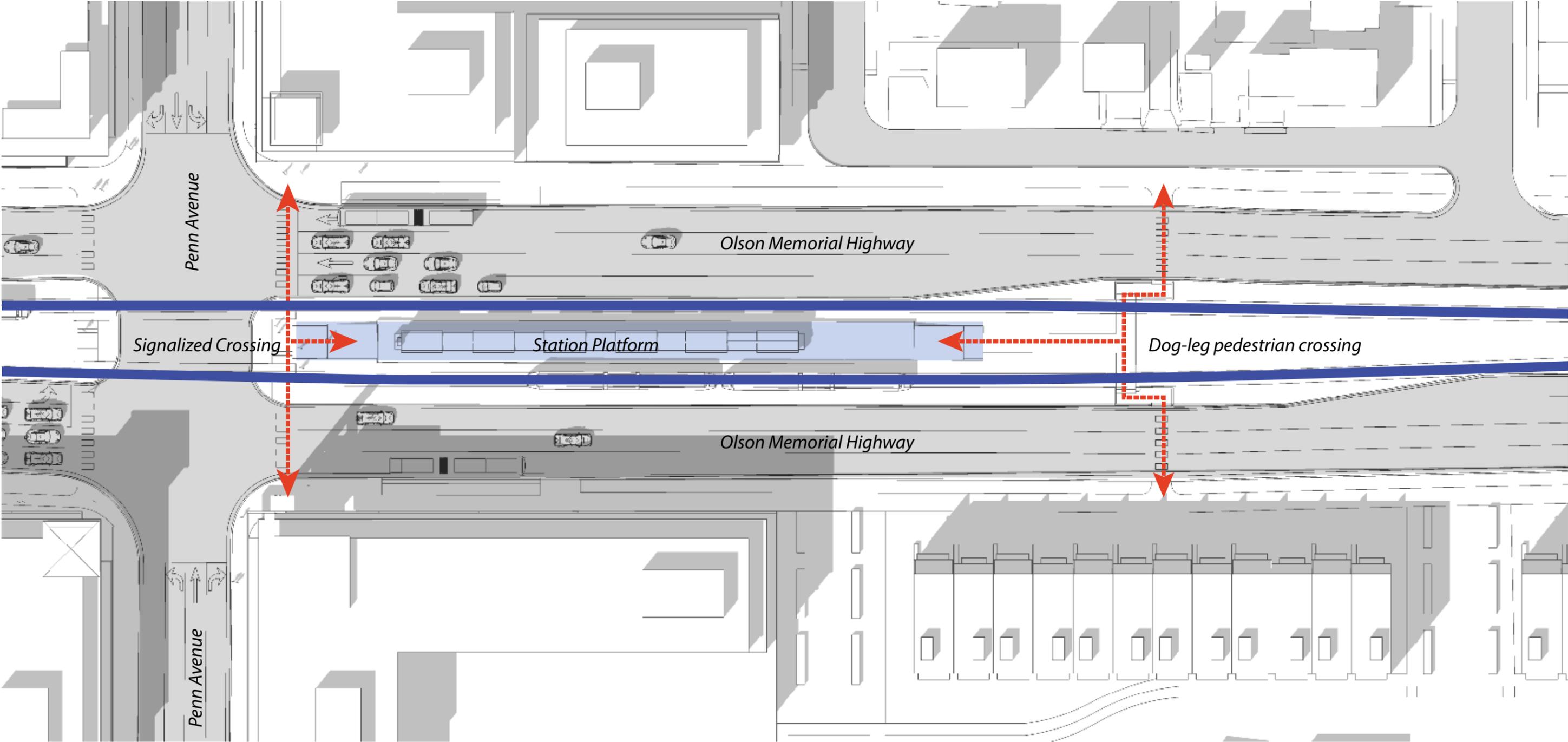
LRT Station

New Road

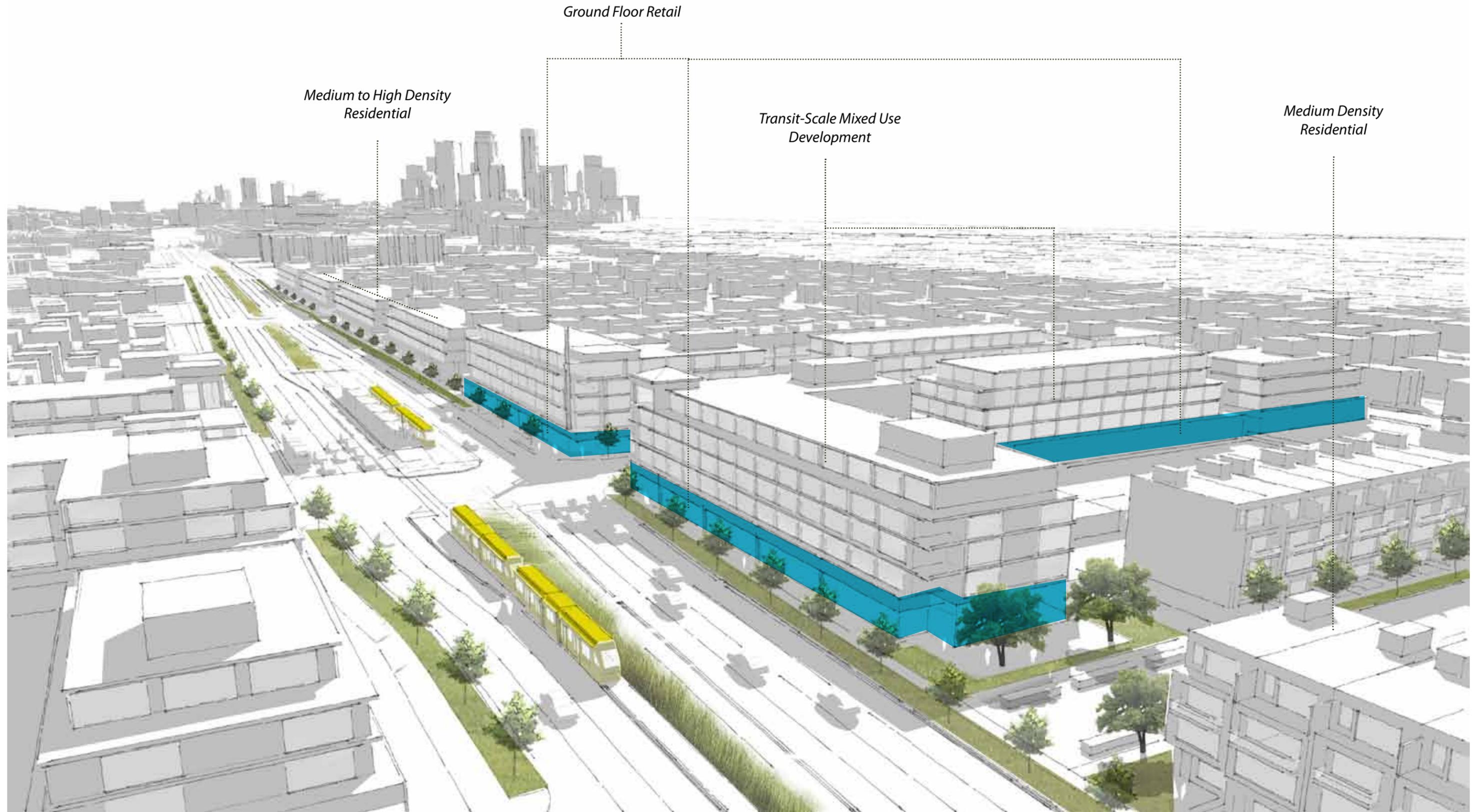
- Transit Scale Development**
Apartment / condominium building heights up to 5 stories
- Urban Scale Development**
Townhomes and apartment/condominium buildings up to four stories.

