

LAND USE APPLICATION SUMMARY

Property Location: 117 27th Avenue Southeast and on adjacent unplatted, formerly railroad land (2512 Essex Street Southeast and a portion of 1710 Franklin Avenue Southeast)

Project Name: Apartment Building

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Applicant: Elsey Partners, LLC

Project Contact: Bryan Elsey

Request: Apartment Building

Required Applications:

Rezoning	Petition for a rezoning from the I1 to the R5 zoning district and to remove the IL/Industrial Living Overlay District for unplatted railroad land (2512 Essex Street Southeast and a portion of 1710 Franklin Avenue Southeast)
Conditional Use Permit for a Planned Unit Development	To allow a Planned Unit Development (PUD) with 195 dwelling units in the R5/Multiple Family District and the UA/University Area Overlay District.
Variance	To reduce the established front yard setback requirement to 15 feet for the building, 5 feet for mechanical equipment, and 0 feet for the plaza.
Variance	To increase the amount of impervious surface allowed on the site from 85 percent to 86 percent.
Site Plan Review	For a six-story apartment building with 195 residential units and 450 bedrooms.
Preliminary and Final Plat	Preliminary and final plat

SITE DATA

Existing Zoning	I1/ Light Industrial District('99) R5/ Multiple Family District('99) IL/ Industrial Living Overlay District('99) UA/ UA University Area Overlay District
Lot Area	88,452 square feet / 2.03 acres
Ward(s)	2
Neighborhood(s)	Prospect Park
Designated Future Land Use	Urban Neighborhood and Transitional Industrial
Land Use Features	Growth Center (University of Minnesota)
Small Area Plan(s)	<u>Stadium Village University Avenue Station Area Plan (2012)</u> <u>Bicycle Master Plan (2011)</u>

Date Application Deemed Complete	December 7, 2015	Date Extension Letter Sent	December 18, 2015
End of 60-Day Decision Period	February 5, 2016	End of 120-Day Decision Period	April 5, 2016

BACKGROUND

SITE DESCRIPTION AND PRESENT USE. The project site is located a block to the east of Huron Avenue, a block north of Interstate 94 and two blocks to the south of University Avenue Southeast. It includes a platted lot and a portion of a vacated Soo Line Railroad rail spur. The Smith-Sharpe Fire Brick Supply Company building built in 1903 is located at 117 27th Avenue Southeast. In addition to the building, there is an unmarked surface parking area located at the southern and western end of the site.

SURROUNDING PROPERTIES AND NEIGHBORHOOD. The project site is located near the University of Minnesota East Bank campus, an identified Growth Center, two blocks to the south of the University Avenue Commercial Corridor, and two blocks south of the Stadium Village Activity Center.

The zoning of the parcels on the west side of 27th Avenue Southeast are predominantly zoned high density residential (R5 or R6) and industrial (I1 or I2). All of the industrial zoned lots in this area include the Industrial Living Overlay District. The east side of 27th Avenue within the area is primarily zoned R4/Multiple-Family.

The adjacent properties include a few smaller multiple family developments to the north, the Glendale Townhome development to the east, and University Commons to the west. Within the last five years there have been at least three major residential development projects built within a quarter mile of the project site and three other developments approved by the City Planning Commission but construction has not been completed.

PROJECT DESCRIPTION. The applicants are proposing to tear down the existing building and build a six-story, multifamily building with 195 residential units, 450 beds and 140 underground parking stalls. The building would be built in two phases; however the intent is to start Phase II right after the completion of Phase I. The project also includes an outdoor plaza and a bike walking path at the rear of the building that would help provide a diagonal connection with 27th Avenue Southeast and Essex Street Southeast.

COW REVIEWS: The project was informally reviewed by the City Planning Commission at two Committee of the Whole meetings. The applicant has incorporated feedback from these meetings including exterior material changes, alternating floor heights of the building, adding additional projecting balconies, and adding a pedestrian/bike path at the back of the building.

RELATED APPROVALS. There are no previous land use files for the development site.

PUBLIC COMMENTS. The Prospect Park Association wrote a letter of support for the project. Any additional correspondence received after the publication of the staff report and prior to the public meeting would be forwarded on to the Planning Commission for consideration.

ANALYSIS

REZONING

The Department of Community Planning and Economic Development has analyzed the application for a petition to rezone the unplatted railroad land at 2512 Essex Street Southeast and a portion of 1710

Franklin Avenue Southeast from I1 to R5 and to remove the IL/Industrial Living Overlay District based on the following findings:

1. Whether the amendment is consistent with the applicable policies of the comprehensive plan.

The rezoning would be consistent with the applicable policies of The Minneapolis Plan for Sustainable Growth. The site is located just south of the Stadium Village Activity Center and the Prospect Park LRT Transit Station Area. Generally, the Minneapolis comprehensive plan is supportive of zoning districts like the R5/Multiple-Family near these future land use features to increase the population of these areas.

The project site has two future land uses: Urban Neighborhood and Transitional Industrial. The Urban Neighborhood land use is supportive of residential zoning with a range of densities based on the surrounding context. Transitional Industrial areas are located outside of the boundaries of Industrial Employment Districts and are recognized as areas that evolve to other uses compatible with surrounding development. A rezoning of the subject property to R5 would be compatible with the surrounding multiple family zoning districts and multiple family uses.

According to the principles and policies outlined in The Minneapolis Plan for Sustainable Growth, the following apply to this proposal:

Land Use Policy 1.8: Preserve the stability and diversity of the city's neighborhoods while allowing for increased density in order to attract and retain long-term residents and businesses.

- 1.8.1 Promote a range of housing types and residential densities, with highest density development concentrated in and along appropriate land use features.

Land Use Policy 1.13: Support high density development near transit stations in ways that encourage transit use and contribute to interesting and vibrant places.

- 1.13.5 Concentrate highest densities and mixed use development adjacent to the transit station and along connecting corridors served by bus.

Land Use Policy 1.15: Support development of Growth Centers as locations for concentration of jobs and housing, and supporting services.

- 1.15.3 Encourage the development of high- to very high-density housing within Growth Centers.

Housing Policy 3.1: Grow by increasing the supply of housing.

- 3.1.1 Support the development of new medium- and high-density housing in appropriate locations throughout the city.

Housing Policy 3.2: Support housing density in locations that are well connected by transit, and are close to commercial, cultural and natural amenities.

- 3.2.1 Encourage and support housing development along commercial and community corridors, and in and near growth centers, activity centers, retail centers, transit station areas, and neighborhood commercial nodes.

The amendment is also consistent with the *Stadium Village University Avenue Station Area Plan (2012)*. It is within the 27th Avenue Open Space District. This plan provides limited land use guidance for the 27th Avenue Open Space District. However, it recognizes that “numerous potential redevelopment opportunity sites defined in the district along 27th Avenue” exist, which is the case with the project site.

2. *Whether the amendment is in the public interest and is not solely for the interest of a single property owner.*

The amendment is in the public interest and not solely for the interest of a single property owner. Allowing for the rezoning would allow the applicant to construct a high-density residential development on the site. As outlined in the finding above, policies within *The Minneapolis Plan for Sustainable Growth* support constructing high-density housing in this area.

3. *Whether the existing uses of property and the zoning classification of property within the general area of the property in question are compatible with the proposed zoning classification, where the amendment is to change the zoning classification of particular property.*

The existing uses of property within the general area are compatible with the rezoning classification as most of the adjacent uses are multiple-family residential. The nearby daycare facility and hotel (501 Huron Boulevard Southeast and 2510 Essex Street Southeast) are also compatible with the proposed residential development.

The zoning classification of property within the general area is also compatible with the proposed zoning classification. The parcels to the north, northeast and east are zoned multiple family residential (R4 and R5). Even though parcels to the west of the project site are zoned II/Light Industrial, these zoning districts include the Industrial Living Overlay District.

4. *Whether there are reasonable uses of the property in question permitted under the existing zoning classification, where the amendment is to change the zoning classification of particular property.*

There are reasonable uses of the property in question permitted under the existing II zoning classification; however the project site with split zoning (R5 and II) is not able to be developed without the rezoning request. As outlined in Section 535.210-Lots Containing Two or More Zoning Classifications, when lots are combined shall not result in more than one (I) primary zoning classification on a single zoning lot, except as allowed by the TP Transitional Parking Overlay District.

5. *Whether there has been a change in the character or trend of development in the general area of the property in question, which has taken place since such property was placed in its present zoning classification, where the amendment is to change the zoning classification of particular property.*

There has been a change in the character in the general area from industrial to multiple-family residential. Within the last five years, there have been at least four, high-density, multiple-family developments either built or approved within a quarter mile of the project site. This includes: Solhaus apartment building (six stories) at 2428 Delaware Street Southeast, Solhaus Tower (six stories) at 515 Huron Boulevard Southeast, WaHu Student Apartments (10 stories) at 1000 Washington Avenue Southeast, and the Brickhouse Lofts at 150 26th Avenue Southeast (approved by the CPC in April 2015). The rezoning and development would be consistent with the trend of development in the surrounding area.

CONDITIONAL USE PERMIT - PLANNED UNIT DEVELOPMENT

The Department of Community Planning and Economic Development has analyzed the application to allow a planned unit development with 195 dwelling units in the R5/Multiple Family District and the UA/University Area Overlay District based on the following findings:

1. *The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.*

The establishment of a planned unit development with 195 dwelling units should not prove detrimental to public health, comfort, or general welfare provided the development complies with all applicable building codes and life safety ordinances as well as Public Works Department standards.

The site is located near the Stadium Village Activity Center, Transit Station Area and near the University of Minnesota Growth Center. There are adopted policies in the Minneapolis comprehensive plan that support having high to very-high density residential development in or near these future land use features. In addition, there are other high-density residential developments in the immediate area.

2. *The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.*

The project would not be injurious to the use and enjoyment of other property in the vicinity. The surrounding area is nearly fully developed with multiple family residential developments to the east, west, and north. A proposed hotel development to the northwest of the subject property was approved earlier this year. The proposed residential use on the project site would be more compatible with the surrounding area than the existing/previous industrial use.

There is a surface parking lot to the north of the project site (2610 Essex Street Southeast). The proposed development would not adversely impact the redevelopment of this surface parking lot nor the existing development in the area.

The building length is larger than most development in the area. However, the scale and massing of the proposed development is compatible with the surrounding development. In addition, the articulation, placement, and varying floor heights of the different sections helps break up the massing and gives the appearance of several buildings.

3. *Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.*

Adequate utilities and drainage would be provided as part of the project. The applicant received initial City of Minneapolis department comments (including Public Works) at a preliminary development review meeting on October 29, 2015. The development team will be required to continue to work closely with Public Works, Construction Code Services, Land Use, Design and Preservation staff and the various utility companies during the duration of the development should the applications be approved to meet these requirements. This would be required to ensure that all procedures are followed and that the development complies with all City and other applicable requirements. Adequate access roads are also provided. Vehicles are proposed to enter and exit the site via one curb cut on 27th Avenue Southeast. This configuration received initial Public Works support.

4. *Adequate measures have been or will be taken to minimize traffic congestion in the public streets.*

The vehicle parking requirement for residential uses in the UA/University Area Overlay District is 0.5 spaces per bedroom. The proposed project includes 195 dwelling units and 450 bedrooms. Therefore, the minimum vehicle parking requirement is 225 spaces. The applicant is proposing 140 vehicle off-street parking spaces, which equates to 0.31 spaces per bedroom.

Although the project is requesting a parking reduction as a PUD alternative, adequate measures would be taken to minimize traffic congestion in the public streets. The applicant is proposing the following measures to help minimize traffic congestion in the public streets:

- Comply with the required bike parking of one space per bedroom and having at least 90 percent of the bike parking spaces meeting the long term bike parking standards. The development will have 450 bicycle parking spaces with 405 of the bike parking spaces inside the building.
- Construct a bike/walk path behind the building that is part of the Prospect Park Trail; a trail called out by the Minneapolis Bicycle Master Plan and the Stadium Village Master Plan. West of 27th Avenue Southeast, the proposed bike/walk starts on the project site and will have a through-connection to Essex Street Southeast via the new hotel development site at 501 Huron Boulevard/2510 Essex Street Southeast (BZZ-7219). This path would reduce the distance residents and others in the area would have to travel to and from the University of Minnesota.
- Install a bike room on the main floor that would allow for easy onsite bicycle repair.
- Install an electronic kiosk in the building common space with a trip planning application and alternative forms of transportation.

In addition, the project location would be able to take advantage of numerous alternative modes of transportation. This includes:

- Metro Transit light rail and bus service. The Prospect Park Light Rail Transit (LRT) station is approximately four blocks away and there are Metro Transit bus routes with stops within three blocks. According to the Stadium Village Master Plan, bus ridership is very high in this area due to frequent service on multiple routes and the availability of the U-pass, a discounted bus pass available to University students.
- Car sharing. In addition to Car2Go access, the project site is within three blocks of four permanent car-sharing locations.
- Bicycle facilities: There are two Nice Ride stations within two blocks of the project site. In addition, there are bike lanes and paths that run along the major corridors of University Avenue SE, 4th Street SE and 27th Avenue SE. Furthermore, on-campus routes provide bicycle access through the University of Minnesota Campus.

It should also be noted that students that live within the proposed building would have the opportunity to lease an off-street parking space within three blocks of the project site through the University of Minnesota.

As outlined in Section 541.330-Size, a maximum of 25 percent of the required parking spaces are allowed to be compact spaces. Currently, the applicant is proposing that all non-accessible parking spaces be compact spaces. Given that all drive aisles will meet the minimum width requirement of 22 feet and that compact vehicle use is on the rise, there are opportunities to not only reduce required stall sizes, but to also allow for more compact parking spaces. Supporting the use of small vehicles and subsequently allowing more compact stalls is one of the ways to reduce the amount of impervious surfaces associated with the project and increase green space. Even with these considerations, it is important to provide standard size parking stalls. Therefore, as a condition of approval, the applicant shall provide a minimum of 35 parking spaces (25 percent of proposed parking) that meet the minimum dimensions for standard spaces. In addition, all parking areas shall be marked as required by Section 541.320-Marking of Spaces and the compact parking stalls shall be labeled as required by Section 541.330-Size.

5. *The conditional use is consistent with the applicable policies of the comprehensive plan.*

Granting the conditional use permit for a planned unit development that would allow a 195-unit, 450-bedroom development near an activity center and transit station area would be consistent with the land use and housing policies listed in Finding 5 of the rezoning application above (Land Use Policy 1.8, 1.13 and 1.15, and Housing Policy 3.1, and 3.2). In addition, approval of the conditional

use would be consistent with the following land use, housing and urban design policies outlined in *The Minneapolis Plan for Sustainable Growth*:

Land Use Policy 1.1: Establish land use regulations to achieve the highest possible development standards, enhance the environment, protect public health, support a vital mix of land uses, and promote flexible approaches to carry out the comprehensive plan.

- 1.1.3 Encourage the use of flexible regulatory options that promote high quality development, such as the Planned Unit Development (PUD) tool.
- 1.1.4 Support context-sensitive regulations for development and land use, such as overlay districts, in order to promote additional land use objectives.
- 1.1.5 Ensure that land use regulations continue to promote development that is compatible with nearby properties, neighborhood character, and natural features; minimizes pedestrian and vehicular conflict; promotes street life and activity; reinforces public spaces; and visually enhances development.

Land Use Policy 1.3: Ensure that development plans incorporate appropriate transportation access and facilities, particularly for bicycle, pedestrian, and transit.

- 1.3.1 Require safe, convenient, and direct pedestrian connections between principal building entrances and the public right-of-way in all new development and, where practical, in conjunction with renovation and expansion of existing buildings.
- 1.3.2 Ensure the provision of high quality transit, bicycle, and pedestrian access to and within designated land use features.

Housing Policy 3.6: Foster complete communities by preserving and increasing high quality housing opportunities suitable for all ages and household types.

- 3.6.2 Promote housing development in all communities that meets the needs of households of different sizes and income levels.

Urban Design Policy 10.6: New multi-family development or renovation should be designed in terms of traditional urban building form with pedestrian scale design features at the street level.

- 10.6.1 Design buildings to fulfill light, privacy, and view requirements for the subject building as well as for adjacent properties by building within required setbacks.
- 10.6.3 Provide appropriate physical transition and separation using green space, setbacks or orientation, stepped down height, or ornamental fencing to improve the compatibility between higher density and lower density residential uses.
- 10.6.4 Orient buildings and building entrances to the street with pedestrian amenities like wider sidewalks and green spaces.
- 10.6.5 Street-level building walls should include an adequate distribution of windows and architectural features in order to create visual interest at the pedestrian level.
- 10.6.6 Integrate transit facilities and bicycle parking amenities into the site design.

6. *The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.*

If the requested land use applications are approved, the proposal would comply with all provisions of the R5/Multiple Family District and the UA/University Area Overlay District.

Findings Required for Planned Unit Developments:

- A. The planned unit development conforms to the applicable standards for alternatives and amenities. All planned unit developments shall provide at least one amenity or a combination of amenities that total at least 10 points, beyond those required for any alternatives. For each alternative requested, amenities shall total at least five points.

Alternatives requested:

Number of principal residential structures.

No alternative requested. When Phase I and Phase II are completed the project will be one building.

Bulk regulations, floor area ratio (FAR).

Alternative requested. The base FAR in the R5 District is 2.0. Since all parking will be enclosed the project qualifies for a 20 percent FAR bonus. Therefore, the allowed FAR is 2.4. In addition, the applicant is requesting an additional 20 percent bonus as an alternative to the PUD. With a successful acceptance of the alternatives, the maximum FAR for the project is 2.8. The project would have a FAR of 2.5 (220,560 square feet of floor area on an 88,452 square foot lot).

Bulk regulations, building height.

Alternative requested. The R5 zoning district allows a building to be four stories in height not to exceed 56 feet. The applicant is proposing a six story building that would be 64.5 feet in height.

In addition to the conditional use permit standards, the Planning Commission shall consider, but not be limited to, the following factors when determining the maximum height:

1. Access to light and air of surrounding properties.

Allowing the building two additional stories and 9.5 feet of greater height would not have an effect on the access to light and air of surrounding properties. The building would be meeting its minimum side yard setback requirements of 15 feet and would be located at least 40 feet from the nearest commercial building (University KinderCare at 525 Huron Boulevard Southeast) and at least 70 feet away from a building with residential units (University Commons at 609 Huron Boulevard Southeast).

2. Shadowing of residential properties, significant public spaces, or existing solar energy systems.

Allowing the building the additional stories and height would not have an adverse impact on the shadowing of residential properties. As previously mentioned, the proposed building would be more than 70 feet from the nearest residential building (University Commons); this distance substantially reduces the shadowing impact on residential buildings. The submitted shadow studies show that the only impact on residential buildings would be in the mornings. If the development was required to stay within the zoning district requirements of four stories or 56 feet in height, the development would have a similar shadowing impact on University Commons.

The proposed development would not impact light access of a significant public space (e.g. park or plaza). The nearest public space, Luxton Park to the southeast of the project site, is located more than 200 feet away. In addition, the development would not impact light access of a nearby solar energy system. The nearest solar array is at 114 Arthur Avenue SE, which is approximately 740 feet away 0.14 (BIRE 3045241).

3. The scale and character of surrounding uses.

The proposed development would fit in with the scale and character of the new taller development that is encouraged by the small area plan. Neighboring buildings include the six-story, 74 foot-tall

Solhaus Tower (515 Huron Boulevard SE), the 10-story Stadium View apartment building (2508 Delaware Street Southeast), and the six-story Solhaus apartment building (2428 Delaware Street SE) located to the northwest. The building would be taller than the smaller multiple family residential development to the north, but those buildings are older and anticipated/encouraged for redevelopment by the University's master plan and the Stadium Village Small Area Plan.

4. *Preservation of views of landmark buildings, significant open spaces or water bodies.*

The project would not have an impact of views of the nearest landmark building (Fire Station #19 to the northwest at University Avenue and Oak Street), significant open spaces (Luxton Park to the southeast), nor the nearest water body (Mississippi River to the southeast).

Lot area requirements.

No alternative requested.

Required yards.

No alternative requested. However, a variance application to reduce the front yard setback has been submitted.

Building placement in PO Pedestrian Oriented Overlay District.

Not applicable

On-premise signs.

No alternative requested.

Off-street parking and loading.

An alternative has been requested to reduce the minimum number of off-street parking spaces from 0.5 spaces per bedroom (225 off-street vehicle spaces) to 0.31 spaces per bedroom (140 off-street vehicle spaces). As outlined in Finding 3 above, the location of the project site and proposals by the applicant to encourage alternative modes of transportation would minimize traffic congestion in the public streets.

Points required for alternatives:

- Establishment of the PUD – **10 points.**
- Bulk regulations, FAR – **5 points.**
- Bulk regulations, Height – **5 points.**
- Off-street parking – **0 points.**

Total = 20 points.

Phasing plan.

Although the documents show a two-phased development, the applicant states in their PUD narrative that their intent is to not stop construction of the project until fully completed.

Phase I (construction would begin in spring 2016 and be completed summer of 2017) includes the southern building segment, the entire underground parking structure, the private courtyard, public plaza, and enhanced landscaping. Phase II (construction would begin in spring 2017 and be completed summer of 2018) includes the northern building segment and pedestrian/bike trail, and the recycling/storage room.

Phase	Approval Date	Alternative	Amenity
Phase I	January 6, 2015	<ul style="list-style-type: none"> • Bulk, Height • Parking 	<ul style="list-style-type: none"> • Underground parking • Plaza • Enhanced landscaping • Recycling storage area
Phase II	January 6, 2015	<ul style="list-style-type: none"> • Bulk, FAR 	<ul style="list-style-type: none"> • Pedestrian improvements

Amenities provided:

The applicant is proposing the following amenities from Table 527-1, Amenities to meet the required points for authorized alternatives.:

Plaza (5 points) Points for this amenity are awarded when a minimum area equivalent to ten (10) percent of the site not occupied by buildings, but not less than two thousand (2,000) square feet comply with all provisions in Chapter 535, Regulations of General Applicability. The project site is 88,452 square feet and the proposed building has a footprint of 55,351 square feet. The remaining lot area is 33,101 square feet. Therefore, a plaza that is required to be 3,310 square feet is order to receive points. The project would have a plaza that is approximately 4,100 square feet (12 percent). The plaza would comply with the provisions of Chapter 535, Article XI-Plazas (see plaza review in site plan section below).

Reflective roof (3 points) Points for this amenity are awarded for a building that utilizes roofing materials for seventy-five (75) percent or more of the total roof surface having a Solar Reflectance Index (SRI) equal to or greater than the values as required by the US Green Building Council (USGBC) for low-sloped roofs.

As stated in the applicant’s PUD narrative, the roof would be covered with a white membrane that meets or exceeds the Solar Reflectance Index (SRI) required by the US Green Building Council.

Enhanced exterior lighting (1 point) Points for this amenity are awarded for a lighting plan that highlights significant areas of the site or architectural features of the building(s), subject to the standards of Chapter 535, Regulations of General Applicability.

The applicant is proposing to install enhanced lighting. This would include regularly spaced bollards or wall lights for lighting the bike path, low-wall lighting in the plazas along the pathway, lighting to highlight landscape features, as well as illumination of the eastern building façade through various lighting methods.

Water feature (1 point) Points for this amenity are awarded for providing a drinking fountain where it is highly visible to and useable by the public.

The applicant is proposing to install a drinking fountain near the rear building entrance along the bike path for public use.

Enhanced landscaping (1 point) Points for this amenity are awarded for a landscaping plan of exceptional design that has a variety of native tree, shrub, and plant types that provide seasonal interest and that exceed the requirements of Chapter 530, Site Plan Review. The landscaped areas shall have a resource efficient irrigation system. The landscaping plan shall be prepared by a licensed landscape architect.

The project would have an enhanced landscape plan of exceptional design with a focus on greening the front of the property along 27th Avenue Southeast. Native plantings include River Birch trees and Little Goldstar Rudbeckia perennials. The proposed plantings will provide seasonal interest with the varied bloom times. The landscaping plan would exceed the requirements of Chapter 530, Site Plan Review in terms of square footage of landscape area, as well as tree and shrub plantings. In addition, the applicant

is proposing a number of bee-friendly plantings that would support the Minneapolis City Council pollinator-friendly resolution.

Recycling storage area (1 point) Points for this amenity are awarded for provision of an easily accessible area that serves the entire building and is dedicated to the collection and storage of non-hazardous materials for recycling, including but not limited to paper, corrugated cardboard, glass, plastics and metals. The recycling storage area shall be located entirely below grade or entirely enclosed within the building.

A recycling storage area is proposed on the first floor at the far northern end of the building. To qualify for this point and to ensure that the recycling area is easily and conveniently accessed by all residents, CPED is recommending the applicant move the recycling storage area to a place that is near equal distance between the units at the northern and southern end of the building or add a second recycling storage area to provide an easily accessible area for all tenants.

Amenities proposed by the applicant or others (8 points)

The city planning commission may consider other amenities not listed in Table 527-1, Amenities that are proportionally related to the alternative requested. The commission may assign one (1), three (3), five (5), or ten (10) points based on the proportionality.

Pedestrian/bike path (5 points) The applicant has proposed to have a 10-foot wide pedestrian/bike path at the rear of the building that would connect 27th Avenue Southeast to Essex Street Southeast via an adjacent property to the northwest; 10 feet is the minimum width that Public Works recommends for a pedestrian/bike path. This path is the northern portion of the proposed Prospect Park Trail; a trail called out by the Minneapolis Bicycle Master Plan. If fully implemented, this trail would provide non-motorized route access from Essex Street to the Midtown Greenway. The landscaping and building design/placement would create a safe, clear, and aesthetically pleasing pedestrian/bike path. As a condition of approval, the pedestrian/bike path shall be maintained for year-round access (including snow and ice removal) and be open to the public. In addition, a lighting plan shall be submitted prior to building permit issuance to show that the trail will be adequately lighted and in compliance with Section 535.590-Lighting standards.

Outdoor courtyard (3 points) The applicant has proposed a second outdoor space to the north of the plaza that is approximately 3,800 square feet. This outdoor space is a more secluded area reserved for residents; however the landscaped areas will be well visible for those traveling along 27th Avenue. Even though this space is not intended to be a plaza and open to the public, the applicant is proposing to have a high-quality outdoor space for community gathering space that will meet the development standards for a plaza outlined in Section 535.810 (with the exception of access). In addition, the enhanced landscaping will help the small area plan’s objective for this area in terms of creating a green corridor along 27th Avenue, framing the edges of the roadway, strengthening connections to the adjacent neighborhoods, and improving the character of public realm with streetscape enhancements.

Table I. Amenity Points Summary

Amenity	Points Requested by Applicant	Points Recommended by Staff
Plaza	5 points	5 points
Reflective roof	3 points	3 points
Enhanced exterior lighting	1 point	1 point
Water feature	1 point	1 point

Enhanced landscaping	1 point	1 point
Recycling storage area	1 point	1 point
Amenities proposed by the applicant-pedestrian/bike path	5 points	5 points
Amenities proposed by the applicant-outdoor courtyard	3 points	3 points
Total	20 points	20 points

In addition, the project is proposing to have underground parking. However, as outlined in Section 527.120, in no case shall any item be counted as an amenity for an alternative if it is utilized to qualify for a density bonus, which is the case with the proposed development.

B. The planned unit development conforms to the required findings for a planned unit development:

1. *That the planned unit development complies with all of the requirements and the intent and purpose of this chapter. In making such determination, the following shall be given primary consideration:*

a) *The character of the uses in the proposed planned unit development, including in the case of residential uses, the variety of housing types and their relationship to other site elements and to surrounding development.*

The PUD would have a variety of housing types. As proposed it will have 8 studios, 34 one-bedrooms, 81 two-bedrooms, 42 three-bedrooms, and 30 four-bedrooms. In addition, the development was designed for the housing to have a relationship to other site elements and to surrounding development. This includes providing greenspace/landscape area along 27th Avenue Southeast, which is encouraged by the Stadium Village Small Area Plan, having residential units open up to the outdoor plaza to help activate the space, providing balconies on the front and back of the building to help with natural surveillance, and varying the building height to provide greater visual interest for those living nearby.

b) *The traffic generation characteristics of the proposed planned unit development in relation to street capacity, provision of vehicle access, parking and loading areas, pedestrian access, bicycle facilities and availability of transit alternatives.*

The use is not expected to contribute to traffic congestion in the public streets. The applicant is proposing less off-street parking than required by the University Area Overlay District, however, the development team has done a number of steps to encourage and promote alternative modes of transportation including: complying with the required number of bike parking spaces, constructing a bike/walk path behind the building that is called out by the Minneapolis Bicycle Master Plan, installing a bike room on the main floor that would allow for onsite bicycle repair, and installing an on-screen trip display that would display real time transit information. In addition, the project location will be able to take advantage of numerous alternative modes of transportation including Metro Transit light rail and bus service, car sharing, and other bicycle facilities (Nice Ride and dedicated bike lanes).

A Travel Demand Management Plan (TDMP) was submitted that evaluates traffic generated by the proposed use and potential for congestion in the adjacent public streets. Results of the operational analysis indicate that the study area roadways and intersections will continue to operate acceptably without improvements.

- c) *The site amenities of the proposed planned unit development, including the location and functions of open space, the preservation or restoration of the natural environment or historic features, sustainability and urban design.*

From Table 527-1 of the zoning code, site amenities proposed in this development include a plaza along 27th Avenue Southeast and a pedestrian/bike path along the rear portion of the building. The open spaces would be functional, designed to encourage active uses, and would be located in a manner that would make them visible for a large portion of the dwelling units within the development and from the public sidewalk.

The sustainability elements of the project include a commitment to encouraging alternative modes of transportation and a landscaping plan that is bee friendly. From an urban design element the project would also be a positive for the area with the building placement, large amount of glazing, active functions around the project site, and variations of building height to help break up the building.

- d) *The appearance and compatibility of individual buildings and parking areas in the proposed planned unit development to other site elements and to surrounding development, including but not limited to building scale and massing, microclimate effects of the development, and protection of views and corridors.*

The scale and massing of the building is compatible with other multifamily buildings in the immediate area and the adjacent hotel that is proposed to the northwest of the project site. Even though the proposed building is longer than buildings in the area, the applicant has effectively broken it up with the building siting and variations in height. It is not anticipated that the project would have an adverse impact on the microclimate. The project would also protect the rail corridor viewshed with the 15 foot building setback along the western property line.

- e) *An appropriate transition area shall be provided between the planned unit development and adjacent residential uses or residential zoning that considers landscaping, screening, access to light and air, building massing, and applicable policies of the comprehensive plan and adopted small area plans.*

There would be appropriate transitions between the PUD and the adjacent residential developments/residential zoning. The University Commons residential development is located to the west and there is residentially zoned property to the north. The proposed building would comply with the minimum 15 foot required side yard setbacks along for the entire project. This setback will help maintain access to light and air for existing and future residential development. In addition, the applicant is proposing to landscape along the perimeter of the project site. This landscaping would provide screening and a buffer between residential properties.

- f) *The relation of the proposed planned unit development to existing and proposed public facilities, including but not limited to provision for stormwater runoff and storage, and temporary and permanent erosion control.*

The Public Works Department reviewed how the project would impact public facilities, stormwater, and erosion control during the initial preliminary development review (see attached). The applicant would be required to work closely with the Public Works Department, the Plan Review Section of the Inspections Department and the various utility companies during the duration of the development should the plan be approved.

- g) *The consideration, where possible, of sustainable building practices during the construction phases and the use of deconstruction services and recycling of materials for the demolition phase.*

The development team is using sustainable building practices during the construction phases and the use of deconstruction services and recycling of materials for the demolition phase. The applicant states that they are allowing deconstruction/salvage/ scrap metal companies to take anything from the building before demo begins and the current property owner of 117 27th Avenue Southeast will be removing heavy timbers from the structure.

2. *That the planned unit development complies with all of the applicable requirements contained in Chapter 598, Land Subdivision Regulations.*

Please see the preliminary and final plat section of this staff report.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to reduce the established front yard setback requirement to 15 feet for the building, 5 feet for mechanical equipment, and 0 feet for the plaza based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The required front setback in the R5/Multiple Family District is 15 feet. Parcels in this residential district are also subject to the established front yard setback requirement outlined in provision 546.160 (b). A portion of the University Commons building at 609 Huron Boulevard Southeast is setback 50 feet from Essex Street Southeast. Therefore the front yard setback is an imaginary line between the University Commons residential building closest to 27th Avenue Southeast and the multiple family building at 133 27th Avenue Southeast to the north.

The applicant is proposing that the building along 27th Avenue Southeast be setback 15 feet from the front property line, mechanical equipment (utility boxes) be setback 5 feet from the front property line, and the plaza be built up to the front property line; the utility boxes and plaza are not permitted obstructions in the front yard setback.

Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The project site has a greater than typical front yard setback due to the neighboring University Commons residential development to the south being a through lot. Practical difficulties also exist with the subject property in terms of its triangular shape. The triangular shape creates unique conditions in terms of the placement of the building, mechanical equipment and the plaza.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The applicant is proposing to use the property in a reasonable manner that would be in keeping with the spirit and intent of the ordinance and the comprehensive plan. Yard controls are established to provide for orderly development and use of land and to minimize conflicts among land uses by regulating the dimensions and use of yard in order to provide adequate light, air, open space and separation of uses.

The placement of the building, mechanical equipment and plaza would be compatible with the placement of adjacent buildings. As previously mentioned, the applicant would be maintaining the 15 foot front yard setback for the proposed building. The mechanical equipment would be located five feet from the front property line; however, it is proposed to be well distanced from the property to the north (52 feet) and south (106 feet). Even though the plaza would be located up to the front property line, it is well landscaped to help provide a green transition to 27th Avenue Southeast.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting of the variance would not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. In addition, granting of the variance would not be detrimental to the health, safety, or welfare of the general public or those utilizing the property or nearby properties. The placement of the building closer to the front property line is consistent with other existing buildings along the western side of 27th Avenue Southeast. The placement of the plaza at the property line would help activate and green this portion of 27th Avenue Southeast which is encouraged by the *Stadium Village University Avenue Station Area Plan*. The placement of the utility boxes five feet from the property line is not ideal; however, the proposed landscape screening and installing an artistic wrap/paint on the utility box would minimize the visual impact from the public sidewalk and street.

VARIANCE

The Department of Community Planning and Economic Development has analyzed the application for a variance to increase the amount of impervious surface allowed on the site from 85 percent to 86 percent, based on the following findings:

1. *Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone.*

The maximum impervious surface allowance for a site in the R5/Multiple Family Residential District is 85 percent. The proposed amount of impervious surface is 86 percent. Practical difficulties exist in complying with the ordinance because of circumstances unique to the property. The unique circumstances were not created by persons presently having an interest in the property and are not based on economic considerations alone. The unique shape of the lot and the absence of an alley create unique conditions in terms of building placement, vehicle access, and fire code requirements (i.e. access turn around requirements for emergency vehicles). The project site has two access points to the garage. The northern access point doubles as a fire access turn around. The driveway that leads to the fire access turn around area adds an additional 6,400 square feet of impervious surface to the site and increases the impervious surface from 80 percent to 86 percent.

2. *The property owner or authorized applicant proposes to use the property in a reasonable manner that will be in keeping with the spirit and intent of the ordinance and the comprehensive plan.*

The applicant is proposing to use the property in a reasonable manner that would be in keeping with the spirit and intent of the ordinance and the comprehensive plan. The intent of the impervious surface maximum is to help projects blend in better with surrounding residential character and to encourage onsite retention of stormwater.

The applicant has placed landscaping along the perimeter of the project site to help the project blend in better with surrounding residential development (existing and future). The combination of the landscape buffer and a proposed underground infiltration system helps the project meet City of Minneapolis storm water management requirements.

In addition, the proposal meets the spirit of the comprehensive plan. The applicant is proposing an extension of the Prospect Park Trail in the rear portion of the site, which is called out as a future trail in the Minneapolis Bicycle Master Plan (2011). This 10-foot wide trail adds 8,200 square feet of

impervious surface area and increases the impervious surface from 79 percent to 86 percent. However, when built, it would promote alternative modes of transportation within the area.

3. *The proposed variance will not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. If granted, the proposed variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.*

Granting of the variance would not alter the essential character of the locality or be injurious to the use or enjoyment of other property in the vicinity. In addition, granting of the variance would not be detrimental to the health, safety, or welfare of the general public or those utilizing the property or nearby properties. The applicant's proposal only exceeds the maximum impervious allowance by 1 percent (1,280 square feet). The proposed landscaping along most of the perimeter of the project site would help reduce the amount of stormwater leaving the site.

SITE PLAN REVIEW

The Department of Community Planning and Economic Development has analyzed the application based on the required [findings](#) and [applicable standards](#) in the site plan review chapter:

I. Conformance to all applicable standards of Chapter 530, Site Plan Review.

BUILDING PLACEMENT AND DESIGN

Building placement – Meets requirements

- The proposed placement of the building along 27th Avenue Southeast reinforces the street wall; portions of the building would be located up to front yard setback requirement (15 feet). The building placement would also maximize natural surveillance and visibility with fenestration levels that exceed zoning code requirements. Finally, the building placement and other site features would facilitate pedestrian access and circulation. Building entrances would face 27th Avenue Southeast and all building entrances would have protected walkways or delineated treatments (e.g. change in surfacing material) leading from building doors to the public sidewalk or pedestrian pathway at the back of the building.
- The project is subject to an established front yard setback and requires a front yard setback variance since the building would be located in front of the established front yard setback.
- The area between the building and the front lot line would include amenities including a plaza, interior courtyard, and enhanced landscaping.
- Onsite accessory parking would be located within the principal building.

Principal entrances – Meets requirements

- The building is oriented to have the principal entrance face 27th Avenue Southeast.
- The principal entrance is clearly defined and emphasized through the use of large amount of glazing and a stairway that leads directly from 27th Avenue to the building entrance.

Visual interest – Requires alternative compliance

- Building walls would provide some architectural detail to provide visual interest including breaking up the large building into separate modules through building placement and changing of materials, having varying floor heights for different modules, including balconies that are at least five feet in depth on the front and back of the building. However, given the height, size and length of the building additional details should be provided to further the visual interest of the building. Alternative compliance is required.

- Alternative compliance is required.
- There are blank, uninterrupted walls exceeding 25 feet in length on the 1st floor of the north elevation and the portions of the sixth floor (mezzanine level). Alternative compliance is required.

Exterior materials – Meets requirements

- The proposed exterior materials are durable. The applicant is proposing three exterior materials excluding windows/glazing: brick, fiber cement panels that are a minimum of 5/8 inches thick, and fiber cement panels that are less than 5/8 inches thick.
- The exterior materials comply with the percentages of allowed durable materials per elevation as specified by the [Guide to Exterior Building Walls and Materials](#) (see Table 1).
- The exterior materials and appearance of the rear and side walls of any building would be similar to and compatible with the front of the building. The rear and side walls have a similar percentage of exterior materials as the front elevation (east). The applicant has also provided projecting balconies on the sides and rear elevations in a similar fashion to the front elevation.
- The project does not include plain face concrete block as an exterior material.

Table 2. Percentage of Exterior Materials per Elevation

Material	Allowed max %	Northeast	East	North	West
Brick (face and panel)	75%	15.3%	33.6%	61.3%	44.4%
Fiber cement panels - A minimum of 5/8 thick	75%	50.1%	19.6%	23.2%	25.6%
Fiber cement panels - Less than 5/8 inch thick	75%	9.1%	8.6%	N/A	5.9%
Glass/windows	100%	25.4%	38.2%	15.5%	24.1%

Windows – Meets requirements

- The building contains windows as required by this section in order to create visual interest and to increase the security of adjacent outdoor spaces by maximizing natural surveillance and visibility. Twenty (20) percent of the walls on the first floor and ten (10) percent of the walls on each floor above the first that face a public street, public sidewalk, public pathway, or onsite parking lot, shall be windows. The east elevation is the only portion of the building that is subject to meet this requirement; as proposed it would meet the window percentage requirement (see Table 2). These windows would also meet the design requirements as they are vertical in proportion and are distributed in a more or less even manner.

Table 3. Percentage of Windows Required for Elevations Facing a Public Street, Sidewalk, Pathway, or Onsite Parking

	Code Requirement		Proposed	
Residential Uses				
1st Floor	20% minimum	635 sq. ft.	34%	1,068 sq. ft.
2nd -5 th Floors	10% minimum	397 sq. ft.	23%	917 sq. ft.

6 th Floor	10% minimum	207 sq. ft.	21%	440 sq. ft.
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Ground floor active functions – Meets requirements

- The entire first floor is designed to accommodate active functions. No portion of the first floor would have parking, loading, storage, or mechanical equipment rooms facing a public street.

Roof line – Meets requirements

- The project would have a flat roof which is common of surrounding buildings.

Parking garages – Meets requirements

- The project includes a parking garage in the lowest level. Most of the parking garage is below grade. Therefore, sloped floors would not dominate the appearance of the walls and the vehicles would be screened from view.

ACCESS AND CIRCULATION

Pedestrian access – Meets requirements with Conditions of Approval

- Clear and well-lighted walkways would connect building entrances to the adjacent public sidewalk and to any parking facilities located on the site. Such walkways shall be a minimum of four (4) feet in width. The main entrance would be connected with the public sidewalk via a hardscaped plaza. A walkway along the northern elevation would be shared space with the driveway, but delineated with a concrete material.

Transit access – Not applicable

- There are no transit shelters in front of the subject property.

Vehicular access – Meets requirements

- Vehicular access and circulation are designed to minimize conflicts with pedestrian traffic and with surrounding residential uses. Pedestrians would have protected walking areas and/or specially delineated surface material. The proposed project would also reduce the number of curb cuts on the project site from three to one.
- The project does not have alley access.
- Access for service vehicles is provided which does not conflict with pedestrian traffic. Service vehicles would enter the project site at the same access point as resident vehicle traffic.
- Even though the project exceeds their impervious surface calculation by one percent, the applicant has minimized the use of impervious surfaces. The combination of a bike/pedestrian path at the rear of the building and a second driveway area to meet fire access turn around requirements puts the project over the impervious surface allowance.

LANDSCAPING AND SCREENING

General landscaping and screening – Meets requirements with Conditions of Approval

- The composition and location of landscaped areas would complement the scale of development and surroundings. The project would include an enhanced landscaping area/plaza along 27th Avenue Southeast, which is encouraged by the *Stadium Village University Avenue Station Area Plan*. The project would also have landscaping along the perimeter of the project site to help blend in and provide a buffer between neighboring residential development (existing and future).

- The landscape plan in general provides well-placed contiguous planting areas including the large amount of landscaping along 27th Avenue.
- The project would comply with the requirement that not less than 20 percent of the site not occupied by buildings shall be landscaped. It would also comply with the canopy tree and shrub requirements (see Table 3).

Table 4. Landscaping and Screening Requirements

	Code Requirement	Proposed
Lot Area	--	88,452 sq. ft.
Building footprint	--	55,321 sq. ft.
Remaining Lot Area	--	33,101 sq. ft.
Landscaping Required	6,620 sq. ft.	11,998 sq. ft.
Canopy Trees (1: 500 sq. ft.)	13 trees	33 trees
Shrubs (1: 100 sq. ft.)	66 shrubs	117 shrubs

Parking and loading landscaping and screening – Meets requirements with Conditions of Approval

- The loading space is required to be screened from the western property line as it abuts a permitted residential use. The screening is required to be three feet in height and at least 95 percent opaque throughout the year. The applicant is proposing a dense planting of evergreen shrubs (Medora Junipers) that would meet this requirement.
- All other areas not governed by sections 530.160 and 530.170 and not occupied by buildings, parking and loading facilities or driveways, would be covered with turf grass, native grasses or other perennial flowering plants, vines, mulch, shrubs, trees or edible landscaping.
- As a condition of approval, the landscape plan shall comply with the requirements in Section 530.200-Plant Material Standards and Section 530.210-Installation and Maintenance of Materials.

ADDITIONAL STANDARDS

Concrete curbs and wheel stops – Meets requirements

- The driveway would be defined by six (6) inch by six (6) inch continuous concrete curb. Since the site is over an acre, the project site will have to meet Chapter 54 of the Minneapolis code ordinances to minimize the negative impacts of stormwater runoff. As outlined in the stormwater report, the applicant is proposing an underground infiltration system to help meet City of Minneapolis stormwater management requirements.

Site context – Meets requirements

- The site plan would not block views of important elements of the city such as parks and greenways, significant buildings and water bodies. None of these elements are within a 200 foot radius of the project site.
- The building would be located and arranged to minimize shadowing on public spaces and adjacent properties. The project site would be in compliance with the side yard setback requirements and the new structure would be more than 70 feet from the nearest residential building, the University Commons development to the west: a four building multiple family development at 609 Huron Boulevard Southeast.

- The six-floor, mid-rise building would have a minimal impact on the generation of wind currents at ground level.

Crime prevention through environmental design – Meets requirements

- The site plan would employ best practices to increase natural surveillance and visibility, to control and guide movement on the site, and to distinguish between public and non-public spaces. The site plan includes the following crime prevention design elements: a large amount of glazing on all elevations to help with natural surveillance and visibility and a retaining wall along the public street to help with territorial reinforcement, space delineation and natural access control.

Historic preservation – Meets requirements

- The project site does not include a locally designated historic structure or structure that has been determined to be eligible to be locally designated as a historic structure. A historic review letter was completed on October 5, 2015 stating that the building at 117 27th Avenue Southeast does not meet the Minneapolis Code of Ordinances definition of a historic resource.

2. Conformance with all applicable regulations of the zoning ordinance.

The proposed planned unit development is a *conditional* in the R5/Multiple-Family District.

Off-street Parking and Loading – Requires conditional use permit

- Table 541 requires one off-street parking space per dwelling unit. However, the project site is located in the University Area Overlay District. The UA Overlay District requires 0.5 parking spaces per bedroom. Therefore, the proposed 450 bedroom development would require 225 off-street parking spaces. The development is proposing 140 off-street parking stalls. An exception of the minimum parking requirements has been requested per the PUD application and that request has been evaluated above.
- There is no maximum parking requirement for dwellings except for parking that is not enclosed. All parking for the proposed development would be enclosed.
- The project would have a total of 450 bike parking spaces; 405 that meet the standards for long-term bicycle parking and 45 short-term bicycle parking spaces. This will meet the requirements outlined in Section 551.1330-Bicycle and Motorized Scooter Parking and Section 541.180-Bicycle Parking.
- As required by Table 541-9 Specific Off-Street Loading Requirement, a multiple family dwelling between 100 and 250 units is required to provide one small off-street loading space. The 195 residential unit development would provide one small off-street loading space in the northern part of the site. The proposed loading space complies with the location and size requirements outlined in Section 541.500-Loading Location and Section 541.560-Size.
- The driveway would be 22 feet wide at the property line, which is in compliance with the minimum (10 feet) and maximum (25 feet) allowance outlined in Section 541.240-Specific District Regulations for Access to Parking and Loading. The driveway would have an asphalt surface which is in compliance with the allowed surfacing materials in Section 541.300-Surfacing.
- The compact parking spaces (8'x15') and the 22-foot, two-way drive aisle complies with Section 541.330-Size.
- Snow will be removed from the project site.

Table 5. Vehicle Parking Requirements Per Use (Chapter 54I)

Use	Minimum	Reductions	Total with Reductions	Maximum Allowed	Proposed
Residential dwellings	225	N/A	225	N/A	140

Table 5. Bicycle Parking Requirements (Chapter 54I)

Use	Minimum	Short-Term	Long-Term	Proposed
Residential Dwellings	450	N/A	Not less than 90%/405	450 total 405 long term/45 short term

Table 6. Loading Requirements (Chapter 54I)

Use	Loading Requirement	Minimum Requirement	Proposed
Residential Dwellings	--	1 small space for residential development with 100 to 250 units	1 small

Building Bulk and Height – Requires conditional use permit

- The base FAR in the R5 District is 2.0. Since all parking would be enclosed the project qualifies for a 20 percent FAR bonus. Therefore, the allowed FAR is 2.4. In addition, the applicant is requesting an additional 20 percent bonus as an alternative to the PUD. With a successful acceptance of the alternatives, the maximum FAR for the project is 2.8. The project would have a FAR of 2.5 (220,560 square feet of floor area on an 88,452 square foot lot).
- The building height requires approval of the conditional use permit for the planned unit development. The flat roof building is 64.5 feet tall as defined by the Minneapolis zoning code, which exceeds the R5 District allowance of four stories or 56 feet, whichever is less. The natural grade 10 feet from the center of the building is 827.5 feet above sea level. The top of the first floor slab is 831 feet above sea level. The height of the building from the first floor slab to the roof is 61 feet (892 feet above sea level). In order for the lowest level (i.e. the parking garage) not to qualify as a story, the finished floor surface directly above the parking garage cannot be more than six (6) feet above grade, for more than fifty (50) percent of the total perimeter, or is more than twelve (12) feet above grade at any point. The proposal meets this requirement. At its most exposed point, the finished floor surface directly above the parking garage would be 5.4' above the typical grade on the western elevation.

Table 7. Building Bulk and Height Requirements

	Code Requirement	Proposed
Lot Area	--	88,452 sq. ft. / 2 acres
Gross Floor Area (GFA)	--	220,560 sq. ft.
Minimum Floor Area Ratio (GFA/Lot Area)	N/A	2.5
Maximum Floor Area Ratio (GFA/Lot Area)	2.8	2.5
Maximum Building Height	4 stories or 56 feet, whichever is less	6 stories and 64.5 ft.

Lot Requirements – Requires variance(s)

- The impervious surface allowance for a site in the R5/Multiple Family Residential District is 85 percent. The project would have an impervious surface of 86 percent. A variance is required and evaluated in the section above.

Table 8. Lot Requirements Summary

	Code Requirement	Proposed
Dwelling Units (DU)	--	195 DUs
Density (DU/acre)	N/A	96 DU/acre
Minimum Lot Area	Minimum of 5,000 square for a multiple family development Minimum of 1 acre for a planned unit development	2.03 acres
Maximum Impervious Surface Area	85%	86%
Maximum Lot Coverage	70%	63%
Minimum Lot Width	40 ft.	512 ft.

Yard Requirements – Requires variance(s)

- Section 546.560-Yard Requirements outlines the yard requirements for the project. The project has an established front yard setback because the property to the south is a through lot with a residential building approximately 50 feet from 27th Avenue Southeast. The project is proposing that the building, plaza, and transformers be located within the front yard setback. A variance is required and evaluated in the section above.
- As outlined in Table 546-14, the interior yard setbacks for the building are 5+2X with x being the number of stories above the first floor. The project is for a six story building. Therefore, the interior yard setbacks are 15 feet. The project would comply with this requirement (see Table 9).
- As outlined in Section 541.260-Driveways and parking areas on a zoning lot, uncovered driveways shall be a permitted obstruction in a required interior side yard, provided such driveway leads to a properly located parking area. The project includes a driveway that would be located along the northern interior property lines.

Table 9. Minimum Yard Requirements

	Zoning District	Overriding Regulations	Total Requirement	Proposed
Front (27th Avenue)	15 ft.	Established	50 ft. at its greatest point	15 ft.
Interior Side (North and Northeast)	15 ft.	--	15 ft.	A minimum of 16.5 ft.
Interior (West)	15 ft.	--	15 ft.	15 ft.

Signs – Not applicable

- There are no signs proposed at this time. All signs are subject to Chapter 543 of the Zoning Code. All new signs are required to meet the requirements in Chapter 543.

Screening of Mechanical Equipment – Meets requirements with Conditions of Approval

- The apartment units would have individual through-the-wall heating and cooling units. As a condition of approval and to be in compliance with Section 535.70-Screening of mechanical equipment, the heating, ventilation and air conditioning screens on the outside walls shall be matched to paint the surrounding wall finish to help reduce their visibility.
- The project would have two transformers along 27th Avenue Southeast; one would be located along the north and one along the south. The transformers would be screened with landscaping (Karl Foerester grass) and an artistic wrap or paint. CPED is recommending that the design of the artistic wrap or paint be reviewed and approved prior to building permit issuance.
- All other mechanical equipment is subject to the screening requirements of Chapter 535 and district requirements.

Refuse Screening – Meets requirements

- The project would be in compliance with Section 535.80-Screening of refuse and recycling storage containers as the refuse area would be within the building.

Lighting – Meets requirements with Conditions of Approval

- A lighting plan was not submitted with the application. As conditioned, a lighting plan shall be submitted prior to building permit issuance that shows the project is in compliance with the lighting requirements outlined in section 535.590-Lighting.

Fences and Retaining Walls – Meets requirements

- There are no fences as part of the project. The applicant is proposing retaining walls along 27th Avenue Southeast in the front yard setback that do not retain natural grade but would not be greater than two feet in height.

Regulations of General Applicability – Meets requirements

- **Unit size:** The project would be in compliance with Section 535.90-General standards for residential uses as all units would exceed the minimum 350 square foot gross floor area for an efficiency unit and 500 square foot floor area for a non-efficiency dwelling unit. Studios are proposed to be a minimum of 415 square feet and all other units would be a minimum of 560 square feet.

Plazas – Meets requirements

- The Minneapolis zoning code defines a plaza as an exterior open space designed for community gathering that is primarily hardscaped and accessible to the public, fronting along a public street or public sidewalk. The southern outdoor space along 27th Avenue Southeast was reviewed for compliance with the plaza standards as it was identified as a community gathering space and accessible to the public. The northern outdoor area was not considered a plaza as it is not intended to be accessible to the public. An analysis of the plaza standards is provided below:
 - **Size:** The plaza is approximately 4,100 square feet which exceeds the 2,000 square foot minimum square foot requirement outlined in Section 535.790.
 - **Placement:** The placement of plaza would not detract from and shall enhance natural surveillance and visibility of adjacent spaces and public sidewalks and facilitate pedestrian access and circulation. Multiple entries (including the main building entrance for pedestrians) and abundant windows would be oriented to the plaza to enhance natural surveillance, visibility and usability.
 - **Access:** The plaza is designed to allow pedestrian and bicycle access through and around the plaza. The plaza contains multiple access points from the public sidewalk. In addition, the plaza will be designed in accordance with the accessibility requirements of the Americans with Disabilities Act, and unobstructed walkways a minimum of four (4) feet in width would connect the plaza to the building. The plaza would not be adjacent to a transit stop.
 - **Natural surveillance and visibility:** The plaza is designed to promote natural surveillance with a large amount of windows and numerous building entrances leading directly to the plaza. In addition, the plaza is designed for people to observe adjacent spaces and the public sidewalks.
 - **Aerial obstructions:** The standards that apply to aerial obstructions require that plazas are unobstructed to the sky except for seating, arbors, trellises, kiosks, lighting, water features, public art and landscaping. Up to 30 percent of the plaza area may include umbrellas, awnings or an arcade. None of the plaza would have aerial obstructions.
 - **Surface materials:** The plaza would be surfaced with colored concrete.
 - **Seating:** The plaza complies with the seating requirements (see Table 10 below).

Table 10. Plaza Seating Requirements

	Code Requirement	Proposed
Plaza Area	--	4,100 sq. ft.
Overall seating (1:50 sq. ft.)	82 linear feet	168 linear feet
Fixed seating (20% of required)	16 linear feet	138 linear feet
Seating with backs (20% of required)	16 linear feet	30 linear feet
Seating types	2 types	2 types Seat wall planters and benches

- *Encroachments and setbacks:* The proposed plaza does not encroach into the public right-of-way.
- *Plantings:* Plazas shall provide a minimum of one (1) tree for each one thousand (1,000) square feet of plaza area. However, a landscaped area equivalent to ten (10) percent of the total plaza area may be provided in lieu of trees. The applicant is compliance with this requirement as they would have 27 percent of the plaza landscaped. The plaza would also include three trees.
- *Additional amenities:* The plaza is required to provide one additional amenity since it is less than 5,000 square feet. The applicant is proposing to provide moveable chairs equivalent to 25 percent of the minimum seating requirement. As outlined in the zoning code, the moveable chairs may be removed during the nighttime hours of 8:00 p.m. to 7:00 a.m.
- *Lighting:* As a condition of approval, a lighting plan shall be submitted prior to building permit issuance showing compliance with Chapter 535-Regulations of General Applicability.
- *Winter use:* The plaza is designed for winter use and relates to the built form with consideration given to elements such as providing shelter from winds, utilizing seasonally appropriate materials, maximizing access to sunlight and providing for snow and ice removal. The plaza would be located in an area that would be sheltered from north and westerly winds. The openness of the plaza to the east would maximize access to the sun in the morning and afternoon hours.
- *Trash receptacles:* A minimum of 1 trash receptacle and 1 recycling receptacle are required to be provided for each two thousand (2,000) square feet of plaza area. The project would be in compliance with this requirement as it would have 2 trash receptacles and 2 recycling receptacles.
- *Mechanical equipment:* The transformers/utility boxes located adjacent to the plaza would be screened with an artistic wrap or paint and plantings.
- *Maintenance:* As a condition of approval, the plaza shall be maintained in good order by the property owner for the life of the plaza. Proper maintenance shall include, but not be limited to, snow and ice removal, annual maintenance of vegetation and green space and annual inspection and repair and/or replacement of furnishings. Minimum landscaping and seating requirements shall be maintained for the life of the plaza. All adjacent streets, sidewalks and pathways shall be inspected regularly for purposes of removing any litter found thereon.

Specific Development Standards – Not applicable

- Since the proposed project is within the UA Overlay District it is not subject to the development standards for a multiple family dwelling with five or more units.

University Area Overlay District Standards – Meets requirements

- The project is requesting alternative compliance as part of the planned unit development in terms of the number of off-street parking spaces required for a multiple family residential project in the University Area Overlay District (Section 551.1320-Off-Street Parking).
- The project would meet the minimum bicycle and motorized scooter parking as outlined in Section 551.1330.

3. Conformance with the applicable policies of *The Minneapolis Plan for Sustainable Growth*.

The Minneapolis Plan for Sustainable Growth identifies the site as Urban Neighborhood and Transitional Industrial on the future land use map. Approving the site plan for the proposed development would be consistent with the following urban design policies outlined in *The Minneapolis Plan for Sustainable Growth*:

Urban Design Policy 10.4: Support the development of residential dwellings that are of high quality design and compatible with surrounding development.

- 10.4.2 Promote the development of new housing that is compatible with existing development in the area and the best of the city's existing housing stock.

Urban Design Policy 10.5: Support the development of multi-family residential dwellings of appropriate form and scale.

- 10.5.2 Medium-scale, multi-family residential development is more appropriate along Commercial Corridors, Activity Centers, Transit Station Areas and Growth Centers outside of Downtown Minneapolis.

Urban Design Policy 10.6: New multi-family development or renovation should be designed in terms of traditional urban building form with pedestrian scale design features at the street level.

- 10.6.1 Design buildings to fulfill light, privacy, and view requirements for the subject building as well as for adjacent properties by building within required setbacks.
- 10.6.3 Provide appropriate physical transition and separation using green space, setbacks or orientation, stepped down height, or ornamental fencing to improve the compatibility between higher density and lower density residential uses.
- 10.6.4 Orient buildings and building entrances to the street with pedestrian amenities like wider sidewalks and green spaces.
- 10.6.5 Street-level building walls should include an adequate distribution of windows and architectural features in order to create visual interest at the pedestrian level.
- 10.6.6 Integrate transit facilities and bicycle parking amenities into the site design.

Urban Design Policy 10.8: Strengthen the character and desirability of the city's urban neighborhood residential areas while accommodating reinvestment through infill development.

- 10.8.1 Infill development shall reflect the setbacks, orientation, pattern, materials, height and scale of surrounding dwellings.
- 10.8.3 Building features of infill development, such as windows and doors, height of floors, and exposed basements, shall reflect the scale of surrounding dwellings.

Urban Design Policy 10.14: Encourage development that provides functional and attractive gathering spaces.

- 10.14.3 Encourage the creation of new parks and plazas.
- 10.14.6 Develop public plaza standards that give specific guidance on preferred design and maintenance of seating, lighting, landscaping and other amenities utilizing climate sensitive design principles.

Urban Design Policy 10.18: Reduce the visual impact of automobile parking facilities.

Urban Design Policy 10.19: Landscaping is encouraged in order to complement the scale of the site and its surroundings, enhance the built environment, create and define public and private spaces, buffer and screen, incorporate crime prevention principles, and provide shade, aesthetic appeal, and environmental benefits.

- 10.19.1 In general, larger, well-placed, contiguous planting areas that create and define public and private spaces shall be preferred to smaller, disconnected areas.
- 10.19.2 Plant and tree types should complement the surrounding area and should include a variety of species throughout the site that include seasonal interest. Species should be indigenous or proven adaptable to the local climate and should not be invasive on native species.
- 10.19.3 Landscaped areas should include plant and tree types that address ecological function, including the interception and filtration of stormwater, reduction of the urban heat island effect, and preservation and restoration of natural amenities.
- 10.19.4 Landscaped areas should be maintained in accordance with Crime Prevention Through Environmental Design (CPTED) principles, to allow views into and out of the site, to preserve view corridors and to maintain sight lines at vehicular and pedestrian intersections.
- 10.19.5 Landscaping plans should be designed to facilitate future maintenance including the consideration of irrigation systems, drought and salt-resistant species, ongoing performance of storm water treatment practices, snow storage, access to sun, proximity to buildings, paved surfaces and overhead utilities.

Urban Design Policy 10.22 Use Crime Prevention Through Environmental Design (CPTED) principles when designing all projects that impact the public realm, including open spaces and parks, on publicly owned and private land.

- 10.22.1 Integrate “eyes on the street” into building design through the use of windows to foster safer and more successful commercial areas in the city.
- 10.22.2 Orient new housing to the street to foster safe neighborhoods.
- 10.22.3 Design the site, landscaping, and buildings to promote natural observation and maximize the opportunities for people to observe adjacent spaces and public sidewalks.
- 10.22.5 Locate landscaping, sidewalks, lighting, fencing and building features to clearly guide pedestrian movement on or through the site and to control and restrict people to appropriate locations.
- 10.22.7 Locate entrances, exits, signs, fencing, landscaping, and lighting to distinguish between public and private areas, control access, and to guide people coming to and going from the site.

Urban Design Policy 10.23 Promote climate-sensitive design principles to make the winter environment safe, comfortable and enjoyable.

- 10.23.2 Locate pedestrian places on the sunny sides of streets and buildings to shelter from the wind and utilize the sun’s warmth.
- 10.23.4 Encourage snow removal and storage practices that promote pedestrian and bicycle activity and safety.

4. Conformance with applicable development plans or objectives adopted by the City Council.

The proposed project would be in compliance with two other plans adopted by the City Council. The project is consistent with the *Minneapolis Bicycle Master Plan* which calls for the Prospect Park Trail to run through the site along the rail corridor. The applicant is proposing a 10-foot wide path within the rail corridor portion of the property to comply with the plan's guidance. In the short term, the completion of this trail would create a shorter route for pedestrians and bikers traveling to and from the University of Minnesota to Southeast Minneapolis via from 27th Avenue Southeast. In the long term if the trail is fully completed it would allow people to directly bike or walk from the Midtown Greenway to Essex Street Southeast.

The project is also in compliance with The *Stadium Village University Avenue Station Area Plan*. This small area plan was divided into subdistricts. The project site is within the 27th Avenue Open Space District. The proposed development is in compliance with the subdistrict's primary objectives of creating a green corridor along 27th Avenue, framing the edges of the roadway, strengthening connections to the adjacent neighborhoods, defining opportunities for new open spaces, and improving the character of public realm with streetscape enhancements, stormwater management and architectural guidance.