- Parks and Open Space**
- BRT Station**
- LRT Station**
- Future Bike Path**
- BRT**
- LRT**

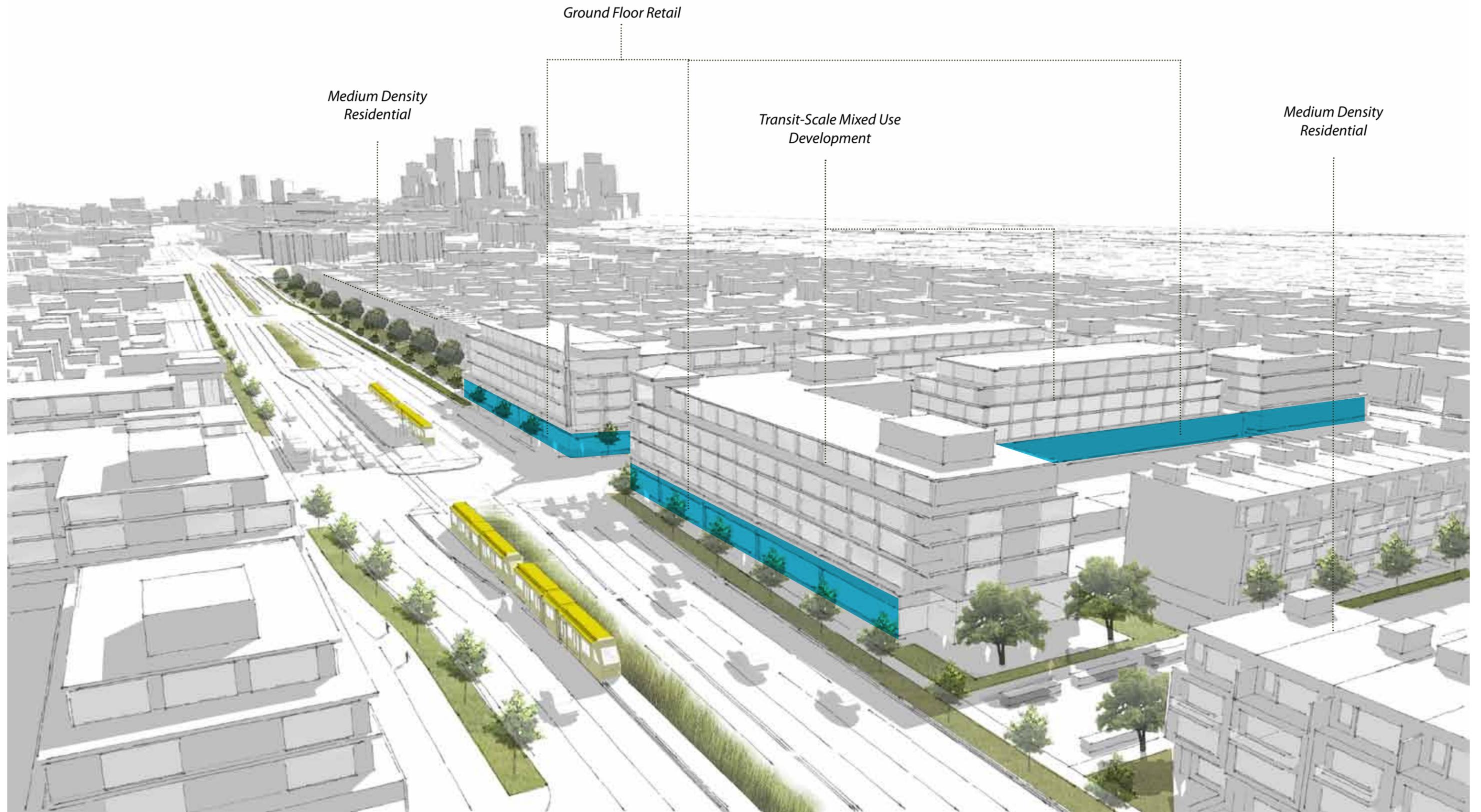
Penn Avenue Pedestrian Infrastructure



Penn Ave Station Area - Transit Oriented Development Vision

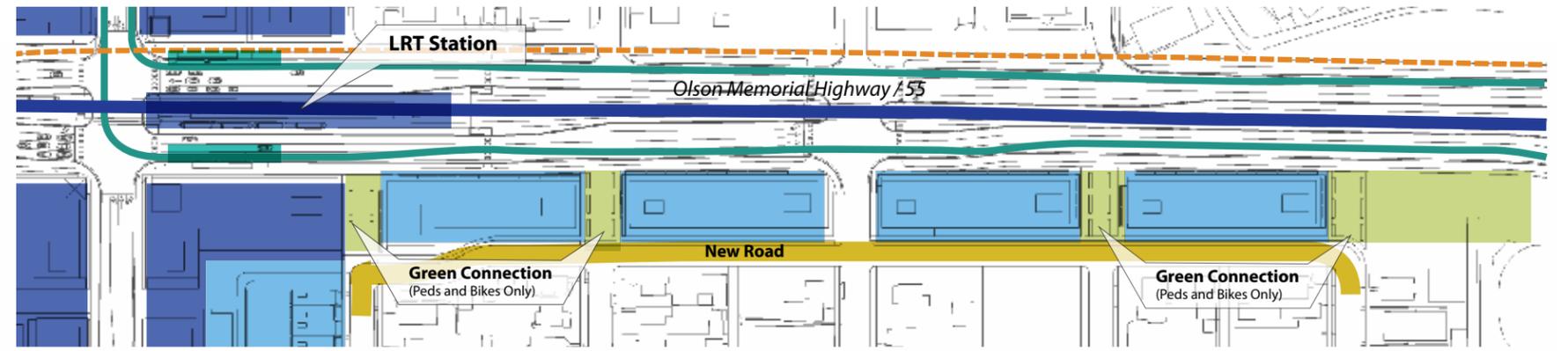


Penn Ave Station Area - Transit Oriented Development Vision



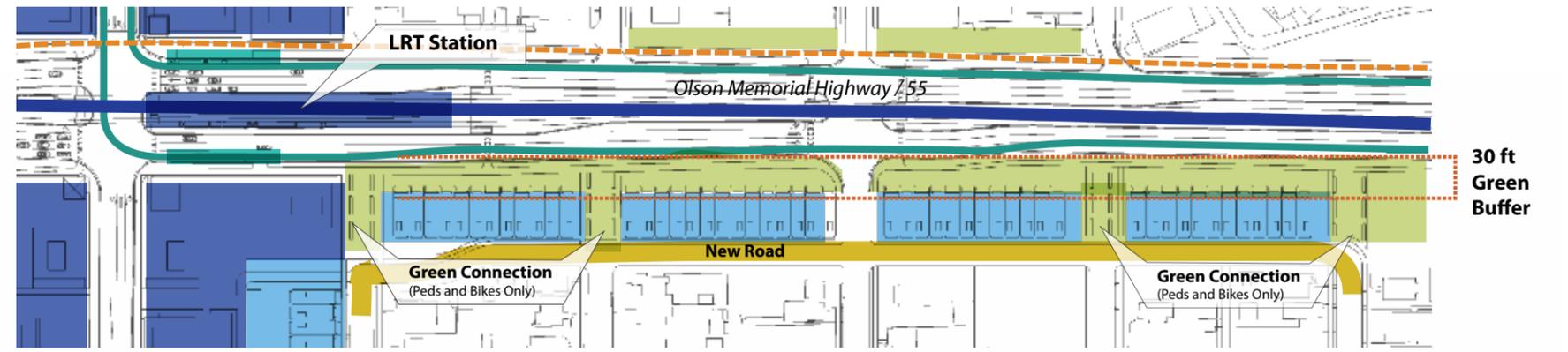
Penn Ave South Side Scenario 1

Detail Plan of Olson Memorial South Side showing development up to the back of the sidewalk with green connectors at side-street intersections



Penn Ave South Side Scenario 1

Detail Plan of Olson Memorial South Side showing development set back from the curb by 30 feet and a green buffer between development and the roadway.





Penn Avenue Street Perspective Looking East