5. Alternative compliance.

The Planning Commission or zoning administrator may approve alternatives to any site plan review requirement upon finding that the project meets one of three criteria required for alternative compliance. Alternative compliance is requested for the following requirements:

- **Building walls shall provide architectural detail to create visual interest.** As required by Section 530.120- building walls shall provide architectural detail in order to create visual interest. The proposed building does contain some architectural detail to provide visual interest including breaking up the large building into separate modules through building placement and changing of materials, having varying floor heights for different modules, including balconies that are at least five feet in depth on the front and back of the building. However, given the height, size and length of the building additional details should be provided to further the visual interest of the building. Alternative compliance is required. The intent of the architectural detail requirement is to improve the aesthetics of the building. In order to improve the building appearance, CPED is recommending that the applicant install a more pronounced cornice and to recess windows within the brick portions of the building for the first four floors. A more pronounced cornice would help greater define the upper portion of the building and give the building greater depth. Recessing the windows within the brick portions of the building would also give the building greater depth for a portion of the building.
- **Blank walls:** The first floor on the north elevation and portions of the 6th floor are proposed to have a blank, uninterrupted wall that does not include windows, entries, recesses, or projections for more than 25 feet. This is not in compliance with Section 530.120-Building design. Alternative compliance is required. The intent of this requirement is to increase safety by providing natural surveillance and enhance the visual interest of a building. CPED does not believe the current proposal meets the intent of this chapter and that strict adherence to the requirements is impractical. Therefore, CPED is recommending that the applicant shall install windows on the first floor on the north elevation and portions of the 6th floor to meet the requirement that there not be a blank wall for more than 25 feet.

PRELIMINARY AND FINAL PLAT

The Department of Community Planning and Economic Development has analyzed the application for a preliminary and final plat based on the following findings:

1. *The subdivision is in conformance with these land subdivision regulations, the applicable regulations of the zoning ordinance and policies of the comprehensive plan.*

The applicant is proposing to combine a platted lot with adjacent unplatted railroad land (2512 Essex Street Southeast and a portion of 1710 Franklin Avenue Southeast) to allow for a planned unit development. Public Works and the Attorney's office reviewed the preliminary and final plat and did not identify any issues.

The preliminary and final plat is in conformance with the design requirements of the land subdivision regulations. The project site is five sides. The preliminary and final plat would also conform with the applicable regulations of the zoning ordinance and the policies of the comprehensive plan as outlined in the land use applications above. Furthermore, with the adoption of the proposed rezoning, a lot with more than one zoning classification would not be created.

2. *The subdivision will not be injurious to the use and enjoyment of other property in the immediate vicinity, nor be detrimental to present and potential surrounding land uses, nor add substantially to congestion in the public streets.*

Approval of the subdivision would not be injurious to the use and enjoyment of other property in the immediate vicinity, nor be detrimental to present and potential surrounding land uses, nor add substantially to congestion in public streets as outlined in the land use applications above.

3. *All land intended for building sites can be used safely without endangering the residents or users of the subdivision or the surrounding area because of flooding, erosion, high water table, soil conditions, improper drainage, steep slopes, rock formations, utility easements or other hazard.*

The land intended for building can be used safely without endangering the residents or users of the subdivision or the surrounding area because of flooding, erosion, high water table, soil conditions, improper drainage, steep slopes, rock formations, utility easements or other hazard. Since the site is over an acre, the project site will have to meet Chapter 54 of the Minneapolis code ordinances to minimize the negative impacts of stormwater runoff. In addition, the applicant has provided a geotechnical report that outlines how the project would not adversely impact the high water table. The project is relatively flat and does not have a steep slope.

4. *The lot arrangement is such that there will be no foreseeable difficulties, for reasons of topography or other conditions, in securing building permits and in providing driveway access to buildings on such lots from an approved street. Each lot created through subdivision is suitable in its natural state for the proposed use with minimal alteration.*

The lot created by this application presents no foreseeable difficulties for this development. No significant alterations to the land appear necessary.

5. *The subdivision makes adequate provision for stormwater runoff, and temporary and permanent erosion control in accordance with the rules, regulations and standards of the city engineer and the requirements of these land subdivision regulations. To the extent practicable, the amount of stormwater runoff from the site after development will not exceed the amount occurring prior to development.*

The Surface Water and Sewers Division of the Public Works Department will review and approve the drainage and sanitary system plans for this development prior to building permits being issued.

FOR REZONINGS ONLY**ZONING PLATE NUMBER. 22****LEGAL DESCRIPTION.**

Parcel 2: That part of the 100 foot wide Soo Line Railroad Company right of way formerly known as the Chicago, Milwaukee and Saint Paul Railway Company right of way across the Northeast 1/4 and the Southwest 1/4 of the Southeast 1/4 of the Northwest 1/4 of Section 30, Township 29 North, Range 23 West lying Northwesterly of the centerline of 27th Avenue SE and lying Southeasterly of the following described parcel:

That part of the 100 foot wide Soo Line Railroad Right-of-Way, formerly known and platted as the Chicago Milwaukee and St. Paul Railway (C.M. and St.P.) in the Northwest Quarter of Section 30, Township 29 North, Range 23 West of the 4th Principal Meridian, Hennepin County, Minnesota described as follows:

Commencing at the northwest corner of Lot 1, Block 32, "Regents Addition" according to the recorded plat thereof, said Hennepin County; thence South 63 degrees 51 minutes 54 seconds East, assumed bearing along the southwesterly line of Essex Street Southeast as taken or opened by the City of Minneapolis, a distance of 246.25 feet of the most northerly corner of Lot 1, Moores Rearrangement of Block 33, Regents' Addition to Minneapolis, according to the recorded plat thereof, said Hennepin County; thence North 63 degrees 51 minutes 54 seconds West along said southwesterly line a distance of 157.38 feet to the westerly line of said railroad right-of-way; thence South 24 degrees 24 minutes 57 seconds East along said westerly line a distance of 206.34 feet to the point of beginning of the land to be described; thence North 65 degrees 35 minutes 03 seconds East a distance of 100.00 feet to the easterly line of said railroad right-of-way; thence South 24 degrees 24 minutes 57 seconds East along said easterly line a distance of 100.00 feet; thence South 65 degrees 35 minutes 03 seconds West a distance of 100.00 feet to said westerly line; thence North 24 degrees 24 minutes 57 seconds West along said westerly line a distance of 100.00 feet to said point of beginning, according to the United States Government Survey thereof and situated in Hennepin County, Minnesota. (Abstract Property)

RECOMMENDATIONS

The Department of Community Planning and Economic Development recommends that the City Planning Commission adopt staff findings for the applications by Elsey Partners, LLC for the properties located at 117 27th Avenue Southeast and on adjacent former railroad land (2512 Essex Street Southeast and a portion of 1710 Franklin Avenue Southeast).

A. Rezoning the former railroad land at 2512 Essex Street Southeast and a portion of 1710 Franklin Avenue Southeast from the II/Light Industrial District to the R5/Multiple-Family District and the removal of the IL/Industrial Living Overlay District.

Recommended motion: **Approve** the application for rezoning the property from the II/Light Industrial District to the R5/Multiple-Family District and the removal of the IL/Industrial Living Overlay District.

B. Conditional Use Permit for a Planned Unit Development.

Recommended motion: **Approve** the conditional use permit for a planned unit development with 195 dwelling units, subject to the following conditions:

1. The conditional use permit shall be recorded with Hennepin County as required by Minn. Stat. 462.3595, subd. 4 before building permits may be issued or before the use or activity requiring a conditional use permit may commence. Unless extended by the zoning administrator, the conditional use permit shall expire if it is not recorded within two years of approval.
2. As required by Section 527.120 of the zoning code, the development shall comply with the standards for some combination of the following amenities from Table 527-1, Amenities totaling a minimum of 20 points: Plaza, reflective roof, enhanced exterior lighting, water feature, enhanced landscaping, recycling storage area, pedestrian/bike trail and an outdoor courtyard. The pedestrian/bike trail shall be maintained for year-round access (including snow and ice removal) and open to the public. In addition, a lighting plan shall be submitted prior to building permit issuance to show that the trail will be adequately lighted and in compliance with Section 535.590-Lighting standards. Furthermore, the recycling storage area shall be moved to a location near equal distance between the units at the northern and southern end of the building or a second recycling storage area shall be installed that provides an easily accessible recycling area for all tenants.
3. A minimum of 35 parking spaces shall be provided that meet the minimum dimensions for standard spaces. In addition, all parking areas shall be marked as required by Section 541.320-Marking of Spaces and the compact parking stalls shall be labeled as required by Section 541.330-Size.
4. The applicant shall implement all amenities as required by Section 527.120 of the zoning code by January 11, 2018.

C. Variance to reduce the front yard setback.

Recommended motion: **Approve** the variance application to reduce the established front yard setback requirement to 15 feet for the proposed building, 5 feet for mechanical equipment, and 0 feet for a plaza, subject to the following condition:

1. The transformers shall be artistically wrapped or painted. The design of the wrap or paint shall be reviewed and approved by CPED.

D. Variance to increase the impervious surface.

Recommended motion: **Approve** the variance application to increase the amount of impervious surface allowed on the project site from 85 percent to 86 percent.

E. Site Plan Review for a new commercial building.

Recommended motion: **Approve** the site plan review for the construction of a six-story, residential building with 195 residential units and 450 bedrooms, subject to the following conditions:

1. As required by Section 530.120- building walls and as a form of alternative compliance, the applicant shall install a more pronounced cornice and recess windows within the brick portions of the building on the first four floors. In addition, the applicant shall install windows on the first floor on the north elevation and portions of the 6th floor to meet the requirement that there not be a blank wall for more than 25 feet.
2. As a condition of approval, the landscape plan shall comply with the requirements in Section 530.200-Plant Material Standards and Section 530.210-Installation and Maintenance of Materials.
3. As required by Section 535.70-Screening of mechanical equipment, the heating, ventilation and air conditioning screens on the outside walls shall be painted to match the surrounding

- wall finish to help reduce their visibility. In addition, CPED shall review the design of the artistic wrap or paint proposed for the utility boxes prior to building permit issuance.
4. As conditioned, a lighting plan shall be submitted prior to building permit issuance that shows the project and plaza is in compliance with the lighting requirements outlined in section 535.590-Lighting.
 5. As required by Section 535.810-Development standards for all permitted plazas, the plaza shall be maintained in good order by the property owner for the life of the plaza. Proper maintenance shall include, but not be limited to, snow and ice removal, annual maintenance of vegetation and green space and annual inspection and repair and/or replacement of furnishings. Minimum landscaping and seating requirements shall be maintained for the life of the plaza. All adjacent streets, sidewalks and pathways shall be inspected regularly for purposes of removing any litter found thereon.

F. Preliminary and Final Plat.

Recommended motion: **Approve** the preliminary and final plat.

ATTACHMENTS

1. Zoning map and zoning comparison
2. Project overview and narrative
3. PDR report
4. Planned unit development information
5. Historic review letter
6. Public comments
7. Renderings
8. Aerials and images
9. Preliminary and final plat
10. Civil plan set-survey, site, grading, landscape, stormwater
11. Plan set-floor plans, building elevations, section
12. Shadow study



	Multiple Family District (High Density)	Light Industrial District
FAR		
Base FAR Maximum	2.00	2.70
20% bonus for enclosed, underground or structured parking	0.40	n/a
20% bonus for 50% ground floor commercial		n/a
20% bonus for 20% affordable units	0.40	n/a
Total possible FAR	2.80	2.70
Required lot area per dwelling unit (sq. ft.)	n/a	n/a
Possible DU Bonuses:		
20% bonus for enclosed, underground or structured parking	Yes	n/a
20% bonus for 50% ground floor commercial	No	n/a
20% bonus for 20% affordable units	Yes	n/a
Base building height maximum (in stories)	4	4
Maximum size of retail store (sq. ft.)	0	n/a
Maximum Lot Coverage	70%	n/a
Maximum Impervious Surface Coverage	85%	n/a
Yard Requirements		
Front	15	0
Interior side or rear ¹	5	5
Corner Side	8	0
Height Requirements		
Feet	56	56
Stories	4	4
Retail		
Maximum size of retail store (sq. ft.)-base	0	n/a
Bonus for no parking b/w structure and street	0	
Bonus for additional stories & parking	0	

PRIME PLACE



Elsy Partners is a vertically integrated multifamily development company that designs, builds and manages its own projects in urban infill locations. The development team started in 2005 and has developed over 2000 beds that encourage a pedestrian environment. The multifamily project at 117 27th Ave. Minneapolis, MN has an ideal location being close to the University of Minnesota/Medical Center, Metro Line, and Downtown. The 5 story project consists of 195 units with 42-One Bedrooms, 81-Two Bedrooms, 42-Three Bedrooms, and 30-Four Bedrooms. There will also be an underground climate controlled parking garage that will have 142 parking spaces and 450 bike parking stalls. The property will have on site management, workout facility, on site management, in unit washer/dryer, granite countertops and landscaped courtyards. The property is requesting rezoning of the abandoned SOO Line rail spur from Industrial to R-5 Residential and will be proceeding through the PUD planning process. Please contact me, Chris Elsey, the owner if you have any further questions.

Regards,

Chris Elsey

1532 College Ave. F19

Manhattan, KS 66502

785-317-5265

chris@myprimeplace.com

Date: November 8, 2015
To: CPED, Minneapolis
From: Chris Elsey - Elsey Partners
Re: GLU Narrative for 117 27th Ave SE Apartment Project

To whom it may concern,

To help evaluate our development proposal, we have prepared the following document in response to the requirements of the General Land Use Application. We continue to be excited about the future of this project and are grateful for your feedback and consideration.

Conditional Use Permit

The establishment, maintenance or operation of the conditional use will not be detrimental to or endanger the public health, safety, comfort or general welfare.

The conditional use will not be injurious to the use and enjoyment of other property in the vicinity and will not impede the normal and orderly development and improvement of surrounding property for uses permitted in the district.

Yes. The project is a high-density residential building located in R-5 zoning. The conditional use is required for a Planned Unit Development. It will not be detrimental to the public health, safety, comfort, or general welfare. It will not be injurious to the use of nearby property, nor will it impede normal development of surrounding property.

Adequate utilities, access roads, drainage, necessary facilities or other measures, have been or will be provided.

We are investigating these items and each will be adequately provided.

Adequate measures have been or will be taken to minimize traffic congestion in the public streets.

A TDMP has been completed, finding that adjacent streets are operating at demands much lower than their capacities. The report concludes that the proposed project will have minimal impact on the roadway system. The project also supports the use of public transportation and encourages bicycle use, with 450 bicycle stalls and a 10' bike trail along the west edge of the site linking 27th Ave SE to Essex St towards downtown and the University of Minnesota campus.

The conditional use is consistent with the applicable policies of the comprehensive plan.

The proposed project is in line with the city's comprehensive plan. According to the Stadium Village University Avenue Station Area Plan, the site is located in the 27th Avenue Open Space District and is identified for high density residential development. The project also helps to establish the "missing link" portion of the Grand Rounds with enhanced streetscape along 27th Avenue.

The conditional use shall, in all other respects, conform to the applicable regulations of the district in which it is located.

Yes. The proposed project requires a Planned Unit Development in order to allow for adequate construction phasing, increasing the maximum height of the structure, and for an increase in FAR. In all other respects, the project conforms to the regulations and goals of R-5 zoning.

Increasing Maximum Height

The proposed project is a five story apartment building with a mezzanine level. The total building height is 69 ft, and the project is of a similar scale to other recent and proposed developments to the northwest. The increased height aligns with the city's development goals for higher density uses in this area.

The project is designed in response to concerns by PPA and the COW to allow for greater transparency through the site. The 2 story "link" building connecting the two 5-story structures allows for movement of air through the private courtyard, and the entire building is offset back from 27th Ave SE to minimize shadow impact on the street and property to the east. It is worth noting that the townhomes to the east of the site are sited on a hill significantly higher in elevation than the project site, and with several large trees along 27th Ave SE, and will receive some shadows from the building only in the late evenings.

The building does not block any existing views of significant buildings or spaces. Please see the attached shadow study for building shadowing plans at various times of the year.

Variance

The proposed project is a high-density, 5 story, residential building appropriate to its location along the west edge of 27th Ave SE just south of Essex street. Local planning documents identify this area for higher density residential development, including the Stadium Village University Avenue Station Area Plan. The proposed project requests variances for the maximum Floor Area Ratio (FAR) in order to support more fully these density goals, and to bring the project in line with other recent developments built and proposed to the northwest. In all other manners, the use of the property will be consistent with the R-5 zoning.

The proposed character of the project is in line with the goals of the 27th Ave Open Space District and will enhance a site that is currently without identity and context. The FAR variance will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.

Additional narrative in response to CPED feedback received 11/13/15:

Front Yard Setback Variance

According to the findings of CPED, the proposed project requires a variance for the front yard setback because the neighboring property to the south and west (609 Huron Blvd SE) is set back from 27th Ave SE beyond the required 15' per R-5 zoning. According to 546.160, the required front yard setback shall not be less than a line joining the front of the buildings on both sides of the lot.

The neighboring property (609 Huron Blvd SE) is an irregularly-shaped through lot, with a private drive connecting 27th Ave SE and Huron Blvd SE to provide vehicular access to several apartment buildings on the property. Since the shape of the lot tapers toward the 27th Ave SE frontage, the resulting building closest to 27th Ave SE is pushed back well beyond the required 15' setback (an estimated 63') to allow for vehicular access into and through the site. There is no structure on the adjacent property to the north.

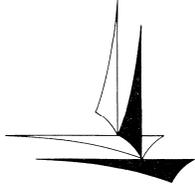
If the front yard setback for the proposed project site were to be enforced, the allowable buildable area of the site would be reduced an additional 30% beyond the setbacks typically enforced for the R5 zoning area. The increased setback would also push the building further from the street, in conflict with the goals of the 27th Ave Open Space District to create an urban streetscape. It seems that the intent of this ordinance is to maintain a uniform front yard along a residential frontage. However, in light of the unique conditions of the project site and the lot shapes of the neighboring properties, the ordinance places a hardship on the project that makes compliance difficult and does not appear to achieve the desired effect of the adopted ordinance.

The proposed project requests a variance to the provision for required front yard increase as specified in 546.160. A variance is also requested for portions of the development within the front yard setback including the plaza and transformer placement. The entirety of the building envelope complies with the 15' front yard setback for R-5 zoning.

Variance to the Maximum Allowable Impervious Area

The proposed project also requests a variance for maximum impervious surface coverage as specified in 546.150. Complying with the provision that no more than 85% of the site area be impervious is difficult with the inclusion of the requested 10' bike path along the western and northern property boundaries. Excluding this improvement, the project impervious area is 78.9%, well below the 85% maximum. However, with the bike trail included the proposed impervious area increases to 88.2% which slightly exceeds the allowable impervious area. Given that the transportation master plan had indicated a bike trail over the existing rail spur that is now a part of the project site, the applicants believe they have met the intent of the impervious area limitation for the private portion of the proposed project, while also helping the City of Minneapolis achieve the goals for alternative modes of transportation.

As previously stated, the proposed project will enhance the property and is in keeping with the spirit of the comprehensive plan. These variances for this front yard setback and impervious surface coverage requirements will not be detrimental to the health, safety, or welfare of the general public or of those utilizing the property or nearby properties.



Minneapolis Development Review
 250 South 4th Street
 Room 300
 Minneapolis, MN 55415

Preliminary Development Review Report

Development Coordinator Assigned: **DONALD ZART**
(612) 673-2726
don.zart@minneapolismn.gov

Status *
RESUBMISSION REQUIRED

Tracking Number:	PDR 1001377
Applicant:	ELSEY PARTNERS 1532 COLLEGE AVE F19 MANHATTAN, KS 66502
Site Address:	117 27TH AVE SE 117 27TH AVE SE 1710 FRANKLIN AVE SE
Date Submitted:	21-OCT-2015
Date Reviewed:	29-OCT-2015

Purpose

The purpose of the Preliminary Development Review (PDR) is to provide Customers with comments about their proposed development. City personnel, who specialize in various disciplines, review site plans to identify issues and provide feedback to the Customers to assist them in developing their final site plans.

The City of Minneapolis encourages the use of green building techniques. For additional information please check out our green building web page at: http://www.ci.minneapolis.mn.us/mdr/GreenBuildingOptions_home.asp.

DISCLAIMER: *The information in this review is based solely on the preliminary site plan submitted. The comments contained in this report are preliminary ONLY and are subject to modification.*

Project Scope

5 story multi-family apartment bldg wit basement level parking garage

Review Findings (by Discipline)

Fire Safety

- Provide required fire suppression system throughout building.
- Fire department connection must be located on the address side of building and within 150 feet of a fire hydrant.
- Provide required fire alarm system throughout building.
- Provide and maintain fire apparatus access at all times.

*Approved: You may continue to the next phase of developing your project.

*Resubmission Required: You cannot move forward or obtain permits until your plans have been resubmitted and approved.

□ Zoning - Planning

- The following land use applications have been identified
 - Rezoning application: I1 to R5
 - Site plan review
 - Conditional use permit to allow a planned unit development
 - Variances
 - Variance for floor area
 - Variance rear yard setback of building
 - Preliminary and final plat
- Provided request for additional information to the architect for land use submittal on October 23, 2015. The following information will be required for site plan/landscape plan.
 - Will likely see the southern outdoor area as a plaza. Make sure that it meets the plaza requirements outlined in Chapter 535.
 - Have any transformers that are required on plan sheets.
 - Provide details of snow storage plan on site plan or in narrative if the plan is to remove snow from site.
 - Provide details/materials of retaining walls.
 - Check your site calculation of site area. Your landscape plan says the site is over 90,000 square feet. Everything else says the site is 88,500.
 - Landscape schedule/landscaping amounts: divide it up on-site and those off-site and confirm that you will meet minimum on-site landscaping requirements for landscape area, shrubs, and trees.
 - Impervious surface amount: if actually close to 85 percent make sure at less so you don't have to apply for a variance. Please make sure what you include for pervious percentage is actual green space; pervious avers do not count towards pervious calculation.

□ Addressing

- The proposed address for the apartment entrance will be 117 27th Ave SE.
- When assigning suite numbers to the interior dwelling spaces the following guidelines apply:
 - The first one to two digits of the suite sequence number will designate the floor number of the site.
 - The last two digits of the suite sequence number will designate the unique ID for the unit (condo, suite, unit, or apartment).
 - Suite sequence digit numbers will be assigned to dwelling, commercial and retail units, not common areas. For example, laundry rooms, saunas, workout rooms, etc., would not be assigned numbers.
 - Please provide each condo, suite, unit or apartment number
- This building is also considered to have a parking ramp per MCO Chapter 108. As such, within 5 years of the date of the certificate of occupancy being issued, the parking ramp will be required to have annual inspections and apply for a Ramp Operating Certificate.

□ Street Design

- References to City Standards on Sheet C2.0 shall be specific and refer to the appropriate details elsewhere in the plans.
- All driveway aprons shall be designed and constructed to City standards. All driveway aprons shall be shown graphically correct on all related plan sheets. The Minneapolis Standard driveway apron provides for 5' curb radii; the current plan shows curb cut radii of 15' which exceeds the standard and shall be revised appropriately.
- All curb & gutter in the Public right-of-way shall be designed and constructed to City standards, curb & gutter to be City standard B624 Curb and Gutter.

❑ Parks - Forestry

- Contact Craig Pinkalla (cpinkalla@minneapolisparcs.org), Telephone (612)-499-9233 regarding removal or protection of trees during construction in the city right of way.
- Skyline Honey Locust on 27th Ave - does not fit diversity guidelines. Ideal would be 3 genera. Require at least 2.
- Boulevard appears to be 3'-4' wide. Would limit species selection to small trees only. Examples highlighted in orange
- Wider Blvd. required for large growing species like Honey Locust
- See attached list showing species distribution for the neighborhood.
- Effective January 1, 2014, the City of Minneapolis and the Minneapolis Park and Recreation Board adopted an update to the existing Parkland Dedication Ordinance.
- The adopted City of Minneapolis Parkland Dedication ordinance is located in Section 598.340 of the City's Land Subdivision ordinance:
- <http://library.municode.com/index.aspx?clientId=11490>
- As adopted, the fee in lieu of dedication for new residential units is \$1,521 per unit (affordable units excluded per ordinance) and for commercial and industrial development it is \$202.80 per development employee (as defined in ordinance). Any dedication fee (if required) must be paid at the time of building permit issuance. There is also an administration fee that is 5% of the calculated park dedication fee.
- As proposed, for the proposed apartment building, the calculated dedication fee is as follows:
- Residential - 196 x \$1521 = \$257,049
- Administrative Fee - 5% of Fee (capped at \$1,000) = \$ 1,000
- Total = \$257,049
- This is a preliminary calculation based on your current proposal; a final calculation will be made at the time of building permit submittal.
- For further information, please contact Don Zart at (612)673-2726.

❑ Right of Way

- An encroachment permit shall be required for all streetscape elements in the Public right-of-way such as: plants & shrubs, planters, tree grates and other landscaping elements, sidewalk furniture (including bike racks and bollards), and sidewalk elements other than standard concrete walkways such as pavers, stairs, raised landings, retaining walls, access ramps, and railings (NOTE: railings may not extend into the sidewalk pedestrian area). Please contact Bob Boblett at (612) 673-2428 for further information.
- Note to the Applicant: Any elements of an earth retention system and related operations (such as construction crane boom swings) that fall within the Public right-of-way will require an encroachment permit application. If there are to be any earth retention systems which will extend outside the property line of the development then a plan must be submitted showing details of the system. All such elements shall be removed from the Public right-of-way following construction with the exception of tie-backs which may remain but must be uncoupled and de-tensioned. Please contact Bob Boblett at (612) 673-2428 for further information.
- In addition, any elements of an earth retention system and related excavations that fall within the Public right-of-way will require a "Right-of-Way Excavation Permit". This permit is typically issued to the General Contractor just prior to the start of construction. However, it is the Applicant's responsibility to insure that all required permits have been acquired by its consultants, contractors, sub-contractor's prior to the start of work.

❑ Business Licensing

- The project as proposed does not require a business license.

❑ Construction Code Services

- Exterior "ramps" shall not exceed 5% slope.
- Floors 3-5 of the north portion require 2 means of egress at the completion of phase I.

□ Sidewalk

- The proposed ADA ramp at the south tip of the development along 27th Ave. S.E. is not necessary; sidewalk shall be shown to match existing at this location.
- All proposed trees in the Public right-of-way are subject to the review and approval of the Minneapolis Park Board. Please contact Craig Pinkalla at (612) 499-9233 to discuss tree species selection, planting method, spacing and locations. Tree planting details shall be included in the plans. For all trees proposed in "hardscape environments" within the Public right-of-way, the Applicant shall provide engineered/structured soil in the form of a tree trench or tree pit for all proposed street trees. Sidewalk layouts and landscaping in the Public right-of-way shall follow established design standards; refer to the following: City of Minneapolis Urban Forest Policy (http://www.minneapolismn.gov/www/groups/public/@cped/documents/webcontent/convert_282934.pdf)

□ Traffic and Parking

- The nature of the proposed development is such that traffic impacts will be an issue; please contact Allan Klugman at (612) 673-2743 to discuss the requirements of a Travel Demand Management Plan (TDMP).
- Please contact Bill Prince (612) 673-3901 regarding the street lighting policy as your development will be required to adhere to the street light policy regarding pole type and spacing.
- Note to the Applicant: The curb lane at this location is a designated bike lane and a loading zone cannot be approved. Remove the proposed loading zone from the plans and all references to parking spaces and striping.
- Current ordinance states that all maneuvers associated with loading, parking or sanitation pick up for a private development shall occur on private property. Please provide a narrative explaining the trash removal operations and show turning maneuvers for all truck type vehicles that will be using the site and loading/parking entrance areas. A Solid Waste Collection Point (SWCP) shall be identified on the site plans; per City Ordinance (Section 2, Ch. 225, Article V - 225.750. Solid waste collection point (SWCP):
- In regards to the proposed bike trail along the west side of the proposed site:
 - The MN State Aid minimum for a shared use trail is 8' with a 2' clear zone on either side of the trail surface, or 12' total. This is an absolute minimum with the assumption that the trail will carry low volumes of bicyclists and pedestrians.
 - In Minneapolis, bicycle and pedestrian traffic is moderate to high on most segments of the trail network. This is especially true near the University of Minnesota campus. There should be the expectation that the trail shown in the plans will carry moderate to high volumes of bicyclists and pedestrians. As such, it is recommended that the trail be designed with a minimum trail surface of 10', or preferably 12' per the City of Minneapolis Bicycle Facility Design Guide (p. 43).
 - The wider trail surface will help mitigate bicyclist-pedestrian conflicts on the trail, especially at access points. Two pedestrian access points are shown on the plan: 1) stairway connection to the development and 2) the assumed connection to the adjacent property to the west.
- Please contact Simon Blenski at (612) 673-5012 for questions and further information.

□ Water

- All existing and proposed underground Public Utilities (water, sanitary sewer, and storm drain) shall be shown on the site plan with corresponding pipe sizes and types. For Public watermain infrastructure records call (612) 673-2865. Any existing connections not in use shall be noted on the plans for removal, and shall be removed per the requirements of the Utility Connections Department, call (612) 673-2451 for more information.
- The water service connections to 27th Ave. S.E. should be designed to avoid bends; rather it is recommended to run water service lines straight from the main into the proposed building to the meter location. The City of Minneapolis Water Treatment and Distribution does not allow 8x3" taps the tap will need to be 8x4" and then reduced to a 3" line. Please contact Rock Rogers at (612) 673-2286, to review domestic and fire service design, connections, and sizes.

□ Environmental Health

- If impacted soil is encountered during site activities work will need to stop and notification provided to the MN State Duty officer at (615) 649-5451.
- If dewatering is required during site construction see below for city permit requirements. The scope of work calls for underground parking. The highest groundwater level expected for this site should be determined and used in establishing the lowest level for underground parking. The underground parking and other subgrade structures should be designed to prevent infiltration of groundwater without the need for a permanent dewatering system being installed. If a continuously operating permanent dewatering system is needed it must be approved as part of the sanitary sewer and storm drain site plan approval prior to construction beginning.
- No construction, demolition or commercial power maintenance equipment shall be operated within the city between the hours of 6:00 p.m. and 7:00 a.m. on weekdays or during any hours on Saturdays, Sundays and state and federal holidays, except under permit. Contact Environmental Services at 612-673-3867 for permit information.
- Permits and approval are required from Environmental Services for the following activities: Temporary storage of impacted soils on site prior to disposal or reuse; Reuse of impacted soils on site; Dewatering and discharge of accumulated storm water or ground water, underground or aboveground tank installation or removal, well construction or sealing. Contact Tom Frame at 612-673-5807 for permit applications and approvals.
- A review of the project, permits issued and an inspection from Environmental Service for identification of equipment and site operations that require annual registration with the City of Minneapolis will occur for this project.

□ Sewer Design

Stormwater Management:

- The project is subject to the stormwater treatment requirements of Chapter 54 of the Minneapolis Code of Ordinances. Please provide the necessary documentation and analysis demonstrating compliance with Chapter 54, including stormwater calculations, BMP maintenance plans, etc.
- Please provide copies of the Phase I and Phase II Environmental Site Assessments referenced in the geotechnical report.
- An operations and maintenance plan is required for the stormwater treatment devices. The O&M plan shall define the maintenance regimen, including type and interval of maintenance and party to conduct such maintenance. Please provide a copy of the O&M Plan.
- Please add the following note to the appropriate plan sheet: The Contractor, property owner or responsible party shall contact Minneapolis Surface Waters and Sewers 48 hours prior to any excavation or construction related to or in the location of the proposed Stormwater Management BMP (Contact Paul Chellsen, 673-2406 or paul.chellsen@minneapolismn.gov).
- Please add the following note to the appropriate plan sheet: Upon the project's completion the General Contractor, Property Owner or Responsible Party shall provide to the Department of Public Works a Final Stormwater Management Report including record drawings. This report will serve as a means of verification that the intent of the approved stormwater management design has been met. This final report shall substantiate that all aspects of the original design have been adequately provided for by the construction of the project.

Utility Connections:

- It is preferred that the sanitary sewer connection be made at the existing manhole just upstream of the current proposed connection location. Please evaluate if this is feasible, if not please contact Jeremy Strehlo, 612-673-3973, for further requirements for the proposed connection directly to the main.

Utility:

- MCES appears to have an interceptor crossing this property. The project is responsible for obtaining any approvals or permits through MCES that may be necessary.
- For comments or questions on Public Works Surface Water & Sewers Division related requirements please contact Jeremy Strehlo, (Professional Engineer) at (612) 673-3973, or jeremy.strehlo@minneapolismn.gov

□ Historical Preservation Committee

- The applicant applied for a historic review letter to review the historic significance of the building on the project site. On or about September 23, 2015, CPED determined that the building was not historic and that CPED will likely be able to administratively approve the demolition permit if submitted.

END OF REPORT

Date: November 8, 2015
To: CPED, Minneapolis
From: Chris Elsey - Elsey Partners
Re: PUD Narrative for 117 27th Ave SE Apartment Project

To Whom It May Concern:

To help evaluate our development proposal, we have prepared the following document in response to the requirements of the Planned Unit Development Application. We continue to be excited about the future of this project and are grateful for your feedback and consideration.

Phasing

The project is to be constructed in two phases as indicated by the site plan drawing submitted with this letter. Generally, the southernmost building, and the 2 story "link" building, will begin construction in spring of 2016 and completed summer of 2017. Included in phase 1 will be the construction of the entirety of the underground parking structure, the private courtyard, public plaza, and enhanced landscaping along 27th Ave SE.

Phase 2 will see the completion of the remainder of the northernmost building, and will include the 10' bike trail along the west side of the building (pedestrian improvement amenity), and the recycling storage room. Phase 2 would commence in the spring of 2017 and completed summer of 2018.

While this is considered phased construction, the intent is to never fully stop construction until the project is fully complete.

Proposed Amenities

The project proposes several amenities in response to the required points for the PUD, FAR increase, and building height increase. ~~All parking shall be underground (10 points). The concrete "podium" structure of the garage will form the base for the proposed courtyard and plaza spaces. The top of the podium will be less than 3' above adjacent grade.~~

The entry plaza formed by the "legs" of the southern "L"-shaped building will be a public plaza (5 points) meeting all requirements of Chapter 535. The plaza will be approximately 4,500 sf and will include planting areas that integrates with the enhanced landscaping (1 point) proposed along 27th Ave SE. The space will include fixed seating, short-term bike stalls, public drinking fountain, adequate trash and recycling bins, and a kiosk displaying public transportation information.

The outdoor courtyard, which is approximately 3800 sf and located north of the entry plaza, is located in the space between the two main building forms. This enhanced outdoor space will meet similar, but modified requirements of a public plaza (3 points). The space will provide an well landscaped exterior space with ample exterior seating, short-term bike stalls, trash and recycling, and a drinking fountain. Additionally, this space is located adjacent to the indoor exercise room and will provide an opportunity for building occupants to take their workout outdoors when weather permits.

Along the west edge of the property, a 10' bike trail will be installed, connecting 27th Ave SE to Essex street via an adjacent property. The bike path will include trees along the route, ample lighting, and balconies and openings in the facade will put "eyes on the street", making the path attractive and safe. The bike path and the improvements to the sidewalk along 27th Ave SE are pedestrian improvements (~~3 points~~) that support city planning goals of enlivening the 27th Ave Open Space District and filling in the "missing link" to the Grand Rounds. *The installation of the path is proposed at 5 points as it exceeds the general requirements of a pedestrian improvement. Additionally, the installation of a public drinking fountain near the building entrance adjacent to the bike path will meet the requirement of a water feature (1 point).*

Along the bike path and around the exterior building the installation of lighting will meet the requirements of enhanced exterior lighting (1 point). These lighting features will include regularly spaced bollard or wall lights for lighting the bike path, low wall lighting in the plazas along pathways, lighting to highlight landscape features, decorative pole and low wall lighting along 27th Ave., and illumination of the eastern building facade through wall various lighting methods.

In the northernmost portion of the building, a 1,100 sf dedicated trash and recycling room will be provided. This space will serve the entirety of the building and will allow for the collection and storage of a variety of non-hazardous materials. This amenity will meet the requirements of a recycling storage area (1 point). *Another building feature will be the utilization of a white roof membrane, like TPO, to meet the reflective roof requirement (3 points).*

PLANNED UNIT DEVELOPMENT APPLICATION WORKSHEETS

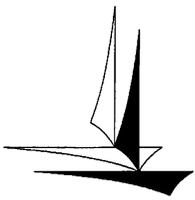
In the table below, identify the requested alternative(s) and determine the minimum points required.

AUTHORIZED ALTERNATIVES		MARK AN 'X' NEXT TO THE REQUESTED ALTERNATIVE(S)	POINTS REQUIRED FOR THE REQUESTED ALTERNATIVE
Number of principal residential structures.	To allow more than one (1) principal residential structure to be placed on one (1) platted or recorded lot.		0
Bulk regulations.	To increase the maximum gross floor area ratio by not more than twenty (20) percent.		5
	To increase the maximum height of structures.		
Lot area requirements.	Reductions in the area of individual lots within the planned unit development from the required lot area of the zoning district.		5
	A density bonus to increase the maximum number of dwelling units by not more than twenty (20) percent.		
Yards.	A reduction or elimination of required yards within the planned unit development.		5
	A reduction or elimination of required yards along the periphery of the planned unit development, except along property lines abutting a side or rear lot line of a residential district or property lines abutting a side or rear lot line of a structure used for permitted or conditional residential purposes.		
Building Placement.	Allow alternatives to building placement requirements in the PO Pedestrian Oriented Overlay District, except where to allow parking between the principal structure and the front or corner side lot line.		5
On-premise signs.	Sign standards.		5
Off-street parking and loading.	Minimum and maximum amount of required off-street parking and loading.		0
	Minimum amount of required bicycle parking.		
	Minimum width of parking aisles.		
	Minimum and maximum width of driveways.		
Amenity(ies) required by section 527.120(1)			10
Add points required for each requested alternative to determine total points required. Enter total at right in the space provided.			

In the table below, identify the proposed amenity(ies) and determine the amount of points proposed. An amenity(ies) used to qualify for a density bonus in any zoning district, a floor area ratio premium in the Downtown Districts, or any other amenity in Table 527-1 cannot be counted as an amenity for a requested alternative. No points are awarded for amenities that do not meet all of the standards required in Table 527-1, unless otherwise authorized by the city planning commission.

AMENITY	MARK AN 'X' NEXT TO THE PROPOSED AMENITY(IES)	POINTS
Active liner uses as part of a parking garage		10
Green roof		10
Historic preservation		10
Leadership in Energy and Environmental Design (LEED)		10
Minnesota Sustainable Building Guidelines (B3-MSBG)		10
Public right-of-way dedication		10
Underground parking		10
Conservation of the built environment		5
Garden(s) or on-site food production		5
On-site renewable energy		5
Outdoor open space		5
Outdoor children's play area		5
Plaza		5
Art feature		3
Decorative or pervious surface for on-site parking and loading areas, drives, driveways and walkways.		3
Energy efficiency		3
Living wall system		3
Natural features		3
Pedestrian improvements		3
Reflective Roof		3
Shared bicycles		3
Shared vehicles		3
Decorative fencing		1

Enhanced exterior lighting		1
Enhanced landscaping		1
Enhanced stormwater management		1
Heated drives or sidewalks		1
Pet exercise area		1
Recycling storage area		1
Tree islands		1
Water feature		1
Amenities proposed by the applicant or others:		1, 3, 5, or 10, as determined by CUP.
Amenities that significantly exceed standards:		Up to 5, as determined by CUP.
Add points for each proposed alternative to determine total points proposed. Enter total at right in the space provided.		



Minneapolis
City of Lakes

**Community Planning &
Economic Development**

105 5th Avenue S, Suite 200
Minneapolis MN 55401

Office 612-673-2597
Fax 612-673-2728
TTY 612-673-5154

October 5, 2015

Chris Elsey
Elsey Partners
1532 College Ave, F19
Manhattan, KS 66502

RE: Historic Review Letter: 117 27th Ave SE
PID: 3002923310006
BZH-28883

Mr. Elsey:

CPED staff understands that a wrecking permit will be required as a component of your land use applications to construct a development at 117 27th Ave SE (also addressed as 127 27th Ave SE and 2610 Essex St SE). This Historic Review Letter BZH-28883 provides background information as input into the City's process to evaluate the wrecking permit.

Section 599.460 of the City of Minneapolis Municipal Code states, "The planning director shall review all building permit applications that meet the definition for demolition for a demolition permit to determine whether the affected property is an historic resource. If the planning director determines that the property is not an historic resource, the demolition building permit shall be approved. If the planning director determines that the property is an historic resource, the building permit shall not be issued without review and approval by the commission following a public hearing as provided in section 599.170." Section 599.110 defines an historic resource as, "A property that is believed to have historical, cultural, architectural, archaeological or engineering significance and to meet at least one of the criteria for designation as a landmark or historic district as provided in this chapter."

The original structure at 117 27th Ave SE, an iron-clad industrial shop, was constructed in 1903. The original building permit is not on file, which is common for buildings of that era. As a result, no architect or builder is identified in the records. The building appears to have a simple vernacular style typical of the era, with minimal architectural detailing visible on the exterior. The

The original owner listed on the permit card was Ellis Woolman. Other records from that era identify him as a maker of railroad equipment, and note that he constructed a shop for rebuilding locomotives in Minneapolis at about the time this building was constructed. The railroad function would be logical for this site, as it is located directly on a rail spur (addressed at 117 ½ 27th Ave SE).

By 1926, the owner was listed as the Smith Beal Fire Brick Company – which appears to have eventually become the Smith Sharpe Fire Brick Company

(identified in the permit records for this property in 1947). The Smith Sharpe Fire Brick Company still owns this building, although their main location is now at 2129 Broadway St NE. Its official website states that it has been in business since 1924 in Minneapolis.

This property has had a number of major changes over the years. While the original structure was built in 1903, it has been added to several times. In 1912, a two-story brick store and flat building was added. In 1926, a frame warehouse was constructed, although it was identified as temporary. In 1947, a large concrete block addition was constructed. There were also repairs in 1966 after a fire, which included a new roof and new joists. Additional work to repair storm damage was done in 1981.

A portion of this property is addressed as 127 27th Ave SE (also listed as 2610 Essex St SE). This property has no structure on it, and is used as a surface parking lot. It appears that there was originally a house on that portion of the site, but it was demolished in 1964.

The property was not included in any recent historic resource survey, including the *Central Core Historic Survey* (2011), although it was within the study area. Furthermore, it was not among the properties resurveyed in this area through the *Historic Resources Inventory Capstone* (2013).

The property has not been designated as a Landmark or portion of a historic district by the City of Minneapolis. The buildings have also not been designated as part of a historic district, site, or place by the State of Minnesota. Furthermore, the buildings have not been listed in the National Register of Historic Places. Staff research indicates that the subject property has no particular associations with significant people, events, city/neighborhood identity, and landscape design which rise to the level of designation.

Based on the information currently available the property does not appear to meet the City of Minneapolis' thresholds for historical significance and therefore the subject property does not meet the Minneapolis Code or Ordinances definition of a historic resource.

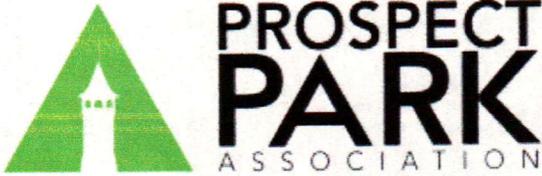
In light of additional information CPED reserves the right to reevaluate its determination in this letter.

Please let me know if you have any questions.

Sincerely,



Haila Maze, AICP
Principal City Planner
City of Minneapolis
Community Planning and Economic Development
(612) 673-2098
haila.maze@minneapolismn.gov



November 17th, 2015

City of Minneapolis-Community Planning and Economic Development
Attn: Aaron Hanauer, AICP, City Planner
250 South 4th Street, Room 300.
Minneapolis, MN 55415
(aaron.hanauer@minneapolis.gov)

Dear Mr. Hanauer:

On behalf of the Prospect Park Association (PPA) Zoning & Project Review Committee we are writing to express our support as presented to the committee for the multi-family development project planned by Prime Design as represented by Brad Buser, AIA for Elsey Partners, LLC.

Prime Design and representatives from Elsey Partners have met with representatives of the PPA Zoning & Project Review Committee on two occasions to review their project with us. In each instance they have responded to our comments by modifying their design. We feel that the modifications have improved the design.

Prospect Park also utilizes a memorandum titled, "University District Alliance Design Principles" when we work with developers. Typically we ask them to respond in writing to the "...Design Principles..." in an effort to gain their responses to neighborhood issues such as Urban Context, Sustainability, Architecture and Landscape Design to name a few. Prime Design responded to all of the five principles in language acceptable to members of the Zoning Committee who were in attendance.

As a result of the design of 127 27th Avenue SE and Prime Design's response to our "...Design Principles..." PPA Zoning & Project Review Committee with only a single no vote overwhelmingly approved their project. Furthermore, the committee is particularly supportive of the project's attention to two community goals for this area: 1) the streetscape on 27th Avenue SE that will complement the anticipated completion of the missing link in the Grand Rounds, and 2) the provision for a bike path on the adjacent abandoned railroad spur.

If you have any questions regarding this letter of support, please feel free to contact me at jonewix@aol.com, or 612-349-9353.

Sincerely,

A handwritten signature in black ink, appearing to read "J. Wicks".

John W. Wicks, AIA
PPA Zoning and Project Review Chairperson

cc. Brad Buser (brad@myprimedesign.com)



RENDERED PERSPECTIVE FROM INTERSECTION OF 27TH AND ESSEX LOOKING SOUTH

PRIME DESIGN

MULTI-FAMILY DEVELOPMENT PROPOSAL
117 27TH AVENUE SE MINNEAPOLIS, MN
ELSEY PARTNERS, LLC

A10

A
A10

RENDERED PERSPECTIVE
NOT TO SCALE



RENDERED PERSPECTIVE FROM 27TH NEAR THE SOUTH CORNER OF THE SITE LOOKING NORTH

PRIME DESIGN

MULTI-FAMILY DEVELOPMENT PROPOSAL

117 27TH AVENUE SE MINNEAPOLIS, MN

ELSEY PARTNERS, LLC

A11



RENDERED PERSPECTIVE

NOT TO SCALE



RENDERED PERSPECTIVE FROM NORTHEAST OF THE SITE LOOKING SOUTHWEST

PRIME DESIGN

MULTI-FAMILY DEVELOPMENT PROPOSAL
117 27TH AVENUE SE MINNEAPOLIS, MN
ELSEY PARTNERS, LLC

A12

A
A12 RENDERED PERSPECTIVE
NOT TO SCALE



AERIAL OBLIQUE VIEW FROM SOUTHWEST LOOKING NORTH EAST



AERIAL OBLIQUE VIEW FROM EAST LOOKING WEST

PRIME DESIGN

MULTI-FAMILY DEVELOPMENT PROPOSAL

117 27TH AVENUE SE MINNEAPOLIS, MN

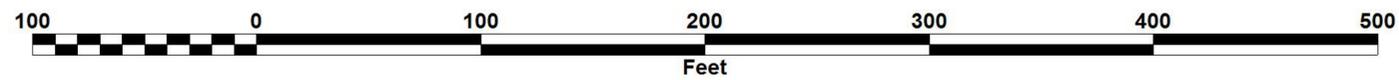
ELSEY PARTNERS, LLC

A14



AERIAL RENDERINGS WITH CONTEXT

NOT TO SCALE





VIEW FROM SOUTH ACROSS 27TH LOOKING NORTH



VIEW FROM NORTH ACROSS 27TH LOOKING SOUTHWEST



VIEW FROM SOUTH OF SITE LOOKING NORTH



ACROSS 27TH LOOKING NORTHWEST AT EXISTING BUILDING



VIEW OF SOUTH PORTION OF SITE FROM CENTER OF SITE



VIEW OF NORTH PORTION OF SITE FROM CENTER OF SITE



VIEW FROM NORTHEAST CORNER OF SITE LOOKING WEST



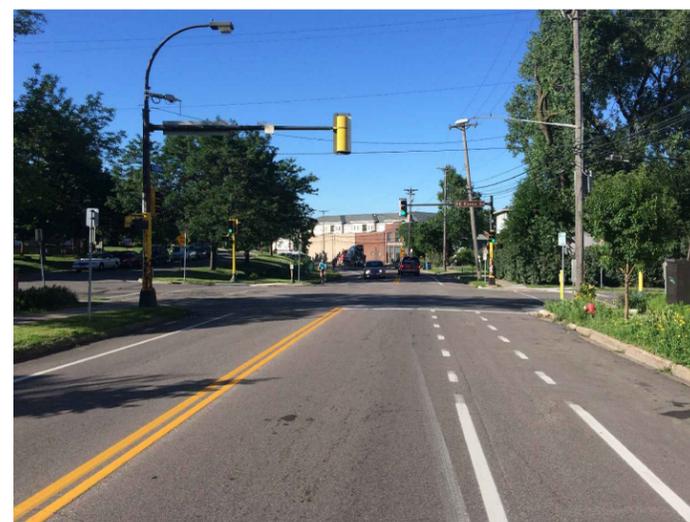
VIEW ACROSS 27TH LOOKING WEST AT EXISTING BUILDING



VIEW ALONG 27TH LOOKING NORTH



VIEW ALONG 27TH LOOKING SOUTH



VIEW FROM INTERSECTION OF 27TH AND ESSEX LOOKING SOUTH

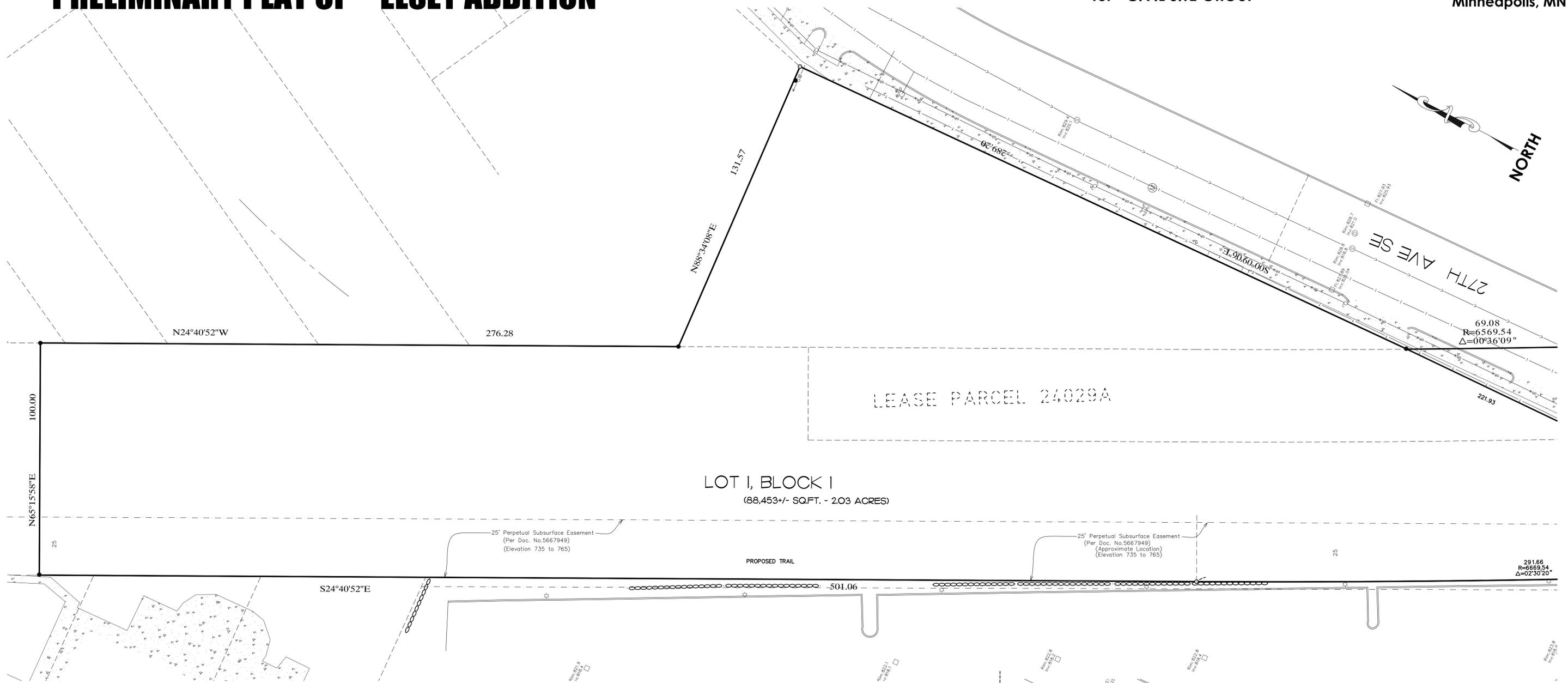


VIEW FROM SOUTH ALONG 27TH LOOKING NORTH

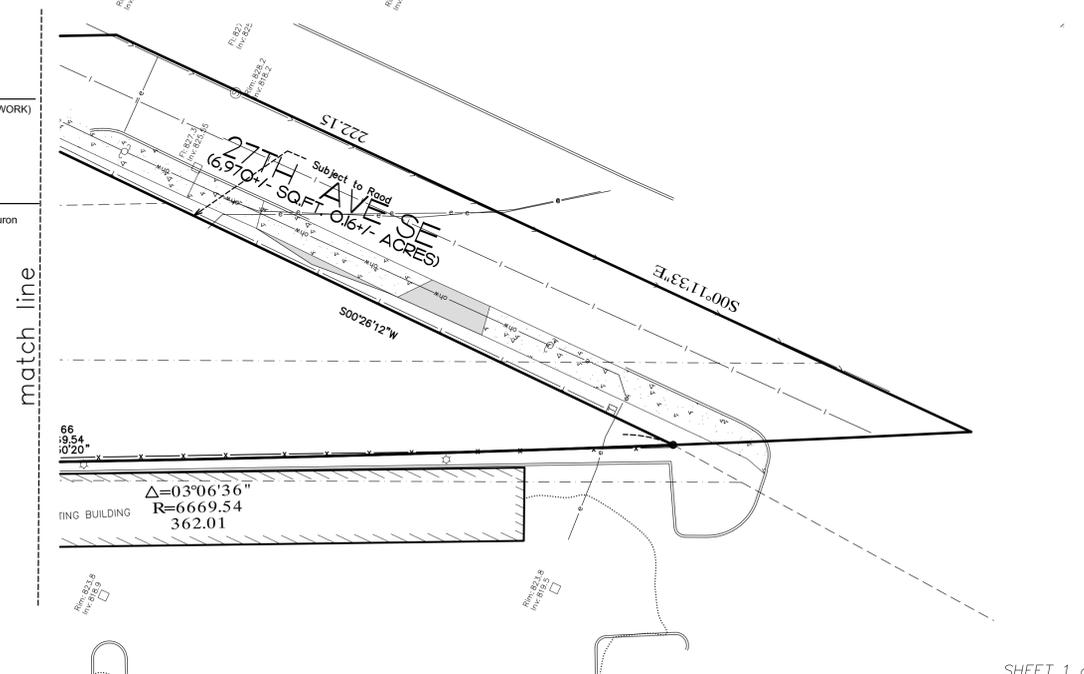
PRELIMINARY PLAT OF "ELSEY ADDITION"

~for~ CIVIL SITE GROUP

PROPERTY ADDRESS: #117 - 27th Ave SE.,
Minneapolis, MN



match line



match line

LEGEND

- DENOTES IRON MONUMENT FOUND
- DENOTES UTILITY POLE
- ☆ DENOTES STREET LIGHT
- ☐ DENOTES TELEPHONE BOX
- ⊙ DENOTES MISCELLANEOUS MANHOLE
- ⊕ DENOTES SANITARY SEWER MANHOLE
- ⊖ DENOTES STORM SEWER MANHOLE
- ⊗ DENOTES CATCH BASIN
- ⊘ DENOTES GATE VALVE
- ⊙ DENOTES FIRE HYDRANT
- ⊚ DENOTES WATER SHUTOFF
- ⊛ DENOTES EXISTING CONTOUR
- GW — DENOTES GUY WIRE
- OW — DENOTES OVERHEAD WIRE
- E — DENOTES UNDERGROUND ELECTRIC
- T — DENOTES UNDERGROUND TELEPHONE
- G — DENOTES UNDERGROUND GAS
- W — DENOTES WATERMAIN
- S — DENOTES SANITARY SEWER
- ST — DENOTES STORM SEWER
- F — DENOTES EXISTING FENCE
- ELEV — DENOTES EXISTING ELEVATION.
- ☐ DENOTES CONCRETE
- ▨ DENOTES BITUMINOUS

LEGAL DESCRIPTION:

Parcel 1:

Lots 1, 2, 3, 4, 5, 6, and 7, Block 4, Meecker Island and Power Company's Addition (Revised), according to the recorded plat thereof, Hennepin County, Minnesota.

(Abstract Property)

Parcel 2:

That part of the 100 foot wide Soo Line Railroad Company right of way formerly known as the Chicago, Milwaukee and Saint Paul Railway Company right of way across the Northeast 1/4 and the Southwest 1/4 of the Southeast 1/4 of the Northwest 1/4 of Section 30, Township 29 North, Range 23 West lying Northwesterly of the centerline of 27th Avenue SE and lying Southeasterly of the following described parcel:

That part of the 100 foot wide Soo Line Railroad Right-of-Way, formerly known and platted as the Chicago Milwaukee and St. Paul Railway (C.M. and St.P.) in the Northwest Quarter of Section 30, Township 29 North, Range 23 West of the 4th Principal Meridian, Hennepin County, Minnesota described as follows:

Commencing at the northwest corner of Lot 1, Block 32, "Regents Addition" according to the recorded plat thereof, said Hennepin County; thence South 63 degrees 51 minutes 54 seconds East, assumed bearing along the southwesterly line of Essex Street Southeast as taken or opened by the City of Minneapolis, a distance of 246.25 feet to the most northerly corner of Lot 1, Moores Rearrangement of Block 33, Regents' Addition to Minneapolis, according to the recorded plat thereof, said Hennepin County; thence North 63 degrees 51 minutes 54 seconds West along said southwesterly line a distance of 157.38 feet to the westerly line of said railroad right-of-way; thence South 24 degrees 24 minutes 57 seconds East along said westerly line a distance of 206.34 feet to the point of beginning of the land to be described; thence North 65 degrees 35 minutes 03 seconds East a distance of 100.00 feet to the easterly line of said railroad right-of-way; thence South 24 degrees 24 minutes 57 seconds East along said easterly line a distance of 100.00 feet; thence South 65 degrees 35 minutes 03 seconds West a distance of 100.00 feet to said westerly line; thence North 24 degrees 24 minutes 57 seconds West along said westerly line a distance of 100.00 feet to said point of beginning, according to the United States Government Survey thereof and situated in Hennepin County, Minnesota.

(Abstract Property)

GENERAL NOTES:

- BEARING SYSTEM IS BASED ON NAD83 (1996 ADJ)- (VIA REAL TIME GPS MEASUREMENTS UTILIZING MNDOT VRS NETWORK)
- SUBJECT PROPERTY ADDRESS IS 117 27TH AVE SE, MINNEAPOLIS MN 55414
- PROPERTY IDENTIFICATION # (P.I.D.) = 30-029-23-31-0006
- FIELD WORK COMPLETED 04/13/2015.

BENCHMARK

BASIS FOR ELEVATION: University of Minnesota 1912 USC&G Datum, Top Nut of Hydrant at the Southwest Quadrant of Huron Blvd SE and Essex Street SE. Elevation=829.55 (Not Shown)

DRAWN BY: MDN	
CHECK BY: ERV	
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NO.	DATE
	DESCRIPTION
	BY

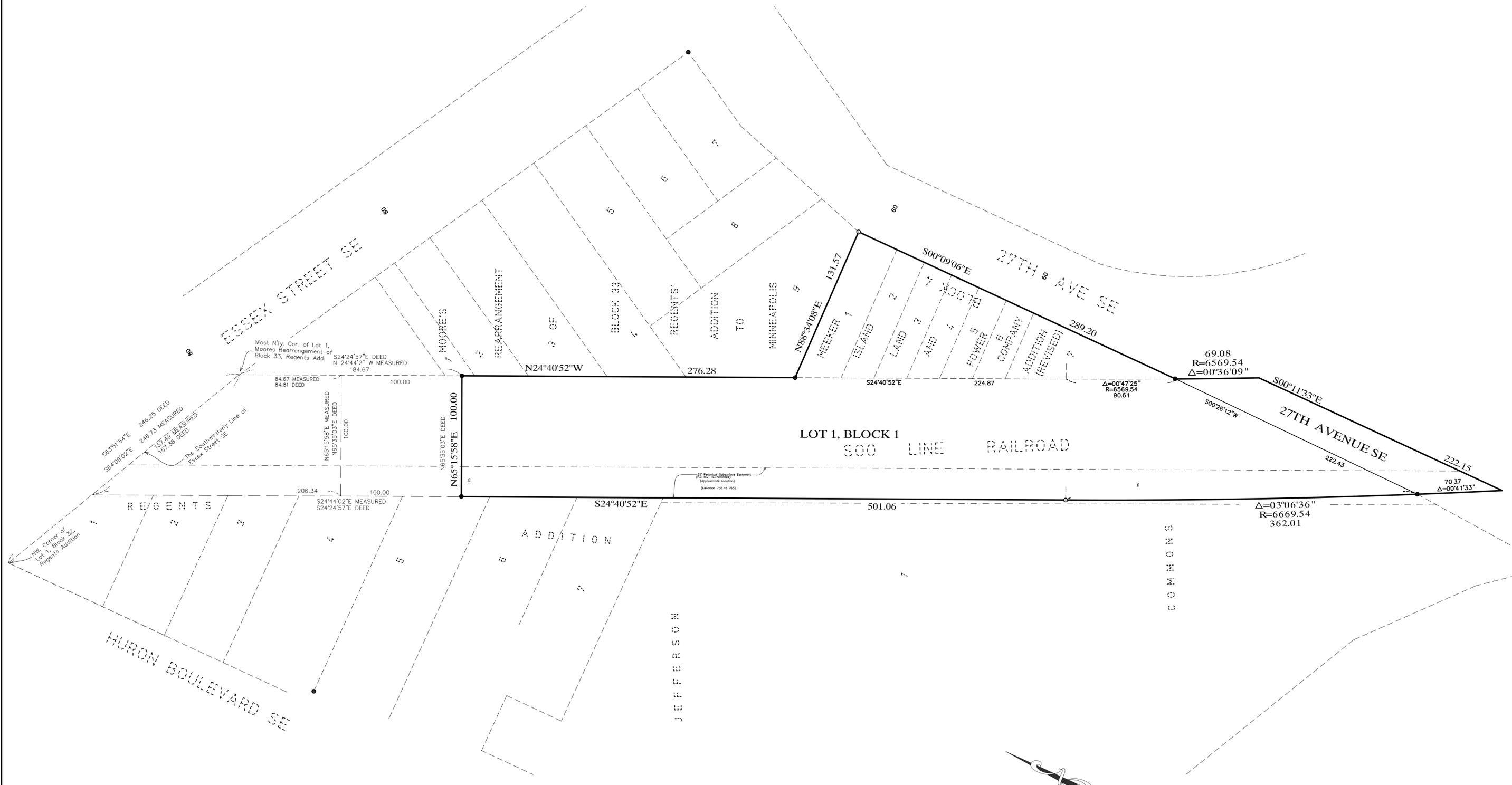
JOB #15153



ACRE LAND SURVEYING
Serving Twin Cities Metro
area and beyond
763-458-2997 acrelandsurvey@gmail.com

ELSEY ADDITION

C.R. DOC. NO.



ACRE LAND SURVEYING
BLAINE, MN

The East line of Block 4, MEEKER ISLAND LAND AND POWER COMPANY (REVISED) has an assumed bearing of South 00 degrees 09 minutes 06 seconds East.

- Denotes found iron monument
- Denotes 1/2 inch by 14 inch iron monument set and marked by L.S. No. 44125.

ELSEY ADDITION

C.R. DOC. NO.

KNOW ALL PERSONS BY THESE PRESENTS: That 117 27th LLC, a Minnesota limited liability company, fee owner of the following described property situated in the County of Hennepin, State of Minnesota, to wit:

Lots 1, 2, 3, 4, 5, 6, and 7, Block 4, Meeker Island and Power Company's Addition (Revised), according to the recorded plat thereof, Hennepin County, Minnesota.

(Abstract Property)

AND that 117 27th LLC, a Minnesota limited liability company, fee owner, of the following described property situated in the County of Hennepin, State of Minnesota, to wit: Minneapolis, according to the recorded plats thereof, and situate in Hennepin County, Minnesota

That part of the 100 foot wide Soo Line Railroad Company right of way formerly known as the Chicago, Milwaukee and Saint Paul Railway Company right of way across the Northeast 1/4 and the Southwest 1/4 of the Southeast 1/4 of the Northwest 1/4 of Section 30, Township 29 North, Range 23 West lying Northwesterly of the centerline of 27th Avenue SE and lying Southeasterly of the following described parcel:

That part of the 100 foot wide Soo Line Railroad Right-of-Way, formerly known and platted as the Chicago Milwaukee and St. Paul Railway (C.M. and SLP.) in the Northwest Quarter of Section 30, Township 29 North, Range 23 West of the 4th Principal Meridian, Hennepin County, Minnesota described as follows:

Commencing at the northwest corner of Lot 1, Block 32, "Regents Addition" according to the recorded plat thereof, said Hennepin County; thence South 63 degrees 51 minutes 54 seconds East, assumed bearing along the southwesterly line of Essex Street Southeast as taken or opened by the City of Minneapolis, a distance of 246.25 feet of the most northerly corner of Lot 1, Moores Rearrangement of Block 33, Regents' Addition to Minneapolis, according to the recorded plat thereof, said Hennepin County; thence North 63 degrees 51 minutes 54 seconds West along said southwesterly line a distance of 157.38 feet to the westerly line of said railroad right-of-way; thence South 24 degrees 24 minutes 57 seconds East along said westerly line a distance of 206.34 feet to the point of beginning of the land to be described; thence North 65 degrees 35 minutes 03 seconds East a distance of 100.00 feet to the easterly line of said railroad right-of-way; thence South 24 degrees 24 minutes 57 seconds East along said easterly line a distance of 100.00 feet; thence South 65 degrees 35 minutes 03 seconds West a distance of 100.00 feet to said westerly line; thence North 24 degrees 24 minutes 57 seconds West along said westerly line a distance of 100.00 feet to said point of beginning, according to the United States Government Survey thereof and situated in Hennepin County, Minnesota.

(Abstract Property)

Have caused the same to be surveyed and platted as ELSEY ADDITION and do hereby dedicate to the public for public use forever the public ways as shown on this plat.

In witness whereof said 117 27th LLC, LLC, a Minnesota limited liability company, has caused these presents to be signed by its proper officer this ____ day of _____, 20__.

Signed: 117 27th LLC, LLC

_____, Chief Manager

STATE OF MINNESOTA
COUNTY OF _____

This instrument was acknowledged before me this ____ day of _____, 20____, by Chief Manager of 117 27th LLC, a Minnesota limited liability company.

Notary Public, _____ County, Minnesota

My Commission expires _____

In witness whereof said 117 27th LLC, a Minnesota limited liability company, has caused these presents to be signed by its proper officer this ____ day of _____, 20____.

Signed: 117 27th LLC

_____, Chief Manager

STATE OF MINNESOTA
COUNTY OF _____

This instrument was acknowledged before me this ____ day of _____, 20____, by Chief Manager of 117 27th LLC, a Minnesota limited liability company.

Notary Public, _____ County, Minnesota

My Commission expires _____

I, Eric R. Vickaryous do hereby certify that this plat was prepared by me or under my direct supervision; that I am a duly Licensed Land Surveyor in the State of Minnesota; that this plat is a correct representation of the boundary survey; that all mathematical data and labels are correctly designated on this plat; that all monuments depicted on this plat have been, or will be correctly set within one year; that all water boundaries and wet lands, as defined in Minnesota Statutes, Section 505.01, Subd. 3, as of the date of this certificate are shown and labeled on this plat, and all public ways are shown and labeled on this plat.

Dated this ____ day of _____, 20____.

Eric R. Vickaryous, Licensed Land Surveyor
Minnesota License Number 44125

STATE OF MINNESOTA
COUNTY OF _____

This instrument was acknowledged before me this ____ day of _____, 20____, by Eric R. Vickaryous.

Notary Public, _____ County, Minnesota

My Commission expires _____

MINNEAPOLIS, MINNESOTA

I hereby certify this plat of ELSEY ADDITION was approved and accepted by the City Planning Commission of the City of Minneapolis, Minnesota at a meeting thereof held this ____ day of _____, 20____. If applicable, the written comments and recommendations of the Commissioner of Transportation and the County Highway Engineer have been received by the City or the prescribed 30 day period has elapsed without receipt of such comments and recommendations, as provided by Minnesota Statutes, Section, 505.03, Subd. 2.

City Clerk of Minneapolis, Minnesota

By _____ City Clerk

RESIDENT AND REAL ESTATE SERVICES, Hennepin County, Minnesota

I hereby certify that taxes payable in 20____ and prior years have been paid for the land described on this plat, dated this ____ day of _____, 20____.

Mark V. Chapin, County Auditor

By _____ Deputy

SURVEY DIVISION, Hennepin County, Minnesota

Pursuant to MN. STAT. Sec. 383B.565 (1969), this plat has been approved this ____ day of _____, 20____.

Chris F. Mavis, County Surveyor

By _____

COUNTY RECORDER, Hennepin County, Minnesota

I hereby certify that this plat of ELSEY ADDITION was recorded in this office this ____ day of _____, 20____, at ____ o'clock ____ m.

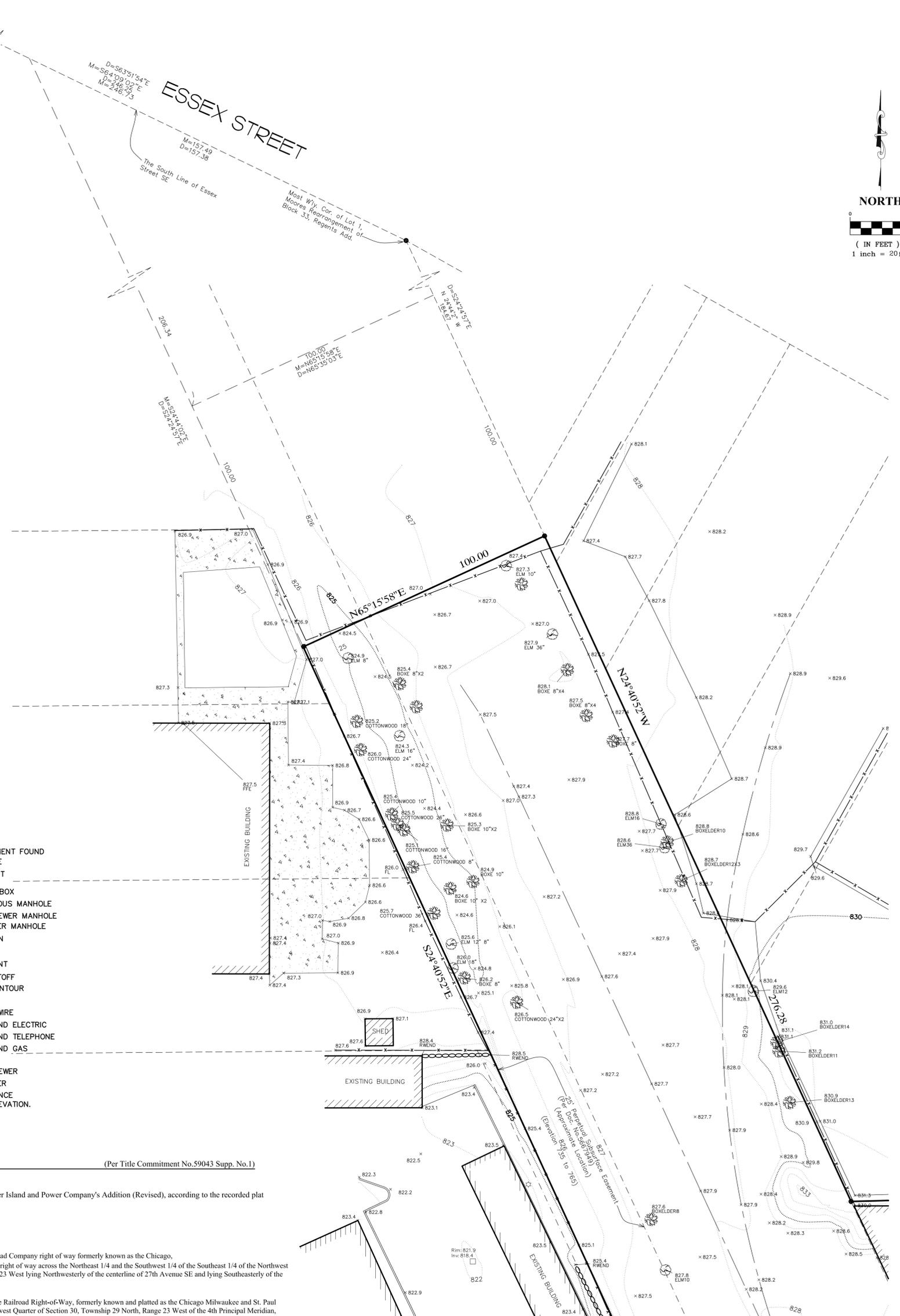
Martin McCormick, County Recorder

By _____ Deputy

ALTA/ACSM LAND TITLE SURVEY

~for~ CIVIL SITE GROUP

PROPERTY ADDRESS: #117 - 27th Ave SE.,
Minneapolis, MN



LEGEND

- DENOTES IRON MONUMENT FOUND
- DENOTES UTILITY POLE
- ☆ DENOTES STREET LIGHT
- ☐ DENOTES TELEPHONE BOX
- ⊙ DENOTES MISCELLANEOUS MANHOLE
- ⊙ DENOTES SANITARY SEWER MANHOLE
- ⊙ DENOTES STORM SEWER MANHOLE
- ⊙ DENOTES CATCH BASIN
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- ⊙ DENOTES FIRE HYDRANT
- ⊙ DENOTES WATER SHUTOFF
- ⊙ DENOTES EXISTING CONTOUR
- GW — DENOTES GUY WIRE
- ohw — DENOTES OVERHEAD WIRE
- e — DENOTES UNDERGROUND ELECTRIC
- t — DENOTES UNDERGROUND TELEPHONE
- g — DENOTES UNDERGROUND GAS
- i — DENOTES WATERMAIN
- s — DENOTES SANITARY SEWER
- ss — DENOTES STORM SEWER
- x — DENOTES EXISTING FENCE
- ×1011.2 DENOTES EXISTING ELEVATION.
- ▭ DENOTES CONCRETE
- ▭ DENOTES BITUMINOUS

LEGAL DESCRIPTION:

(Per Title Commitment No.59043 Supp. No.1)

Parcel 1:
Lots 1, 2, 3, 4, 5, 6, and 7, Block 4, Meeker Island and Power Company's Addition (Revised), according to the recorded plat thereof, Hennepin County, Minnesota.

(Abstract Property)

Parcel 2:
That part of the 100 foot wide Soo Line Railroad Company right of way formerly known as the Chicago, Milwaukee and Saint Paul Railway Company right of way across the Northeast 1/4 and the Southwest 1/4 of the Northeast 1/4 of the Northwest 1/4 of Section 30, Township 29 North, Range 23 West lying Northwesterly of the centerline of 27th Avenue SE and lying Southeasterly of the following described parcel:

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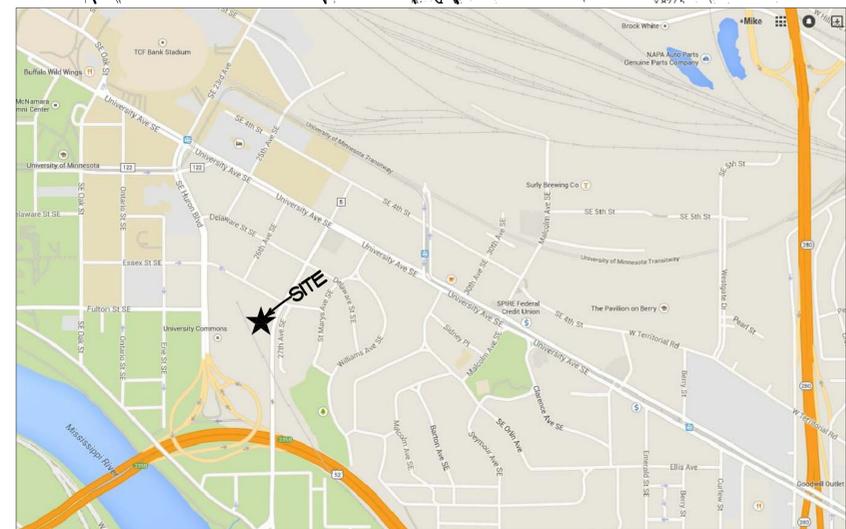
(Abstract Property)

GENERAL NOTES:

- BEARING SYSTEM IS BASED ON NAD83 (1996 ADJ)- (VIA REAL TIME GPS MEASUREMENTS UTILIZING MNDOT VRS NETWORK)
- SUBJECT PROPERTY ADDRESS IS 117 27TH AVE SE, MINNEAPOLIS MN 55414
- PROPERTY IDENTIFICATION # (P.I.D.) = 30-029-23-31-0006
- FIELD WORK COMPLETED 04/13/2015.

BENCHMARK

BASIS FOR ELEVATION: University of Minnesota 1912 USC&G Datum, Top Nut of Hydrant at the Southwest Quadrant of Huron Blvd SE and Essex Street SE. Elevation=829.55 (Not Shown)



VICINITY MAP:

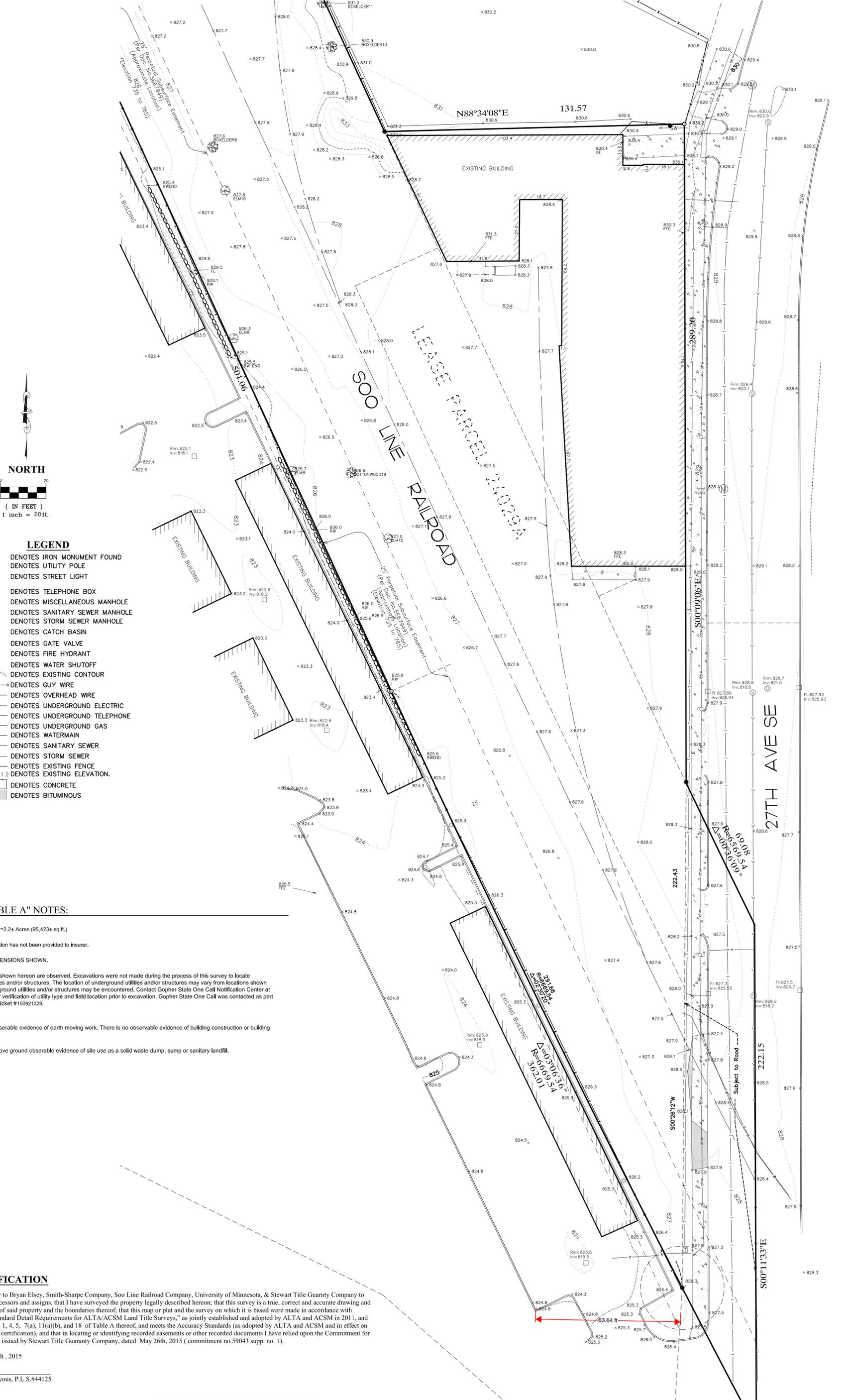
JOB #15153

ACRE LAND SURVEYING
Serving Twin Cities Metro area and beyond
763-458-2997 acrelandsurveying@gmail.com

ALTA/ACSM LAND TITLE SURVEY

~for~ CIVIL SITE GROUP

PROPERTY ADDRESS: #117 - 27th Ave SE,
Minneapolis, MN



LEGEND

- DENOTES IRON MONUMENT FOUND
- DENOTES UTILITY POLE
- ☆ DENOTES STREET LIGHT
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- ×1011.2 DENOTES EXISTING ELEVATION.
- ▨ DENOTES CONCRETE
- ▩ DENOTES BITUMINOUS

ALTA "TABLE A" NOTES:

4. Gross land area = 2.2± Acres (95,423± sq.ft.)
- 6b. Zoning information has not been provided to insurer.
- 7a. BUILDING DIMENSIONS SHOWN.
- 11(a & b). Utilities shown hereon are observed. Excavations were not made during the process of this survey to locate underground utilities and/or structures. The location of underground utilities and/or structures may vary from locations shown hereon and underground utilities and/or structures may be encountered. Contact Gopher State One Call Notification Center at (651) 454-0002 for verification of utility type and field location prior to excavation. Gopher State One Call was contacted as part of this survey per ticket #150921329.
16. There is no observable evidence of earth moving work. There is no observable evidence of building construction or building additions.
18. There is no above ground observable evidence of site use as a solid waste dump, sump or sanitary landfill.

CERTIFICATION

I hereby certify to Bryan Eelsey, Smith-Sharp Company, Soo Line Railroad Company, University of Minnesota, & Stewart Title Guaranty Company to their heirs, successors and assigns, that I have surveyed the property legally described hereon; that this survey is a true, correct and accurate drawing and representation of said property and the boundaries thereof; that this map or plat and the survey on which it is based were made in accordance with "Minimum Standard Detail Requirements for ALTA/ACSM Land Title Surveys," as jointly established and adopted by ALTA and ACSM in 2011, and includes Items 1, 4, 5, 7(a), 11(a)(b), and 18 of Table A thereof, and meets the Accuracy Standards (as adopted by ALTA and ACSM and in effect on the date of this certification), and that in locating or identifying recorded easements or other recorded documents I have relied upon the Commitment for Title Insurance issued by Stewart Title Guaranty Company, dated May 26th, 2015 (commitment no.59043 supp. no. 1).

Dated : June 9th , 2015

Eric R. Vickaryous, P.L.S.#44125

ACRE LAND SURVEYING
Serving Twin Cities Metro
area and beyond
763-458-2997 acrelandsurvey@gmail.com

DRAWN BY: MDN	JOB NO: 15153	DATE: 04/28/15	
CHECK BY: ERV			
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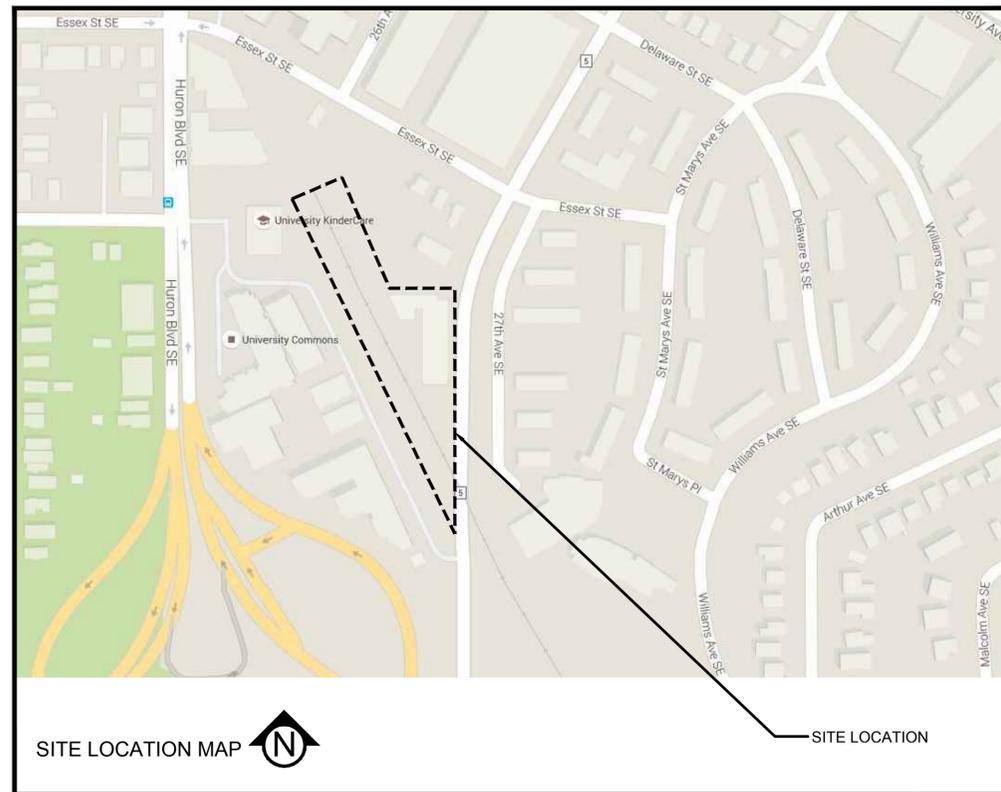
JOB #15153

SHEET 2 OF 2

ESSEX AND 27TH AVENUE SE

MINNEAPOLIS, MINNESOTA

ISSUED FOR: LAND USE SUBMITTAL



OWNER, DEVELOPER, CONTRACTOR SIGNED STATEMENT

ALL CLEARING, GRADING, CONSTRUCTION OR DEVELOPMENT WILL BE DONE PURSUANT TO THE PLAN, SIGNED BY PARTIES BELOW:

OWNER: _____

DEVELOPER: _____

CONTRACTOR: _____

DEVELOPER / PROPERTY OWNER:

ELSEY PARTNERS, LLC
1532 COLLEGE AVENUE, F-19
MANHATTAN, KS 66502

ARCHITECT:

PRIME DESIGN
1532 COLLEGE AVENUE, F-19
MANHATTAN, KS 66502

ENGINEER / LANDSCAPE ARCHITECT:

CIVIL SITE GROUP
4931 W 35TH STREET
SUITE 200
ST LOUIS PARK, MN 55416
612-615-0060

SURVEYOR:

ACRE LAND SURVEYING, INC.
9140 BALTIMORE STREET NE, SUITE 100
BLAINE, MN 55449
763-458-2997

GEOTECHNICAL ENGINEER:

HAUGO GEOTECHNICAL SERVICES, LLC
2825 CEDAR AVENUE SOUTH
MINNEAPOLIS, MN 55407
612-269-4027

MASTER LEGEND:

----- 932 -----	EX. 1' CONTOUR ELEVATION INTERVAL	●	PROPOSED MANHOLE STORM
x 932	EXISTING SPOT GRADE ELEVATION	■	PROPOSED CATCH BASIN OR CATCH BASIN MANHOLE STORM
----- 939 -----	PROPOSED 1' CONTOUR ELEVATION INTERVAL	⊕	PROPOSED GATE VALVE
----- 932.0 -----	SPOT GRADE ELEVATION (GUTTER/FLOW LINE UNLESS OTHERWISE NOTED)	⊕	PROPOSED FIRE HYDRANT
932.0BC / 932.0TC	SPOT GRADE ELEVATION BACK OF CURB (TOP OF CURB)	⊕	PROPOSED MANHOLE SANITARY
932.0TW	SPOT GRADE ELEVATION TOP OF WALL	⊕	PROPOSED SIGN
932.0BW	SPOT GRADE ELEVATION BOTTOM OF WALL	⊕	PROPOSED LIGHT
→	DRAINAGE ARROW	⊕	PROPOSED SANITARY SEWER
EO↑	EMERGENCY OVERFLOW	⊕	PROPOSED STORM SEWER
-----	SILT FENCE / GRADING LIMIT	⊕	PROPOSED WATER MAIN
⊕	INLET PROTECTION	⊕	EXISTING SANITARY SEWER
⊕	STABILIZED CONSTRUCTION ENTRANCE	⊕	EXISTING STORM SEWER
⊕	SOIL BORING LOCATION	⊕	EXISTING WATER MAIN
⊕	CURB AND GUTTER (T.O. = TIP OUT)	⊕	EXISTING GAS MAIN
		⊕	EXISTING UNDERGROUND ELECTRIC
		⊕	EXISTING UNDERGROUND CABLE
		⊕	EXISTING MANHOLE
		⊕	EXISTING ELECTRIC BOX
		⊕	EXISTING CATCH BASIN
		⊕	EXISTING LIGHT
		⊕	EXISTING HYDRANT
		⊕	EXISTING GAS METER
		⊕	EXISTING STOPBOX
		⊕	EXISTING GAS VALVE
		⊕	EXISTING GATE VALVE

SHEET INDEX	
SHEET NUMBER	SHEET TITLE
C0.0	TITLE SHEET
C0.1	SITE SURVEY: 1 OF 2
C0.2	SITE SURVEY: 2 OF 2
C1.0	REMOVALS PLAN
C2.0	SITE PLAN
C3.0	GRADING PLAN
C4.0	UTILITY PLAN
C5.0	CIVIL DETAILS
C5.1	CIVIL DETAILS
C5.2	CIVIL DETAILS
C5.3	CIVIL DETAILS
L1.0	LANDSCAPE PLAN
L1.1	LANDSCAPE PLAN
L1.2	PLAZA PLAN, DETAILS & NOTES
SW1.0	STORM WATER POLLUTION PREVENTION PLAN - EXISTING CONDITIONS
SW1.1	STORM WATER POLLUTION PREVENTION PLAN - PROPOSED CONDITIONS
SW1.2	STORM WATER POLLUTION PREVENTION PLAN - DETAILS
SW1.3	STORM WATER POLLUTION PREVENTION PLAN - NARRATIVE
SW1.4	STORM WATER POLLUTION PREVENTION PLAN - ATTACHMENTS
SW1.5	STORM WATER POLLUTION PREVENTION PLAN - ATTACHMENTS

PROJECT
ESSEX AND 27TH AVENUE SE REDEVELOPMENT
117 27TH AVENUE SE, MINNEAPOLIS, MN
ELSEY PARTNERS, LLC
1532 COLLEGE AVENUE, F-19, MANHATTAN, KS 66502

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Matthew R. Pavek
Matthew R. Pavek
DATE 11/06/15 LICENSE NO. 44263

ISSUE/SUBMITTAL SUMMARY	
DATE	DESCRIPTION
10/16/15	PDR SUBMITTAL
11/06/15	LAND USE SUBMITTAL

REVISION SUMMARY	
DATE	DESCRIPTION

PROJECT NUMBER: 15033
TITLE SHEET
C0.0



ALTA/ACSM LAND TITLE SURVEY

~for~ CIVIL SITE GROUP

PROPERTY ADDRESS: #117 - 27th Ave SE.,
Minneapolis, MN

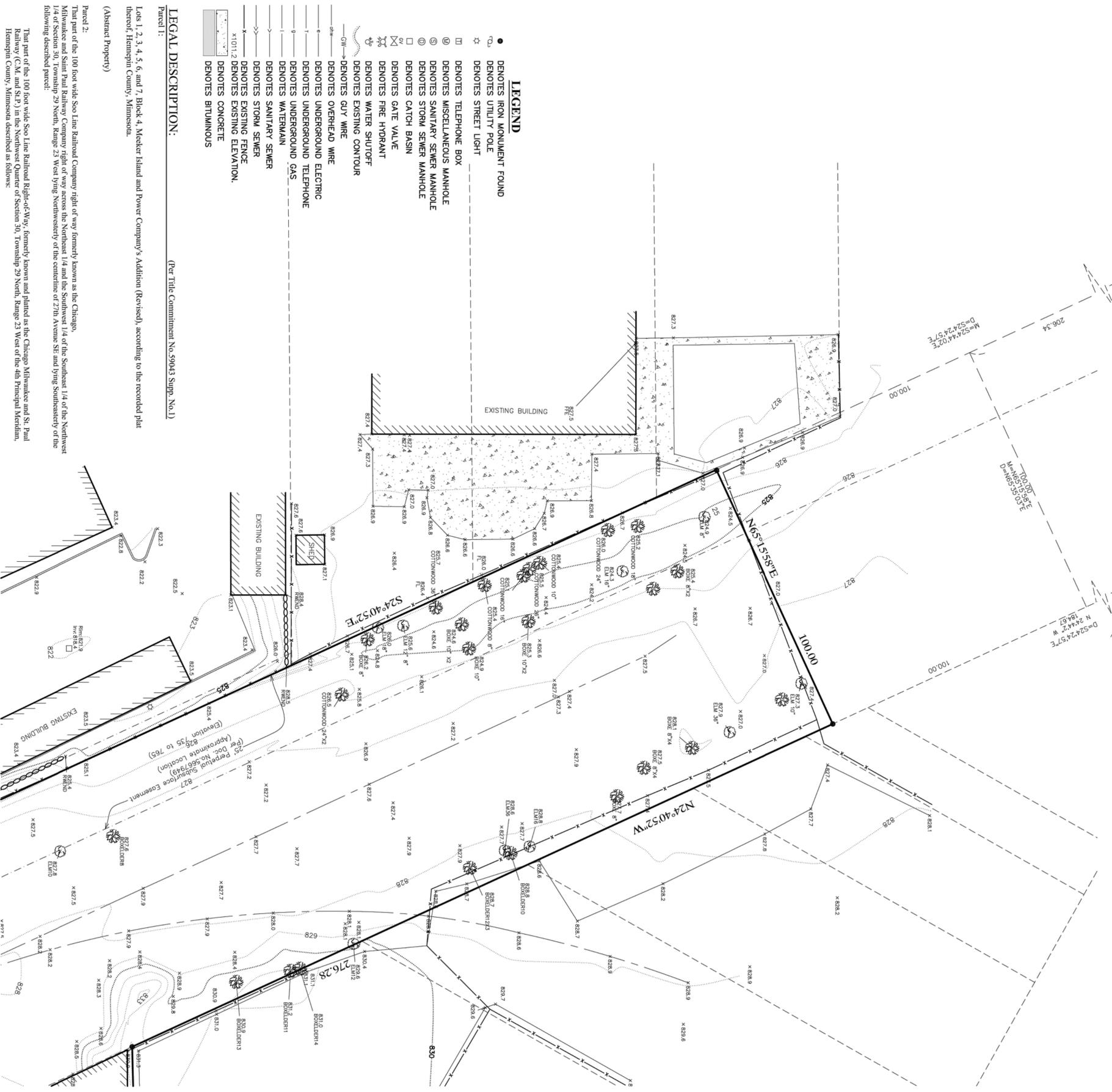
ESSEX STREET

REVISION SUMMARY

DATE	DESCRIPTION

SITE SURVEY - 1 OF 2

C0.1



- ### LEGEND
- DENOTES IRON MONUMENT FOUND
 - DENOTES UTILITY POLE
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 - E — DENOTES EXISTING FENCE
 - ⊗⊙⊚⊙⊚ DENOTES EXISTING ELEVATION.
 - ⊗⊙⊚⊙⊚ DENOTES CONCRETE
 - ⊗⊙⊚⊙⊚ DENOTES BITUMINOUS

LEGAL DESCRIPTION:

Parcel 1: (Per Title Commitment No.59043 Supp. No.1)
 Lots 1, 2, 3, 4, 5, 6, and 7, Block 4, Wecker Island and Power Company's Addition (Revised), according to the recorded plat thereof, Hennepin County, Minnesota.
 (Abstract Property)

Parcel 2:
 That part of the 100 foot wide Soo Line Railroad Company right of way formerly known as the Chicago, Milwaukee and Saint Paul Railway, Company right of way, more or less, the Southeast 1/4 of the Southeast 1/4 of the Northwest 1/4 of Section 30, Township 29 North, Range 23 West 9th Principal Meridian, following described parcel:

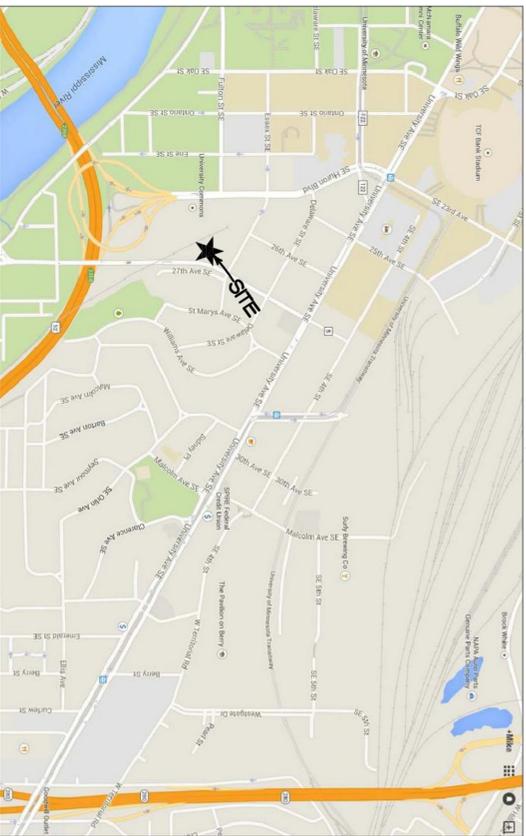
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- ### GENERAL NOTES:
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 - SUBJECT PROPERTY ADDRESS IS 117 27TH AVE SE, MINNEAPOLIS MN 55414
 - PROPERTY IDENTIFICATION # (P.ID.) = 30A29-23-3-1-0006
 - FIELD WORK COMPLETED 04/19/2015

BENCHMARK

BASIS FOR ELEVATION: University of Minnesota 1912 USCG&G Datum, Top Nail of Hydrant at the Southwest Quadrant of Huron Blvd SE and Essex Street SE, Elevation=829.59 (Not Shown)

JOB #15153



VICINITY MAP:

ACRE LAND SURVEYING
 Serving Twin Cities Metro area and beyond
 763-458-2997 acrlandsurvey@gmail.com

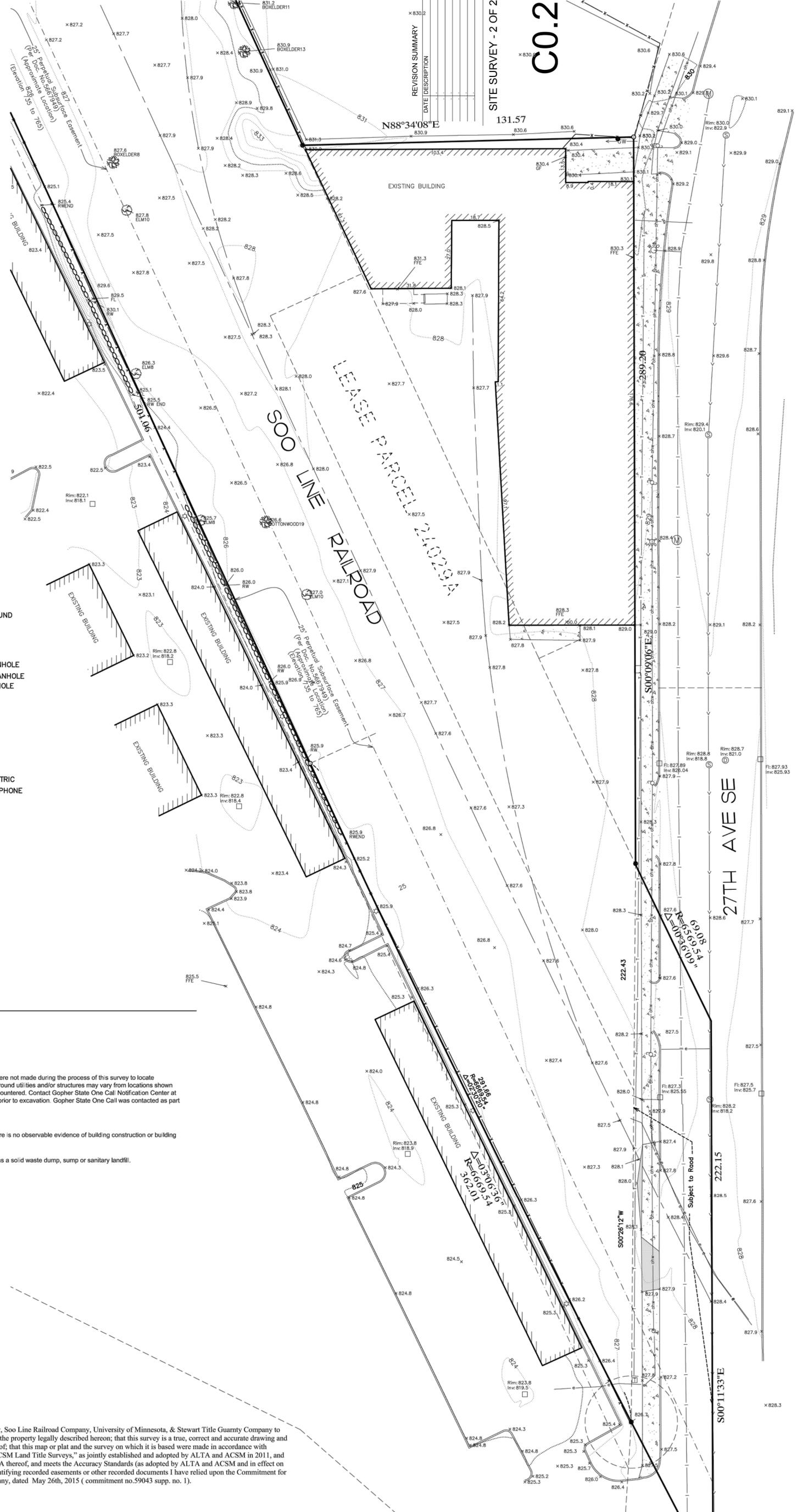
ALTA/ACSM LAND TITLE SURVEY

~for~ CIVIL SITE GROUP

PROPERTY ADDRESS: #117 - 27th Ave SE,
Minneapolis, MN

C0.2

SITE SURVEY - 2 OF 2



REVISION	DESCRIPTION	DATE
1		
2		
3		



LEGEND

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- DENOTES SANITARY SEWER MANHOLE
- DENOTES STORM SEWER MANHOLE
- DENOTES CATCH BASIN
- DENOTES GATE VALVE
- DENOTES FIRE HYDRANT
- DENOTES WATER SHUTOFF
- DENOTES EXISTING CONTOUR
- GW— DENOTES GUY WIRE
- ohw— DENOTES OVERHEAD WIRE
- e— DENOTES UNDERGROUND ELECTRIC
- t— DENOTES UNDERGROUND TELEPHONE
- g— DENOTES UNDERGROUND GAS
- w— DENOTES WATERMAIN
- s— DENOTES SANITARY SEWER
- >>— DENOTES STORM SEWER
- x— DENOTES EXISTING FENCE
- ×1011.2 DENOTES EXISTING ELEVATION.
- DENOTES CONCRETE
- DENOTES BITUMINOUS

ALTA "TABLE A" NOTES:

4. Gross land area = 2.2± Acres (95,423± sq.ft.)
- 6b. Zoning information has not been provided to insurer.
- 7a. BUILDING DIMENSIONS SHOWN.
- 11(a & b). Utilities shown hereon are observed. Excavations were not made during the process of this survey to locate underground utilities and/or structures. The location of underground utilities and/or structures may vary from locations shown hereon and underground utilities and/or structures may be encountered. Contact Gopher State One Call Notification Center at (651) 454-0002 for verification of utility type and field location prior to excavation. Gopher State One Call was contacted as part of this survey per ticket #150921329.
16. There is no observable evidence of earth moving work. There is no observable evidence of building construction or building additions.
18. There is no above ground observable evidence of site use as a solid waste dump, sump or sanitary landfill.

CERTIFICATION

I hereby certify to Bryan Elsey, Smith-Sharp Company, Soo Line Railroad Company, University of Minnesota, & Stewart Title Guaranty Company to their heirs, successors and assigns, that I have surveyed the property legally described hereon; that this survey is a true, correct and accurate drawing and representation of said property and the boundaries thereof; that this map or plat and the survey on which it is based were made in accordance with "Minimum Standard Detail Requirements for ALTA/ACSM Land Title Surveys," as jointly established and adopted by ALTA and ACSM in 2011, and includes Items 1, 4, 5, 7(a), 11(a)(b), and 18 of Table A thereof, and meets the Accuracy Standards (as adopted by ALTA and ACSM and in effect on the date of this certification), and that in locating or identifying recorded easements or other recorded documents I have relied upon the Commitment for Title Insurance issued by Stewart Title Guaranty Company, dated May 26th, 2015 (commitment no.59043 supp. no. 1).

Dated : June 9th , 2015

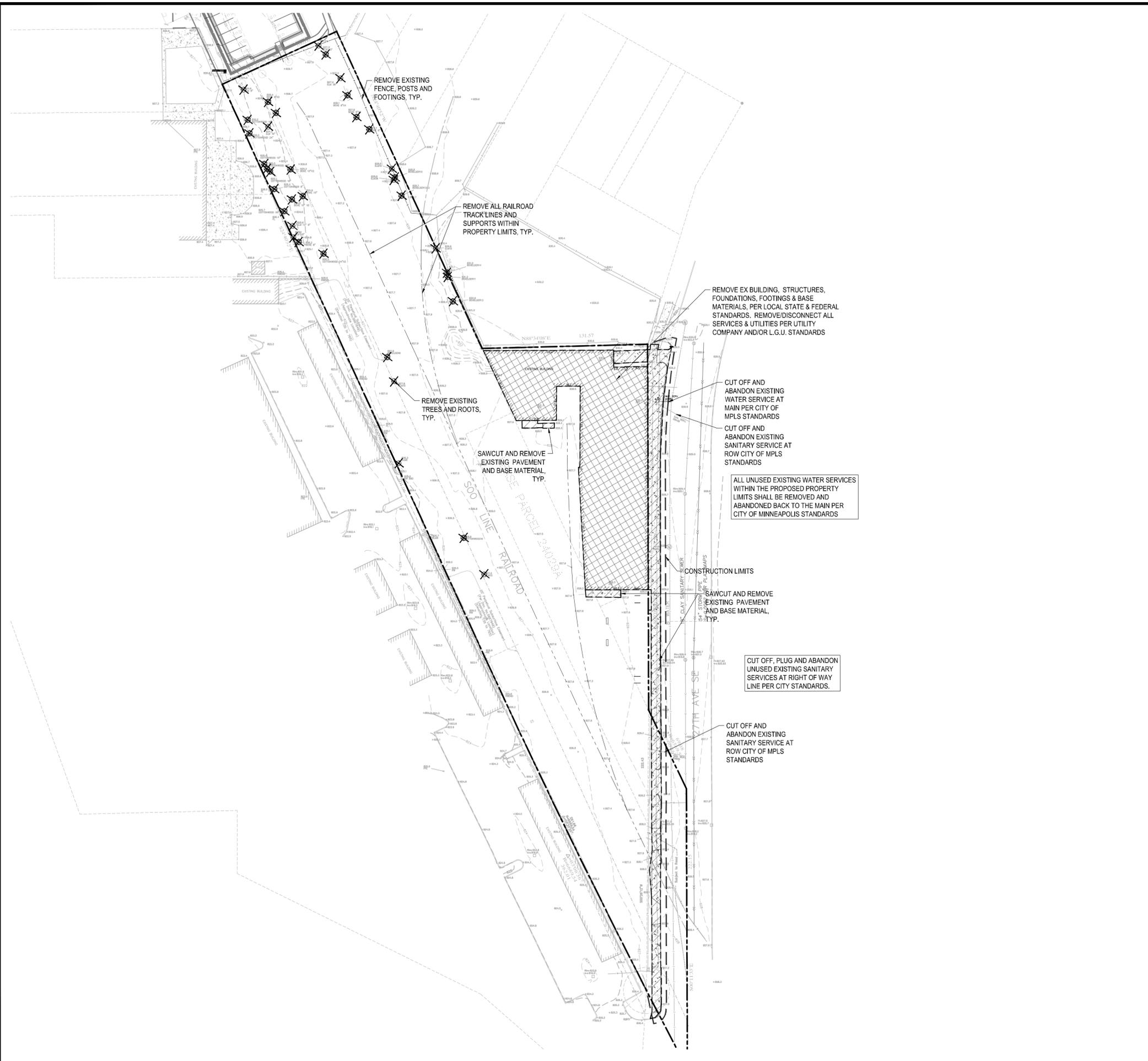
Eric R. Vickaryous, P.L.S.#44125

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DRAWN BY: MDN	JOB NO: 15153	DATE: 04/28/15	
CHECK BY: ERV			
1			
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3			
NO.	DATE	DESCRIPTION	BY

JOB #15153

SHEET 2 OF 2



OWNER, DEVELOPER, CONTRACTOR SIGNED STATEMENT

ALL CLEARING, GRADING, CONSTRUCTION OR DEVELOPMENT WILL BE DONE PURSUANT TO THE PLAN, SIGNED BY PARTIES BELOW:

OWNER: _____
 DEVELOPER: _____
 CONTRACTOR: _____

OWNER INFORMATION

OWNER: ELSEY PARTNERS, LLC
 1532 COLLEGE AVENUE, F-19
 MANHATTAN, KS 66502

REMOVAL NOTES:

- SEE STORM WATER POLLUTION PREVENTION PLAN (SWPPP) PLAN FOR CONSTRUCTION STORM WATER MANAGEMENT PLAN.
- REMOVAL OF MATERIALS NOTED ON THE DRAWINGS SHALL BE IN ACCORDANCE WITH MNDOT, STATE AND LOCAL REGULATIONS.
- REMOVAL OF PRIVATE UTILITIES SHALL BE COORDINATED WITH UTILITY OWNER PRIOR TO CONSTRUCTION ACTIVITIES.
- EXISTING PAVEMENTS SHALL BE SAWCUT IN LOCATIONS AS SHOWN ON THE DRAWINGS OR THE NEAREST JOINT FOR PROPOSED PAVEMENT CONNECTIONS.
- REMOVED MATERIALS SHALL BE DISPOSED OF TO A LEGAL OFF-SITE LOCATION AND IN ACCORDANCE WITH STATE AND LOCAL REGULATIONS.
- ABANDON, REMOVAL, CONNECTION, AND PROTECTION NOTES SHOWN ON THE DRAWINGS ARE APPROXIMATE. COORDINATE WITH PROPOSED PLANS.
- EXISTING ON-SITE FEATURES NOT NOTED FOR REMOVAL SHALL BE PROTECTED THROUGHOUT THE DURATION OF THE CONTRACT.
- PROPERTY LINES SHALL BE CONSIDERED GENERAL CONSTRUCTION LIMITS UNLESS OTHERWISE NOTED ON THE DRAWINGS. WORK WITHIN THE GENERAL CONSTRUCTION LIMITS SHALL INCLUDE STAGING, DEMOLITION AND CLEAN-UP OPERATIONS AS WELL AS CONSTRUCTION SHOWN ON THE DRAWINGS.
- MINOR WORK OUTSIDE OF THE GENERAL CONSTRUCTION LIMITS SHALL BE ALLOWED AS SHOWN ON THE PLAN AND PER CITY REQUIREMENTS.
- DAMAGE BEYOND THE PROPERTY LIMITS CAUSED BY CONSTRUCTION ACTIVITY SHALL BE REPAIRED IN A MANNER APPROVED BY THE ENGINEER/LANDSCAPE ARCHITECT OR IN ACCORDANCE WITH THE CITY.
- PROPOSED WORK (BUILDING AND CIVIL) SHALL NOT DISTURB EXISTING UTILITIES UNLESS OTHERWISE SHOWN ON THE DRAWINGS AND APPROVED BY THE CITY PRIOR TO CONSTRUCTION.
- SITE SECURITY MAY BE NECESSARY AND PROVIDED IN A MANNER TO PROHIBIT VANDALISM, AND THEFT, DURING AND AFTER NORMAL WORK HOURS, THROUGHOUT THE DURATION OF THE CONTRACT. SECURITY MATERIALS SHALL BE IN ACCORDANCE WITH THE CITY.
- VEHICULAR ACCESS TO THE SITE SHALL BE MAINTAINED FOR DELIVERY AND INSPECTION ACCESS DURING NORMAL OPERATING HOURS. AT NO POINT THROUGHOUT THE DURATION OF THE CONTRACT SHALL CIRCULATION OF ADJACENT STREETS BE BLOCKED WITHOUT APPROVAL BY THE CITY PRIOR TO CONSTRUCTION ACTIVITIES.
- ALL TRAFFIC CONTROLS SHALL BE PROVIDED AND ESTABLISHED PER THE REQUIREMENTS OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MMUTCD) AND THE CITY. THIS SHALL INCLUDE, BUT NOT BE LIMITED TO, SIGNAGE, BARRICADES, FLASHERS, AND FLAGGERS AS NEEDED. ALL PUBLIC STREETS SHALL REMAIN OPEN TO TRAFFIC AT ALL TIMES. NO ROAD CLOSURES SHALL BE PERMITTED WITHOUT APPROVAL BY THE CITY.
- SHORING FOR BUILDING EXCAVATION MAY BE USED AT THE DISCRETION OF THE CONTRACTOR AND AS APPROVED BY THE OWNERS REPRESENTATIVE AND THE CITY PRIOR TO CONSTRUCTION ACTIVITIES.
- STAGING, DEMOLITION, AND CLEAN-UP AREAS SHALL BE WITHIN THE PROPERTY LIMITS AS SHOWN ON THE DRAWINGS AND MAINTAINED IN A MANNER AS REQUIRED BY THE CITY.

CITY OF MINNEAPOLIS REMOVAL NOTES:

- RESERVED FOR CITY SPECIFIC REMOVAL NOTES.

EROSION CONTROL NOTES:

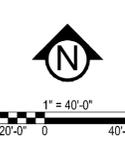
SEE SWPPP ON SHEETS SW1.0-SW1.5

REMOVALS LEGEND:

- 932 EX. 1' CONTOUR ELEVATION INTERVAL
- REMOVAL OF PAVEMENT AND ALL BASE MATERIAL, INCLUDING BIT., CONC., AND GRAVEL PAVTS.
- REMOVAL OF STRUCTURE INCLUDING ALL FOOTINGS AND FOUNDATIONS.
- REMOVAL OF UTILITY LINES - COORDINATE WITH UTILITY COMPANY PRIOR TO CONSTRUCTION
- TREE REMOVAL - INCLUDING ROOTS AND STUMPS



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 (800) 252-1166 TOLL FREE
 (651) 454-0002 LOCAL



CivilSite GROUP
 4931 W. 35TH ST. SUITE 200
 ST. LOUIS PARK, MN 55416
 CivilSiteGroup.com
 Matt Pavak 763-213-3944 Pat Sarver 952-250-2003

PROJECT
ESSEX AND 27TH AVENUE SE REDEVELOPMENT
 117 27TH AVENUE SE, MINNEAPOLIS, MN
ELSEY PARTNERS, LLC
 1532 COLLEGE AVENUE, F-19, MANHATTAN, KS 66502

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Matthew R. Pavak
 DATE 11/06/15 LICENSE NO. 44263

ISSUE/SUBMITTAL SUMMARY

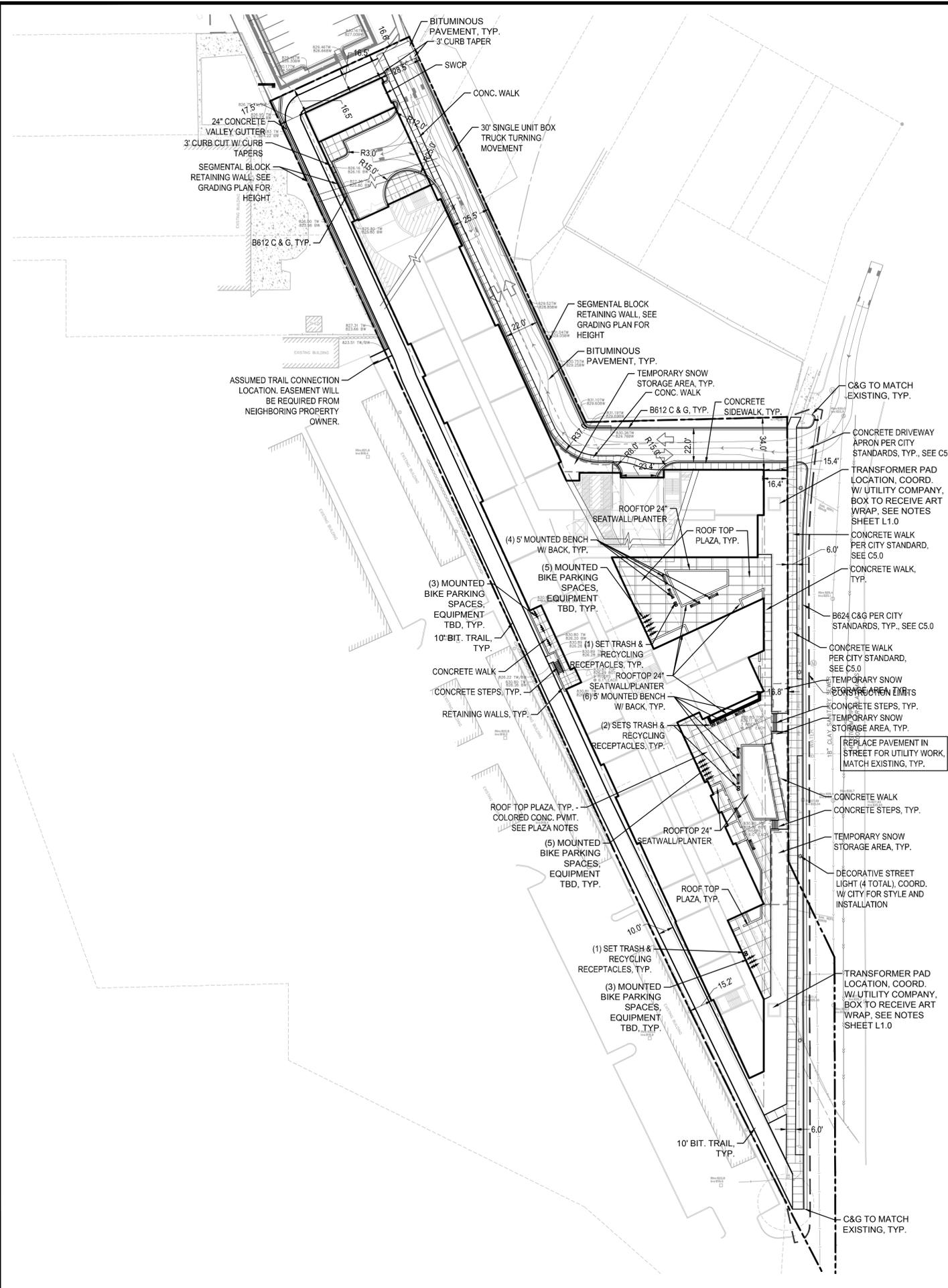
DATE	DESCRIPTION
10/16/15	PDR SUBMITTAL
11/06/15	LAND USE SUBMITTAL

REVISION SUMMARY

DATE	DESCRIPTION

PROJECT NUMBER: 15033

REMOVALS PLAN
C1.0



CITY OF MINNEAPOLIS SITE SPECIFIC NOTES:

- RESERVED FOR CITY SPECIFIC NOTES.

SITE LAYOUT NOTES:

- CONTRACTOR SHALL VERIFY LOCATIONS AND LAYOUT OF ALL SITE ELEMENTS PRIOR TO BEGINNING CONSTRUCTION, INCLUDING BUT NOT LIMITED TO, LOCATIONS OF EXISTING AND PROPOSED PROPERTY LINES, EASEMENTS, SETBACKS, UTILITIES, BUILDINGS AND PAVEMENTS. CONTRACTOR IS RESPONSIBLE FOR FINAL LOCATIONS OF ALL ELEMENTS FOR THE SITE. ANY REVISIONS REQUIRED AFTER COMMENCEMENT OF CONSTRUCTION, DUE TO LOCAL ADJUSTMENTS SHALL BE CORRECTED AT NO ADDITIONAL COST TO OWNER. ADJUSTMENTS TO THE LAYOUT SHALL BE APPROVED BY THE ENGINEER/LANDSCAPE ARCHITECT PRIOR TO INSTALLATION OF MATERIALS. STAKE LAYOUT FOR APPROVAL.
- THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS PRIOR TO CONSTRUCTION, INCLUDING A RIGHT-OF-WAY AND STREET OPENING PERMIT.
- THE CONTRACTOR SHALL VERIFY RECOMMENDATIONS NOTED IN THE GEO TECHNICAL REPORT PRIOR TO INSTALLATION OF SITE IMPROVEMENT MATERIALS.
- CONTRACTOR SHALL FIELD VERIFY COORDINATES AND LOCATION DIMENSIONS OF THE BUILDING AND STAKE FOR REVIEW AND APPROVAL BY THE OWNERS REPRESENTATIVE PRIOR TO INSTALLATION OF FOOTING MATERIALS.
- LOCATIONS OF STRUCTURES, ROADWAY PAVEMENTS, CURBS AND GUTTERS, BOLLARDS, AND WALKS ARE APPROXIMATE AND SHALL BE STAKED IN THE FIELD, PRIOR TO INSTALLATION, FOR REVIEW AND APPROVAL BY THE ENGINEER/LANDSCAPE ARCHITECT.
- CURB DIMENSIONS SHOWN ARE TO FACE OF CURB. BUILDING DIMENSIONS ARE TO FACE OF CONCRETE FOUNDATION. LOCATION OF BUILDING IS TO BUILDING FOUNDATION AND SHALL BE AS SHOWN ON THE DRAWINGS.
- THE CONTRACTOR SHALL SUBMIT SHOP DRAWINGS OR SAMPLES AS SPECIFIED FOR REVIEW AND APPROVAL BY THE ENGINEER/LANDSCAPE ARCHITECT PRIOR TO FABRICATION FOR ALL PREFABRICATED SITE IMPROVEMENT MATERIALS SUCH AS, BUT NOT LIMITED TO THE FOLLOWING: FURNISHINGS, PAVEMENTS, WALLS, RAILINGS, BENCHES, FLAGPOLES, LANDING PADS FOR CURB RAMPS, AND LIGHT AND POLES. THE OWNER RESERVES THE RIGHT TO REJECT INSTALLED MATERIALS NOT PREVIOUSLY APPROVED.
- PEDESTRIAN CURB RAMPS SHALL BE CONSTRUCTED WITH TRUNCATED DOME LANDING AREAS IN ACCORDANCE WITH A.D.A. REQUIREMENTS-SEE DETAIL.
- CROSSWALK STRIPING SHALL BE 24" WIDE WHITE PAINTED LINE, SPACED 48" ON CENTER PERPENDICULAR TO THE FLOW OF TRAFFIC. WIDTH OF CROSSWALK SHALL BE 5' WIDE. ALL OTHER PAVEMENT MARKINGS SHALL BE WHITE IN COLOR UNLESS OTHERWISE NOTED OR REQUIRED BY ADA OR LOCAL GOVERNING BODIES.
- CURB AND GUTTER TYPE SHALL BE B612 UNLESS OTHERWISE NOTED ON THE DRAWINGS-TAPER BETWEEN CURB TYPES-SEE DETAIL.
- ALL CURB RADII ARE MINIMUM 3' UNLESS OTHERWISE NOTED.
- CONTRACTOR SHALL REFER TO FINAL PLAT FOR LOT BOUNDARIES, NUMBERS, AREAS AND DIMENSIONS PRIOR TO SITE IMPROVEMENTS.
- FIELD VERIFY ALL EXISTING SITE CONDITIONS, DIMENSIONS.
- PARKING IS TO BE SET PARALLEL OR PERPENDICULAR TO EXISTING BUILDING UNLESS NOTED OTHERWISE.
- ALL PARKING LOT PAINT STRIPING TO BE WHITE, 4" WIDE TYP.
- BITUMINOUS PAVING TO BE "LIGHT DUTY" UNLESS OTHERWISE NOTED. SEE DETAIL SHEETS FOR PAVEMENT SECTIONS.
- ALL TREES THAT ARE TO REMAIN ARE TO BE PROTECTED FROM DAMAGE WITH A CONSTRUCTION FENCE AT THE DRIP LINE. SEE LANDSCAPE DOCUMENTS.

SITE AREA CALCULATIONS:

	EXISTING - TOTAL	
BUILDING COVERAGE	12,758 SF	14.4%
PAVEMENTS	550 SF	0.6%
GRAVEL	26,054 SF	29.5%
ALL NON-PAVEMENTS	49,090 SF	55.5%
PROPOSED - TOTAL		
BUILDING COVERAGE	55,351 SF	62.6%
PAVEMENTS	12,858 SF	14.5%
BITUMINOUS TRAIL	8,255 SF	9.3%
ALL NON-PAVEMENTS	11,988 SF	13.6%
TOTAL SITE AREA	88,452 SF	100.0%
IMPERVIOUS SURFACE		
EXISTING CONDITION	39,362 SF	44.5%
PROPOSED CONDITION WITHOUT TRAIL	68,209 SF	77.1%
PROPOSED CONDITION WITH TRAIL	76,464 SF	86.4%
DIFFERENCE	28,847 SF	32.6%

OWNER INFORMATION

OWNER: ELSEY PARTNERS, LLC
 1532 COLLEGE AVENUE, F-19
 MANHATTAN, KS 66502

OPERATIONAL NOTES:

- ALL SNOW WILL BE REMOVED FROM SITE AT EACH PLOWING - TEMPORARY SNOW STORAGE LOCATIONS ARE SHOWN ON THE PLAN.
- TRASH REMOVAL AND REGULAR DELIVERIES SHALL OCCUR IN THE NORTH SIDE OF THE PROPOSED BUILDING IN THE AREA IDENTIFIED AS THE SWCP

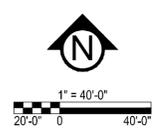
CITY COORDINATION NOTES:

- TRAFFIC & PARKING
 - COORDINATE WITH BILL PRINCE, CITY OF MINNEAPOLIS, (612) 673-3901 FOR ALL WORK REGARDING STREET LIGHTING AND ELECTRICAL SYSTEMS. STREET LIGHTING INSTALLED AS PART OF THE PROJECT SHALL BE INSPECTED BY THE CITY, CONTACT DAVE PREHALL (612) 673-5759. ANY LIGHTING INSTALLATIONS NOT MEETING CITY SPECIFICATIONS WILL BE REQUIRED TO BE REINSTALLED AT CONTRACTOR'S EXPENSE.
 - COORDINATE WITH BOB BOBLETT CITY OF MINNEAPOLIS, (612) 673-2428 FOR ALL ISSUES REGARDING WORK IN AND ADJACENT TO CITY RIGHTS-OF-WAY. AN ENROACHMENT PERMIT SHALL BE REQUIRED FOR ALL STREETScape ELEMENTS IN THE PUBLIC ROW. ANY ELEMENTS OF AN EARTH RETENTION SYSTEM AND RELATED OPERATIONS WILL REQUIRE AN ENROACHMENT PERMIT. ANY EXCAVATIONS THAT FALL WITHIN THE PUBLIC ROW WILL REQUIRE A ROW EXCAVATION PERMIT.
 - COORDINATE WITH SCOTT KRAMER, CITY OF MINNEAPOLIS, (612) 673-2383 REGARDING ANY WORK THAT IS PERFORMED IN THE RIGHT-OF-WAY. AN OBSTRUCTION PERMIT WILL BE REQUIRED FOR ALL WORK IN THE RIGHT-OF-WAY. CONTACT SCOTT KRAMER REGARDING DETAILS OF SIDEWALK AND LANE CLOSURES.
 - COORDINATE WITH CRAIG PINKALLA, CITY OF MINNEAPOLIS, (612) 499-9233 FOR ALL WORK REGARDING REMOVAL OR PROTECTION OF TREES DURING CONSTRUCTION IN THE CITY RIGHT-OF-WAY.
 - COORDINATE WITH PAUL CAO, CITY OF MINNEAPOLIS, (612) 673-2943 FOR ALL WORK REGARDING BIKE RACKS IN THE CITY RIGHT-OF-WAY.
 - COORDINATE WITH ALLAN KLUGMAN, CITY OF MINNEAPOLIS, (612) 673-5750 PRIOR TO CONSTRUCTION FOR THE TEMPORARY REMOVAL/RELOCATION OF ANY CITY OF MINNEAPOLIS SIGNAL SYSTEM. ALL COSTS FOR RELOCATION AND/OR REPAIR OF CITY TRAFFIC SHALL BE BORNE BY THE CONTRACTOR.
 - COORDINATE WITH DOUG MADAY, CITY OF MINNEAPOLIS, (612) 673-5755 PRIOR TO CONSTRUCTION FOR THE REMOVAL OR RELOCATION OF ANY CITY OF MINNEAPOLIS RIGHT-OF-WAY SIGNS.
- ENVIRONMENTAL HEALTH
 - COORDINATE WITH ENVIRONMENTAL SERVICES, CITY OF MINNEAPOLIS, (612) 673-3867 FOR PERMITS RELATING TO AFTER HOURS WORK. TEMPORARY STORAGE OF IMPACTED SOILS ON SITE PRIOR TO DISPOSAL OR REUSE, REMEDIATION OF CONTAMINATED SOIL AND GROUNDWATER, REUSE OF IMPACTED SOILS ON SITE. DEWATERING AND DISCHARGE OF ACCUMULATED STORM WATER OR GROUND WATER TO CITY SEWERS, FLAMMABLE WASTE TRAPS, UNDERGROUND OR ABOVEGROUND TANK INSTALLATION OR REMOVAL, WELL CONSTRUCTION OR SEALING, OR ON-SITE ROCK CRUSHING.
 - NO CONSTRUCTION, DEMOLITION OR COMMERCIAL POWER MAINTENANCE EQUIPMENT SHALL BE OPERATED WITHIN THE CITY BETWEEN THE HOURS OF 6:00 PM AND 7:00 AM ON WEEKDAYS OR DURING ANY HOURS ON SATURDAYS, SUNDAYS AND STATE AND FEDERAL HOLIDAYS, EXCEPT UNDER PERMIT.
 - IF CONTAMINATED SOIL IS ENCOUNTERED, IT MUST BE REPORTED TO THE MINNESOTA DUTY OFFICER AT (651) 649-5451. PREAPPROVAL FOR REMOVAL OR REUSE MUST OCCUR FROM THE MPCA AND THE CITY OF MINNEAPOLIS.
- FORESTRY DEPARTMENT
 - TO PROTECT ROOT ZONES, NO CONSTRUCTION EQUIPMENT OR MATERIALS SHALL BE PLACED, PARKED, OR STORED ON ANY UNPAVED AREA WITHIN THE DRIP LINE OF ANY CITY OWNED TREE. NO CHEMICALS OR PETROLEUM PRODUCTS SHALL BE DEPOSITED ON ANY UNPAVED AREA IN THE CITY RIGHT-OF-WAY.
 - ANY TREE ROOTS ENCOUNTERED ARE TO BE CLEANLY CUT USING HAND TOOLS.
 - NO OPEN EXCAVATION OR BORE PITS ALLOWED WITHIN 8 FEET OF CITY STREET TREES.
 - CARE SHALL BE TAKEN NOT TO DAMAGE TREE TRUNKS OR BRANCHES. CONTRACTOR MUST CONTACT FORESTRY DEPARTMENT INSPECTION AT (612) 499-9233 AT LEAST 3 DAYS PRIOR TO STARTING WORK TO DISCUSS PROBLEMS OF OVERHANGING BRANCHES THAT MAY BE DAMAGED.

SITE PLAN LEGEND:

- CONCRETE PAVEMENT AS SPECIFIED (PAD OR WALK)
- PROPERTY LINE
- CURB AND GUTTER-SEE NOTES (T.O.) TIP OUT GUTTER, WHERE APPLICABLE-SEE PLAN
- TRAFFIC DIRECTIONAL ARROWS
- SIGN AND POST ASSEMBLY. SHOP DRAWINGS REQUIRED.
 HC = ACCESSIBLE SIGN
 NP = NO PARKING FIRE LANE
 ST = STOP
 CP = COMPACT CAR PARKING ONLY

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 (800) 252-1166 TOLL FREE
 (651) 454-0002 LOCAL



CivilSite GROUP
 4931 W. 35TH ST. SUITE 200
 ST. LOUIS PARK, MN 55416
 CivilSiteGroup.com
 Matt Pavak 763-213-3944 Pat Sarver 952-250-2003

PROJECT
ESSEX AND 27TH AVENUE SE REDEVELOPMENT
 117 27TH AVENUE SE, MINNEAPOLIS, MN
ELSEY PARTNERS, LLC
 1532 COLLEGE AVENUE, F-19, MANHATTAN, KS 66502

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

 Matthew R. Pavak
 DATE 11/06/15 LICENSE NO. 44263

ISSUE/SUBMITTAL SUMMARY

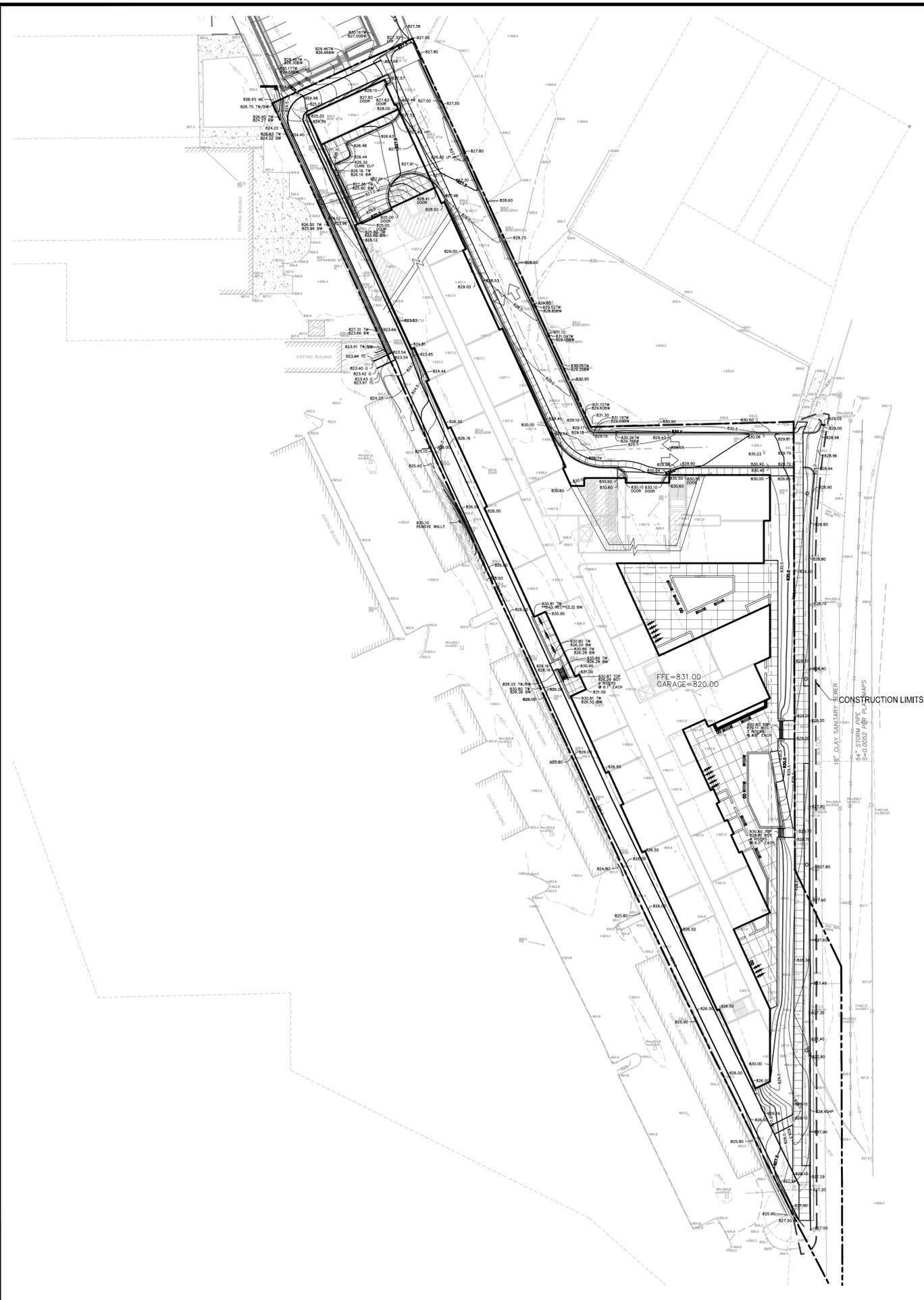
DATE	DESCRIPTION
10/16/15	PDR SUBMITTAL
11/06/15	LAND USE SUBMITTAL

REVISION SUMMARY

DATE	DESCRIPTION

PROJECT NUMBER: 15033

SITE PLAN
C2.0



NON STORM WATER DISCHARGES:

THERE ARE NO KNOWN NON-STORM WATER DISCHARGES ON THE EXISTING SITE AND NONE ARE PROPOSED AS PART OF THIS DEVELOPMENT.

GROUNDWATER STATEMENT:

THIS PROJECT DOES NOT PROPOSE ANY PERMANENT GROUNDWATER DISCHARGE TO THE STORM WATER SYSTEM.

OWNER, DEVELOPER, CONTRACTOR SIGNED STATEMENT

ALL CLEARING, GRADING, CONSTRUCTION OR DEVELOPMENT WILL BE DONE PURSUANT TO THE PLAN, SIGNED BY PARTIES BELOW:

OWNER: _____

DEVELOPER: _____

CONTRACTOR: _____

GENERAL GRADING NOTES:

- SEE SITE PLAN FOR HORIZONTAL LAYOUT & GENERAL GRADING NOTES.
- THE CONTRACTOR SHALL COMPLETE THE SITE GRADING CONSTRUCTION (INCLUDING BUT NOT LIMITED TO SITE PREPARATION, SOIL CORRECTION, EXCAVATION, EMBANKMENT, ETC.) IN ACCORDANCE WITH THE REQUIREMENTS OF THE OWNER'S SOILS ENGINEER. ALL SOIL TESTING SHALL BE COMPLETED BY THE OWNER'S SOILS ENGINEER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL REQUIRED SOIL TESTS AND INSPECTIONS WITH THE SOILS ENGINEER.
- GRADING AND EXCAVATION ACTIVITIES SHALL BE PERFORMED IN ACCORDANCE WITH THE NATIONAL POLLUTION DISCHARGE ELIMINATION SYSTEM (NPDES) PERMIT REQUIREMENTS & PERMIT REQUIREMENTS OF THE CITY.
- PROPOSED SPOT GRADES ARE FLOW-LINE FINISHED GRADE ELEVATIONS, UNLESS OTHERWISE NOTED.
- GRADES OF WALKS SHALL BE INSTALLED WITH 5% MAX. LONGITUDINAL SLOPE AND 1% MIN. AND 2% MAX. CROSS SLOPE, UNLESS OTHERWISE NOTED.
- PROPOSED SLOPES SHALL NOT EXCEED 3:1 UNLESS INDICATED OTHERWISE ON THE DRAWINGS. MAXIMUM SLOPES IN MAINTAINED AREAS IS 4:1
- PROPOSED RETAINING WALLS, FREESTANDING WALLS, OR COMBINATION OF WALL TYPES GREATER THAN 4' IN HEIGHT SHALL BE DESIGNED AND ENGINEERED BY A REGISTERED RETAINING WALL ENGINEER. DESIGN DRAWINGS SHALL BE SUBMITTED FOR REVIEW AND APPROVAL PRIOR TO CONSTRUCTION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE OF GRADE STAKES THROUGHOUT THE DURATION OF CONSTRUCTION TO ESTABLISH PROPER GRADES. THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR A FINAL FIELD CHECK OF FINISHED GRADES ACCEPTABLE TO THE ENGINEER/LANDSCAPE ARCHITECT PRIOR TO TOPSOIL AND SODDING ACTIVITIES.
- IF EXCESS OR SHORTAGE OF SOIL MATERIAL EXISTS, THE CONTRACTOR SHALL TRANSPORT ALL EXCESS SOIL MATERIAL OFF THE SITE TO AN AREA SELECTED BY THE CONTRACTOR, OR IMPORT SUITABLE MATERIAL TO THE SITE.
- EXCAVATE TOPSOIL FROM AREAS TO BE FURTHER EXCAVATED OR REGRADED AND STOCKPILE IN AREAS DESIGNATED ON THE SITE. THE CONTRACTOR SHALL SALVAGE ENOUGH TOPSOIL FOR RESPREADING ON THE SITE AS SPECIFIED. EXCESS TOPSOIL SHALL BE PLACED IN EMBANKMENT AREAS, OUTSIDE OF BUILDING PADS, ROADWAYS AND PARKING AREAS. THE CONTRACTOR SHALL SUBCUT CUT AREAS, WHERE TURF IS TO BE ESTABLISHED, TO A DEPTH OF 6 INCHES. RESPREAD TOPSOIL IN AREAS WHERE TURF IS TO BE ESTABLISHED TO A MINIMUM DEPTH OF 6 INCHES.
- FINISHED GRADING SHALL BE COMPLETED. THE CONTRACTOR SHALL UNIFORMLY GRADE AREAS WITHIN LIMITS OF GRADING, INCLUDING ADJACENT TRANSITION AREAS. PROVIDE A SMOOTH FINISHED SURFACE WITHIN SPECIFIED TOLERANCES, WITH UNIFORM LEVELS OR SLOPES BETWEEN POINTS WHERE ELEVATIONS ARE SHOWN, OR BETWEEN SUCH POINTS AND EXISTING GRADES. AREAS THAT HAVE BEEN FINISH GRADED SHALL BE PROTECTED FROM SUBSEQUENT CONSTRUCTION OPERATIONS, TRAFFIC AND EROSION. REPAIR ALL AREAS THAT HAVE BECOME RUTTED BY TRAFFIC OR ERODED BY WATER OR HAS SETTLED BELOW THE CORRECT GRADE. ALL AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED TO EQUAL OR BETTER THAN ORIGINAL CONDITION OR TO THE REQUIREMENTS OF THE NEW WORK.
- PRIOR TO PLACEMENT OF THE AGGREGATE BASE, A TEST ROLL WILL BE REQUIRED ON THE STREET AND/OR PARKING AREA SUBGRADE. THE CONTRACTOR SHALL PROVIDE A LOADED TANDEM AXLE TRUCK WITH A GROSS WEIGHT OF 25 TONS. THE TEST ROLLING SHALL BE AT THE DIRECTION OF THE SOILS ENGINEER AND SHALL BE COMPLETED IN AREAS AS DIRECTED BY THE SOILS ENGINEER. THE SOILS ENGINEER SHALL DETERMINE WHICH SECTIONS OF THE STREET OR PARKING AREA ARE UNSTABLE. CORRECTION OF THE SUBGRADE SOILS SHALL BE COMPLETED IN ACCORDANCE WITH THE REQUIREMENTS OF THE SOILS ENGINEER.
- TOLERANCES
 - THE BUILDING SUBGRADE FINISHED SURFACE ELEVATION SHALL NOT VARY BY MORE THAN 0.30 FOOT ABOVE, OR 0.30 FOOT BELOW, THE PRESCRIBED ELEVATION AT ANY POINT WHERE MEASUREMENT IS MADE.
 - THE STREET OR PARKING AREA SUBGRADE FINISHED SURFACE ELEVATION SHALL NOT VARY BY MORE THAN 0.05 FOOT ABOVE, OR 0.10 FOOT BELOW, THE PRESCRIBED ELEVATION OF ANY POINT WHERE MEASUREMENT IS MADE.
 - AREAS WHICH ARE TO RECEIVE TOPSOIL SHALL BE GRADED TO WITHIN 0.30 FOOT ABOVE OR BELOW THE REQUIRED ELEVATION, UNLESS DIRECTED OTHERWISE BY THE ENGINEER.
 - TOPSOIL SHALL BE GRADED TO PLUS OR MINUS 1/2 INCH OF THE SPECIFIED THICKNESS.
- MAINTENANCE
 - THE CONTRACTOR SHALL PROTECT NEWLY GRADED AREAS FROM TRAFFIC AND EROSION, AND KEEP AREA FREE OF TRASH AND DEBRIS.
 - CONTRACTOR SHALL REPAIR AND REESTABLISH GRADES IN SETTLED, ERODED AND RUTTED AREAS TO SPECIFIED TOLERANCES. DURING THE CONSTRUCTION, IF REQUIRED, AND DURING THE WARRANTY PERIOD, ERODED AREAS WHERE TURF IS TO BE ESTABLISHED SHALL BE RESEED AND MULCHED.
 - WHERE COMPLETED COMPACTED AREAS ARE DISTURBED BY SUBSEQUENT CONSTRUCTION OPERATIONS OR ADVERSE WEATHER, CONTRACTOR SHALL SCARIFY, SURFACE, RESHAPE, AND COMPACT TO REQUIRED DENSITY PRIOR TO FURTHER CONSTRUCTION.

CITY OF MINNEAPOLIS GRADING NOTES:

- RESERVED FOR CITY SPECIFIC GRADING NOTES.

EROSION CONTROL NOTES:

SEE SWPPP ON SHEETS SW1.0-SW1.5

GROUNDWATER INFORMATION:

PER GEOTECHNICAL REPORT BY HAUGO GEOTECHNICAL SERVICES, LLC., DATED 4-27-2015 AND GROUNDWATER SUMMARY DATED 10-12-2015. GROUNDWATER ELEVATIONS ARE ESTIMATED AT ELEVATIONS RANGING FROM 816.0 TO 817.5 ACROSS THE SITE.

THE PIEZOMETER NUMBER & GROUNDWATER ELEVATIONS ARE AS FOLLOWS:

- SB-8 817.50
- SB-9 816.60
- SB-10 816.60

PLEASE SEE REPORT FOR MORE DETAILED GROUNDWATER INFORMATION.

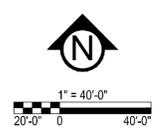
BASED ON THE REPORT BY HAUGO GEOTECHNICAL SERVICES, THEY RECOMMEND THE LOWEST FLOOR SLAB TO BE 2 FEET ABOVE THE GROUNDWATER LEVEL. THE LOWEST FLOOR ELEVATION WILL BE CONSTRUCTED AT OR ABOVE AN ELEVATION OF 819.5. PROPOSED FOOTING DRAIN TILE INVERT ELEVATIONS WILL BE ABOVE THE ESTIMATED SEASONAL HIGH GROUND WATER ELEVATION OF THE SITE. PLEASE SEE ARCHITECTURAL PLANS FOR LOW FLOOR ELEVATIONS, FLOOR DRAIN ELEVATIONS AND DRAIN TILE ELEVATIONS.

NO PERMANENT GROUNDWATER DISCHARGES WILL OCCUR FROM THIS SITE.

GRADING PLAN LEGEND:

- 891 --- EX. 1' CONTOUR ELEVATION INTERVAL
- 819 — 1.0' CONTOUR ELEVATION INTERVAL
- 891.0 SPOT GRADE ELEVATION (FLOW LINE UNLESS OTHERWISE NOTED)
- 891.0 G SPOT GRADE ELEVATION GUTTER
- 891.0 BC SPOT GRADE ELEVATION BACK OF CURB (TOP OF CURB)
- 891.0 BS/TS SPOT GRADE ELEVATION BOTTOM OF STAIRS/TOP OF STAIRS
- T.O. TIP OUT (T.O.) CURB AND GUTTER WHERE APPLICABLE - TAPER GUTTERS TO DRAIN AS SHOWN
- EXISTING AND PROPOSED DRAINAGE ARROWS

GOPHER STATE ONE CALL
 WWW.GOPHERSTATEONECALL.ORG
 (800) 252-1166 TOLL FREE
 (651) 454-0002 LOCAL



CivilSite GROUP
 4931 W. 35TH ST. SUITE 200
 ST. LOUIS PARK, MN 55416
 CivilSiteGroup.com
 Matt Pavak 763-213-3944 Pat Sarver 952-250-2003

PROJECT
ESSEX AND 27TH AVENUE SE REDEVELOPMENT
 117 27TH AVENUE SE, MINNEAPOLIS, MN
ELSEY PARTNERS, LLC
 1532 COLLEGE AVENUE, F-19, MANHATTAN, KS 66502

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Matthew R. Pavak
 Matthew R. Pavak
 DATE 11/06/15 LICENSE NO. 44263

ISSUE/SUBMITTAL SUMMARY	
DATE	DESCRIPTION
10/16/15	PDR SUBMITTAL
11/06/15	LAND USE SUBMITTAL
REVISION SUMMARY	
DATE	DESCRIPTION

PROJECT NUMBER: 15033
GRADING PLAN
C3.0

PROJECT
ESSEX AND 27TH AVENUE SE REDEVELOPMENT

117 27TH AVENUE SE, MINNEAPOLIS, MN

ELSEY PARTNERS, LLC

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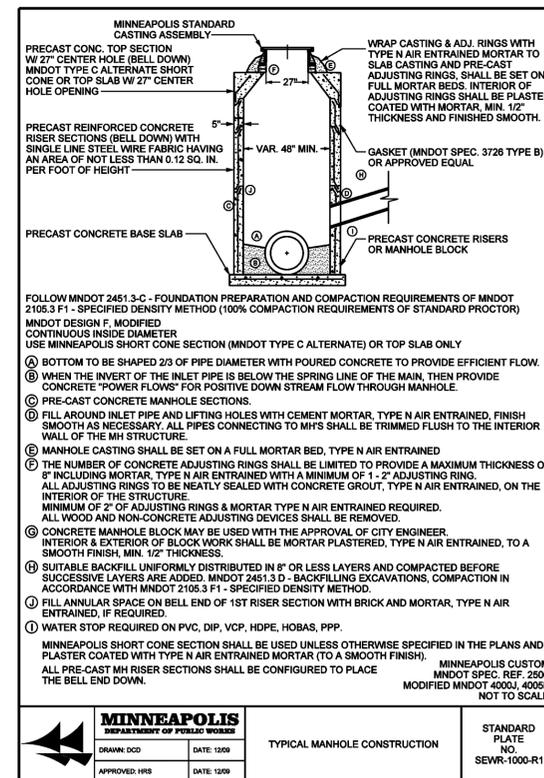
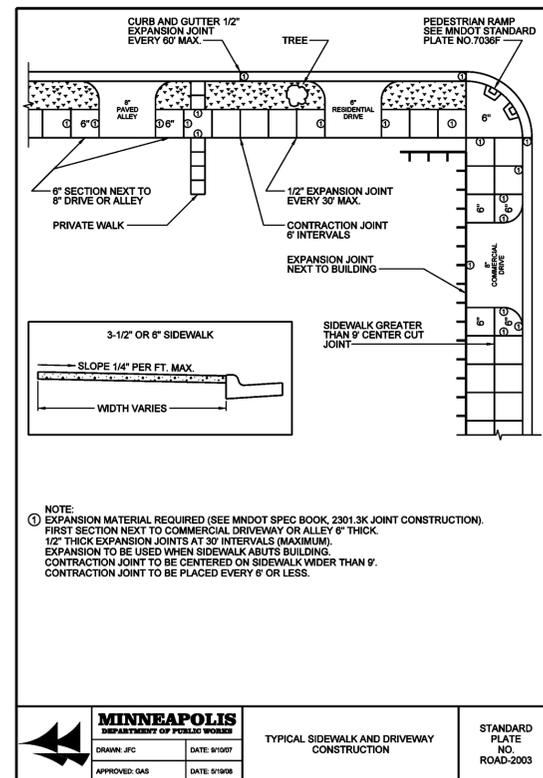
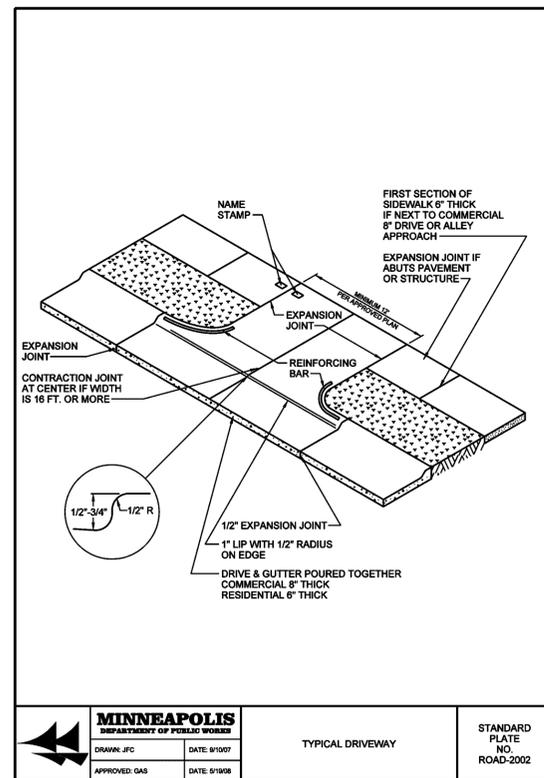
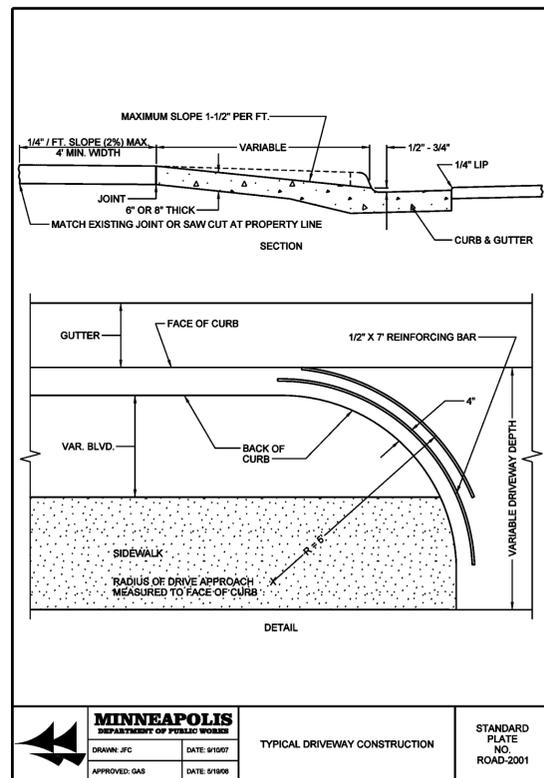
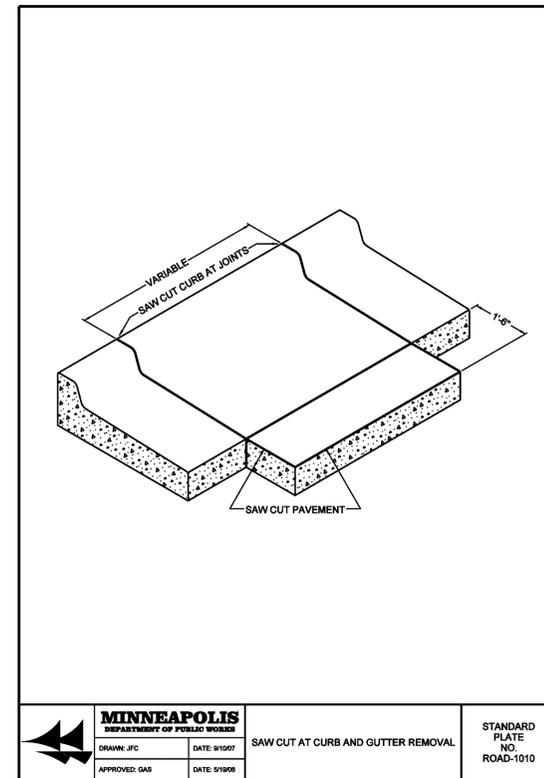
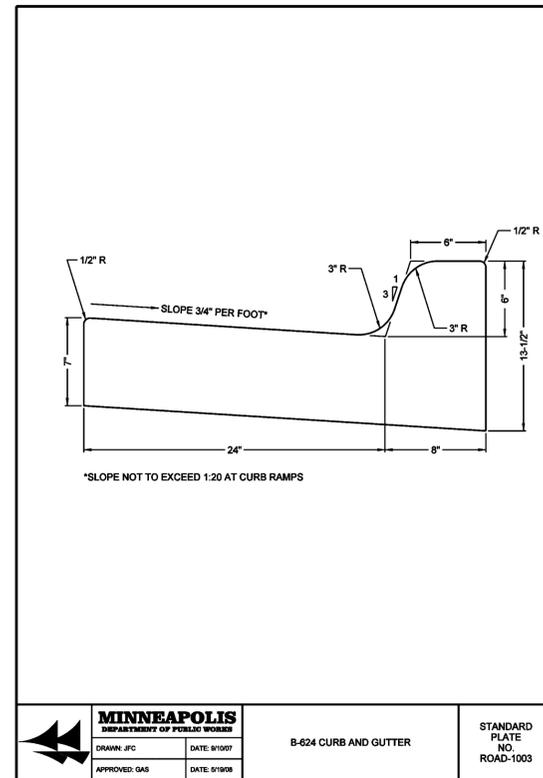
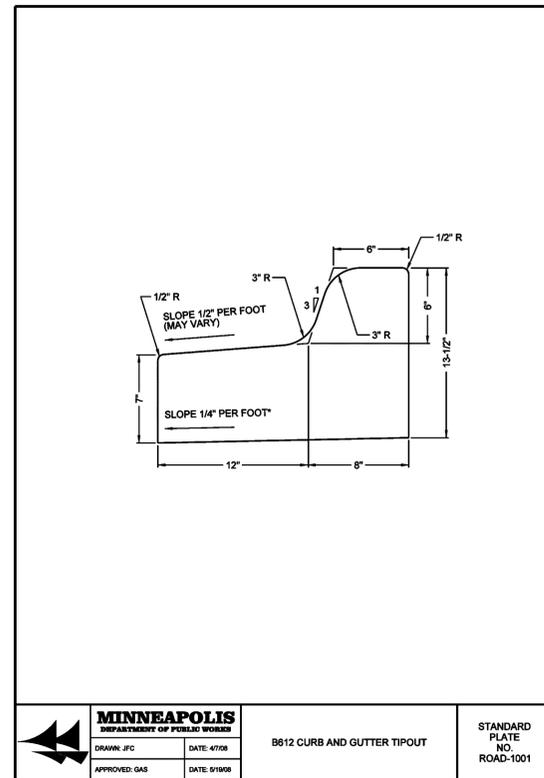
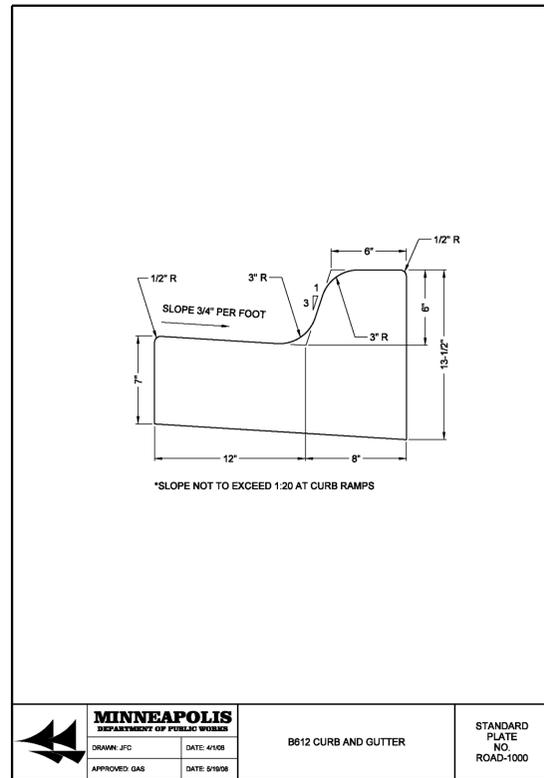
ISSUE/SUBMITTAL SUMMARY	
DATE	DESCRIPTION
10/16/15	PDR SUBMITTAL
11/06/15	LAND USE SUBMITTAL

REVISION SUMMARY	
DATE	DESCRIPTION

PROJECT NUMBER: 15033	
-----------------------	--

CIVIL DETAILS

C5.0



STRUCTURE	PRECAST CONCRETE BASE				WT. (lbs.)	
	SIZE (in.)	OUTSIDE DIA. (in.)	T" (in.)	MIN. REINFORCEMENT (in.)		
				BAR NUMBER	SPACING (in.)	
30	44	6	#4	12	1680	
48	66	6	#4	12	1680	
60	78	8	#4	12	3320	
72	92	8	#4	12	4620	
84	106	8	#4	8	6130	
96	120	8	#4	8	7850	
108	132	10	#4	8	10690	
120	146	12	#4	8	17440	

ALL REBARS ARE IN ENGLISH DESIGNATIONS

MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS
DRAWN: DCD DATE: 2003
APPROVED: HRS DATE: 1206

MANHOLE BASE SLAB
STANDARD PLATE NO. SEWR-1003

MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS
DRAWN: DCD DATE: 1206
APPROVED: HRS DATE: 1206

PRECAST CATCH BASIN POT
STANDARD PLATE NO. SEWR-1006-R1

MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS
DRAWN: DCD DATE: 2003
APPROVED: HRS DATE: 1206

CATCH BASIN INSTALLATION
STANDARD PLATE NO. SEWR-1009

MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS
DRAWN: DCD DATE: 1206
APPROVED: HRS DATE: 1206

CONCRETE ADJUSTING RINGS
STANDARD PLATE NO. SEWR-1011-R1

MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS
DRAWN: DCD DATE: 1212
APPROVED: JMM DATE: 1212

STANDARD CATCH BASIN CONSTRUCTION
STANDARD PLATE NO. SEWR-1017

MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS
DRAWN: ZTT DATE: 1202
APPROVED: HRS DATE: 1206

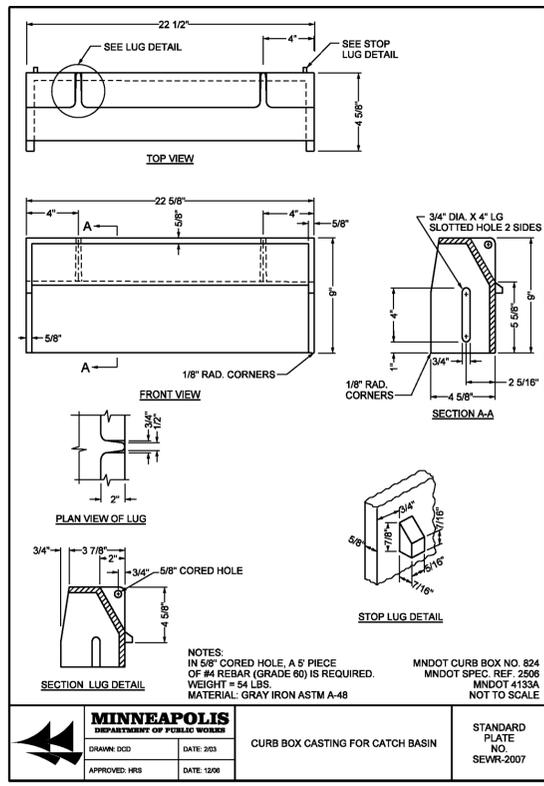
CIRCULAR VANE GRATE
STANDARD PLATE NO. SEWR-2003

MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS
DRAWN: DCD DATE: 2007
APPROVED: HRS DATE: 2007

STANDARD MANHOLE CASTING
STANDARD PLATE NO. SEWR-2004

MINNEAPOLIS DEPARTMENT OF PUBLIC WORKS
DRAWN: DCD DATE: 2003
APPROVED: HRS DATE: 1206

CATCH BASIN CASTING
STANDARD PLATE NO. SEWR-2006

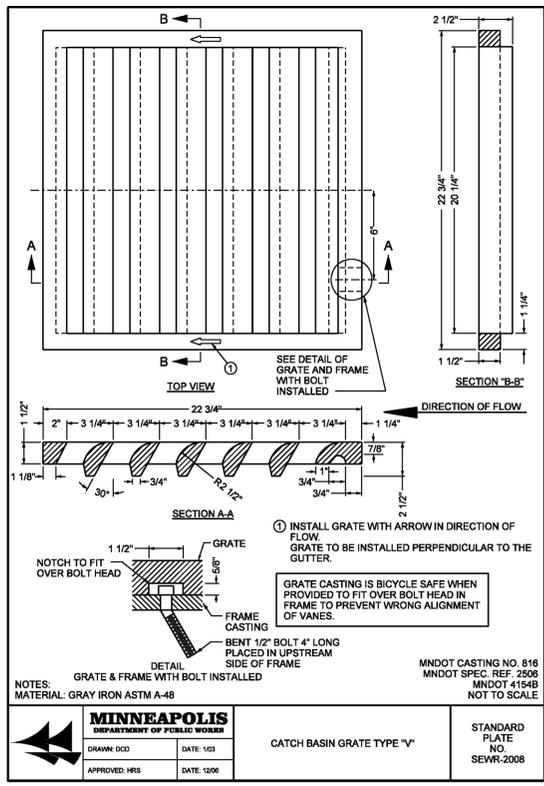


MINNEAPOLIS
DEPARTMENT OF PUBLIC WORKS

DRAWN DCD DATE: 2013
APPROVED HRS DATE: 1206

CURB BOX CASTING FOR CATCH BASIN

STANDARD PLATE NO. SEWR-2007

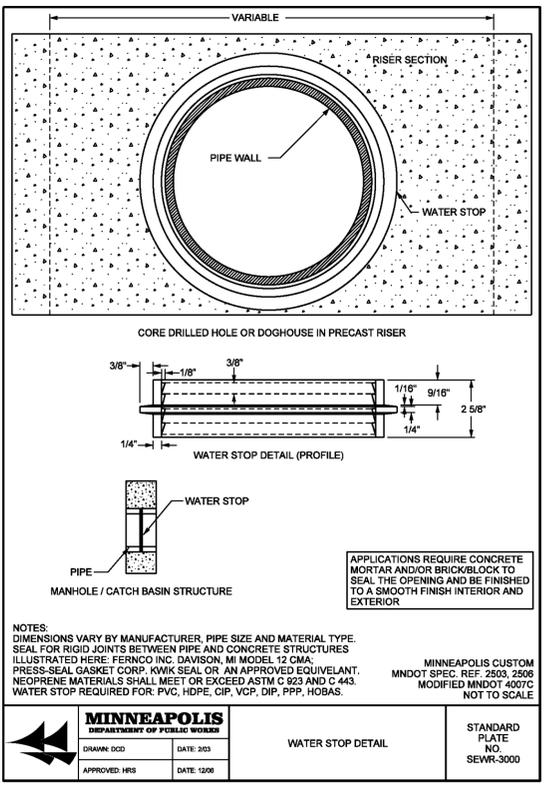


MINNEAPOLIS
DEPARTMENT OF PUBLIC WORKS

DRAWN DCD DATE: 1913
APPROVED HRS DATE: 1206

CATCH BASIN GRATE TYPE "V"

STANDARD PLATE NO. SEWR-2008

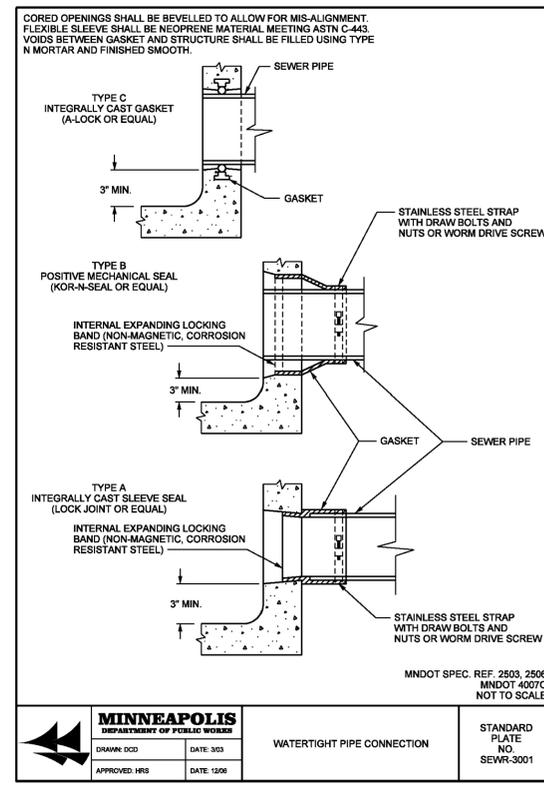


MINNEAPOLIS
DEPARTMENT OF PUBLIC WORKS

DRAWN DCD DATE: 2013
APPROVED HRS DATE: 1206

WATER STOP DETAIL

STANDARD PLATE NO. SEWR-3000

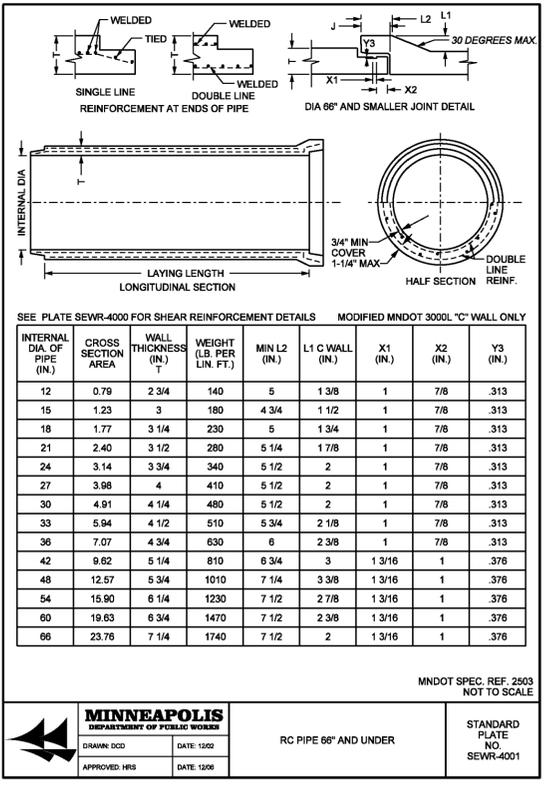


MINNEAPOLIS
DEPARTMENT OF PUBLIC WORKS

DRAWN DCD DATE: 2013
APPROVED HRS DATE: 1206

WATERTIGHT PIPE CONNECTION

STANDARD PLATE NO. SEWR-3001

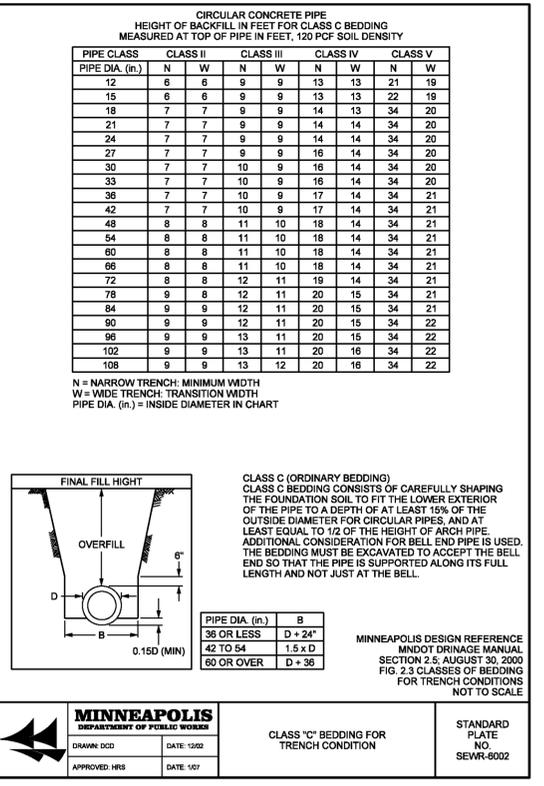


MINNEAPOLIS
DEPARTMENT OF PUBLIC WORKS

DRAWN DCD DATE: 1202
APPROVED HRS DATE: 1206

RC PIPE 66" AND UNDER

STANDARD PLATE NO. SEWR-4001

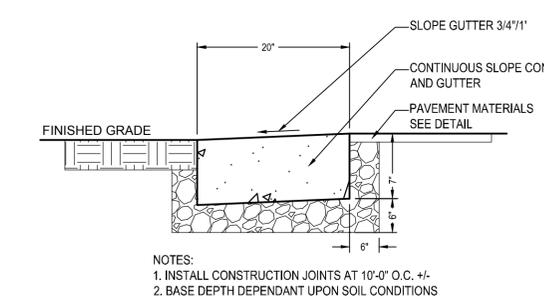


MINNEAPOLIS
DEPARTMENT OF PUBLIC WORKS

DRAWN DCD DATE: 1202
APPROVED HRS DATE: 107

CLASS "C" BEDDING FOR TRENCH CONDITION

STANDARD PLATE NO. SEWR-8002



1 RIBBON CURB
NTS

CivilSite GROUP
4931 W. 35TH ST. SUITE 200
ST. LOUIS PARK, MN 55416
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Matt Pavak 763-213-3944 Pat Sarver 952-250-2003

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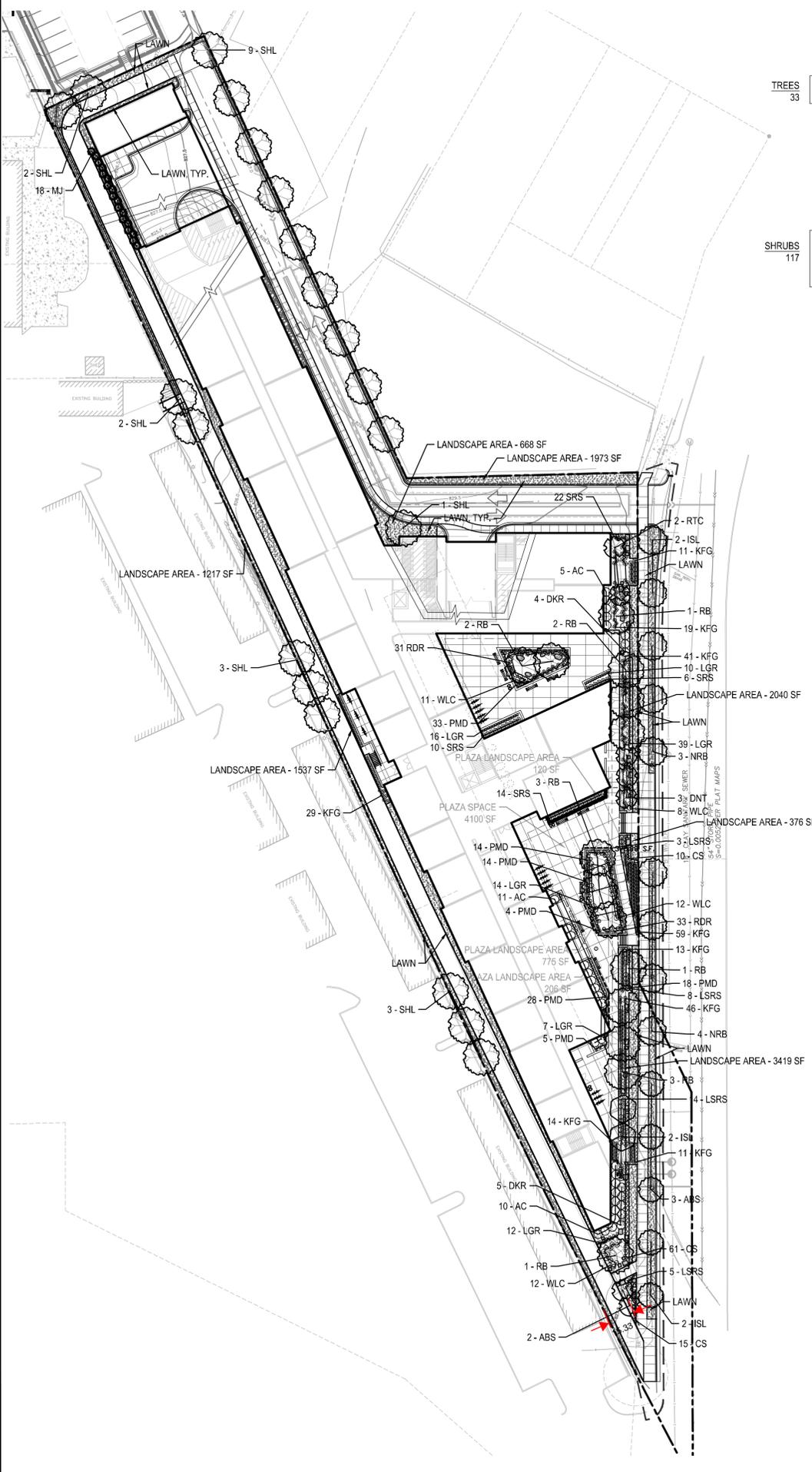
REVISION SUMMARY

DATE	DESCRIPTION
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PROJECT NUMBER: 15033

CIVIL DETAILS

C5.2



PLANT SCHEDULE

	SYM	QUANT. ON-SITE	QUANT. OFF-SITE	QUANT. PLAZA	COMMON NAME	BOTANICAL NAME	SIZE	ROOT	COMMENTS	POLLINATOR FRIENDLY	
TREES 33	DECIDUOUS TREES										
	RB	8	-	5	RIVER BIRCH CLUMP FORM	Betula nigra	2" CAL	B&B			
	SHL	20	-	-	SKYLINE HONEYLOCUST	Gleditsia triacanthos 'Skycole'	2" CAL	B&B			
ORNAMENTAL TREES											
ABS	2	3	-	-	AUTUMN BRILLIANCE SERVICEBERRY	Amelanchier x grandiflora 'Autumn Brilliance (tree form)'	1.5" CAL	B&B			
RTC	2	-	-	-	RUBY TEARS FLOWERING CRABAPPLE	Malus 'Bailears'	1.5" CAL	B&B		X	
ISL	2	4	-	-	IVORY SILK TREE LILAC (TREE FORM)	Syringa reticulata 'Ivory Silk (tree form)'	1.5" CAL	B&B		X	
NRB	-	-	7	-	NORTHERN REDBUD CLUMP	Cercis canadensis 'Northern Strain'	1.5" CAL	B&B			
DNT	3	-	-	-	DIABLO NINEBARK TREE	Physocarpus opulifolius 'Diablo (tree form)'	1.5" CAL	B&B			
DECIDUOUS & EVERGREEN SHRUBS											
AC	15	-	-	11	ALPINE CURRANT	Ribes alpinum	#5	CONT.	DENSE BRANCHING		
MJ	18	-	-	-	MEDORA JUNIPER	Juniperus scopulorum 'Medora'					
RDR	-	-	-	64	RED DRIFT ROSE	Rosa 'Red Drift'				X	
DKR	9	-	-	-	KNOCK OUT DOUBLE RED ROSE	Rosa 'Red Double Knock Out'				X	
PERENNIALS, GRASSES & GROUND COVERS											
KFG	243	-	-	-	KARL FOERSTER GRASS	Calamagrostis x acutiflora 'Karl Foerster'	#2	CONT.			
WLC	31	-	-	12	WALKER'S LOW CATMINT	Nepeta x faassenii 'Walker's Low'	#2	CONT.			
PMD	51	-	-	65	PARDON ME DAYLILY	Hemerocallis 'Pardon Me'	#2	CONT.			
SRS	22	-	-	30	SHENANDOAH RED SWITCH GRASS	Panicum virgatum 'Shenandoah'	#2	CONT.		X	
LGR	51	-	-	47	LITTLE GOLDSTAR RUDBECKIA	Rudbeckia fulgida 'Little Goldstar'	#2	CONT.		X	
LSRS	30	-	-	-	LITTLE SPIRE RUSSIAN SAGE	Perovskia 'Little Spire'	#2	CONT.		X	
CS	86	-	-	-	CARADONNA SALVIA	Salvia x sylvestris 'Caradonna'	#2	CONT.		X	

SEE SHEET L1.1 FOR LANDSCAPE E PLAN ENLARGEMENTS, INCLUDING GREEN ROOF PLANTING LAYOUT & SCHEDULE.

LANDSCAPE NOTES:

- ALL SHRUB BEDS SHALL BE MULCHED WITH 4" DEPTH (MIN. AFTER INSTALLATION AND/OR TOPDRESSING OPERATIONS) OF 3/4" BUFF LIMESTONE ROCK MULCH OVER WEED BARRIER. OWNER TO APPROVE ROCK SAMPLE PRIOR TO INSTALLATION. STEEL EDGING AS SHOWN ON PLAN, SUBMIT SAMPLE FOR APPROVAL.
- 36"-48" SIZED "TIGER STRIPE" LIMESTONE BOULDERS, GENERALLY CUBE SHAPED, TO COMPLIMENT NEW LANDSCAPING. OWNER TO APPROVE BOULDER SAMPLE PRIOR TO INSTALLATION.
- PLANT MATERIALS SHALL CONFORM WITH THE AMERICAN ASSOCIATION OF NURSERYMEN STANDARDS AND SHALL BE OF HARDY STOCK, FREE FROM DISEASE, DAMAGE AND DISFIGURATION. CONTRACTOR IS RESPONSIBLE FOR MAINTAINING PLUMBNESS OF PLANT MATERIAL FOR DURING OF ACCEPTANCE PERIOD.
- UPON DISCOVERY OF A DISCREPANCY BETWEEN THE QUANTITY OF PLANTS SHOWN ON THE SCHEDULE AND THE QUANTITY SHOWN ON THE PLAN, THE PLAN SHALL GOVERN.
- CONDITION OF VEGETATION SHALL BE MONITORED BY THE LANDSCAPE ARCHITECT THROUGHOUT THE DURATION OF THE CONTRACT. LANDSCAPE MATERIALS PART OF THE CONTRACT SHALL BE WARRANTED FOR TWO (2) FULL GROWING SEASONS FROM SUBSTANTIAL COMPLETION DATE.
- AREAS DISTURBED BY CONSTRUCTION ACTIVITIES SHALL RECEIVE 4" LAYER LOAM AND SOD AS SPECIFIED UNLESS OTHERWISE NOTED ON THE DRAWINGS.
- COORDINATE LOCATION OF VEGETATION WITH UNDERGROUND AND OVERHEAD UTILITIES, LIGHTING FIXTURES, DOORS AND WINDOWS. CONTRACTOR SHALL STAKE IN THE FIELD FINAL LOCATION OF TREES AND SHRUBS FOR REVIEW AND APPROVAL BY THE OWNER PRIOR TO INSTALLATION.
- ALL PLANT MATERIALS SHALL BE WATERED AND MAINTAINED UNTIL ACCEPTANCE.
- REPAIR AT NO COST TO OWNER ALL DAMAGE RESULTING FROM LANDSCAPE CONTRACTOR'S ACTIVITIES.
- SWEEP AND MAINTAIN ALL PAVED SURFACES FREE OF DEBRIS GENERATED FROM LANDSCAPE CONTRACTOR'S ACTIVITIES.
- REPAIR AT NO COST TO THE OWNER IRRIGATION SYSTEM DAMAGED FROM LANDSCAPE CONSTRUCTION ACTIVITIES.
- PROVIDE SITE WIDE IRRIGATION SYSTEM DESIGN AND INSTALLATION. SYSTEM SHALL BE A FULLY PROGRAMMABLE SYSTEM CAPABLE OF ALTERNATE DATE WATERING. THE SYSTEM SHALL PROVIDE HEAD TO HEAD COVERAGE AND BE CAPABLE OF DELIVERING ONE INCH OF PRECIPITATION PER WEEK. SYSTEM SHALL EXTEND INTO THE PUBLIC RIGHT-OF-WAY TO THE EDGE OF PATHWAY/BACK OF CURB.
- CONTRACTOR SHALL SECURE APPROVAL OF PROPOSED IRRIGATION SYSTEM INCLUDING PRICING FROM OWNER, PRIOR TO INSTALLATION.

IRRIGATION NOTES:

- ENTIRE SITE SHALL BE FULLY IRRIGATED. THE CONTRACTOR SHALL SUBMIT IRRIGATION SHOP DRAWINGS FOR REVIEW AND APPROVAL BY THE LANDSCAPE ARCHITECT PRIOR TO INSTALLATION.
- SEE MECHANICAL AND ELECTRICAL PLANS AND SPECIFICATIONS FOR IRRIGATION WATER, METER, AND POWER CONNECTIONS.
- CONTRACTOR TO VERIFY LOCATION OF ALL UNDERGROUND/ABOVE GROUND FACILITIES PRIOR TO ANY EXCAVATION/INSTALLATION. ANY DAMAGE TO UNDERGROUND/ABOVE GROUND FACILITIES SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND COSTS ASSOCIATED WITH CORRECTING DAMAGES SHALL BE BORNE ENTIRELY BY THE CONTRACTOR.
- SERVICE EQUIPMENT AND INSTALLATION SHALL BE PER LOCAL UTILITY COMPANY STANDARDS AND SHALL BE PER NATIONAL AND LOCAL CODES. EXACT LOCATION OF SERVICE EQUIPMENT SHALL BE COORDINATED WITH THE LANDSCAPE ARCHITECT OR EQUIVALENT AT THE JOB SITE.
- CONTRACTOR SHALL COORDINATE WITH LOCAL UTILITY COMPANY FOR THE PROPOSED ELECTRICAL SERVICE AND METERING FACILITIES.
- IRRIGATION WATER LINE CONNECTION SIZE IS 1-1/2" AT BUILDING. VERIFY WITH MECHANICAL PLANS/COVERAGE.
- ALL MAIN LINES SHALL BE 18" BELOW FINISHED GRADE.
- ALL LATERAL LINES SHALL BE 12" BELOW FINISHED GRADE.
- CONTRACTOR SHALL LAY ALL SLEEVES AND CONDUIT AT 2'-0" BELOW THE FINISHED GRADE OF THE TOP OF PAVEMENT. EXTEND SLEEVES TO 2'-0" BEYOND PAVEMENT.
- CONTRACTOR SHALL MARK THE LOCATION OF ALL SLEEVES AND CONDUIT WITH THE SLEEVING MATERIAL "ELLED" TO 2'-0" ABOVE FINISHED GRADE AND CAPPED.
- FABRICATE ALL PIPE TO MANUFACTURE'S SPECIFICATIONS WITH CLEAN AND SQUARE CUT JOINTS. USE QUALITY GRADE PRIMER AND SOLVENT CEMENT FORMULATED FOR INTENDED TYPE OF CONNECTION.
- BACKFILL ALL TRENCHES WITH SOIL FREE OF SHARP OBJECTS AND DEBRIS.
- ALL VALVE BOXES AND COVERS SHALL BE BLACK IN COLOR.
- GROUP VALVE BOXES TOGETHER FOR EASE WHEN SERVICE IS REQUIRED. LOCATE IN PLANT BED AREAS WHENEVER POSSIBLE.
- IRRIGATION CONTROLLER LOCATION SHALL BE VERIFIED ON-SITE WITH OWNER'S REPRESENTATIVE.
- CONTROL WIRES: 14 GAUGE DIRECT BURIAL, SOLID COPPER IRRIGATION WIRE. RUN UNDER MAIN LINE. USE MOISTURE-PROOF SPLICES AND SPLICE ONLY AT VALVES OR PULL BOXES. RUN SEPARATE HOT AND COMMON WIRE TO EACH VALVE AND ONE (1) SPARE WIRE AND GROUND TO FURTHEST VALVE FROM CONTROLLER. LABEL OR COLOR CODE ALL WIRES.
- AVOID OVERSPRAY ON BUILDINGS, PAVEMENT, WALLS AND ROADWAYS BY INDIVIDUALLY ADJUSTING RADIUS OR ARC ON SPRINKLER HEADS AND FLOW CONTROL ON AUTOMATIC VALVE.
- ADJUST PRESSURE REGULATING VALVES FOR OPTIMUM PRESSURE ON SITE.
- USE SCREENS ON ALL HEADS.
- A SET OF AS-BUILT DRAWINGS SHALL BE MAINTAINED ON-SITE AT ALL TIMES IN AN UPDATED CONDITION.
- ALL PIPE 3" AND OVER SHALL HAVE THRUST BLOCKING AT EACH TURN.
- ALL AUTOMATIC REMOTE CONTROL VALVES WILL HAVE 3" MINIMUM DEPTH OF 3/4" WASHED GRAVEL UNDERNEATH VALVE AND VALVE BOX. GRAVEL SHALL EXTEND 3" BEYOND PERIMETER OF VALVE BOX.
- THERE SHALL BE 3" MINIMUM SPACE BETWEEN BOTTOM OF VALVE BOX COVER AND TOP OF VALVE STRUCTURE.

SITE FURNISHINGS NOTES:

- PLAZA SHALL HAVE MOUNTED BENCHES AS SHOWN ON PLAN, AS WELL AS MOVABLE TABLES AND CHAIRS. TOTAL NUMBER, STYLE, MAKE & MODEL, AND FINAL LOCATIONS SHALL BE PER FF&E PLAN AND BE APPROVED BY CITY PRIOR TO CONSTRUCTION.
- PLAZA SHALL HAVE TRASH AND RECYCLING BINS TO MATCH FURNISHING TOTAL NUMBER, STYLE, MAKE & MODEL, AND FINAL LOCATIONS SHALL BE PER FF&E PLAN AND BE APPROVED BY CITY PRIOR TO CONSTRUCTION.
- UTILITY BOXES: ALL UTILITY BOXES (ON-SITE OR ADJACENT) SHALL RECEIVE AN ADHESIVE VINYL "ART WRAP". FINAL ARTWORK SHALL BE DETERMINED AT TIME OF CONSTRUCTION AND UNDER THE ADVICE OF THE CITY AND/OR NEIGHBORHOOD BOARD.

LANDSCAPE CALCULATIONS:

PROPOSED LANDSCAPE AREA CALCULATION:
SITE AREA - BUILDING COVERAGE = OPEN SPACE

88,452.0 SF - 55,351.0 SF = 33,101.0 SF

20% OF OPEN SPACE = LANDSCAPE AREA

20% X 33,101.0 SF = 6,620.2 SF REQUIRED
36% X 33,101.0 SF = 11,988.0 SF PROVIDED

77.1% PROPOSED IMPERVIOUS RATIO

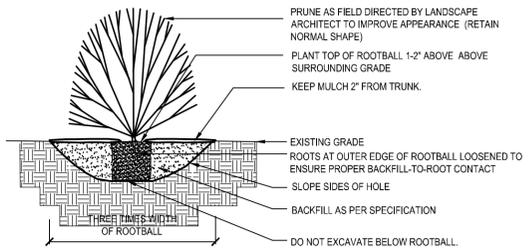
PROPOSED LANDSCAPE:

1 TREE/ 500 SF OF "LANDSCAPE AREA"
6,620.2 SF / 500 = 13 TREES REQUIRED

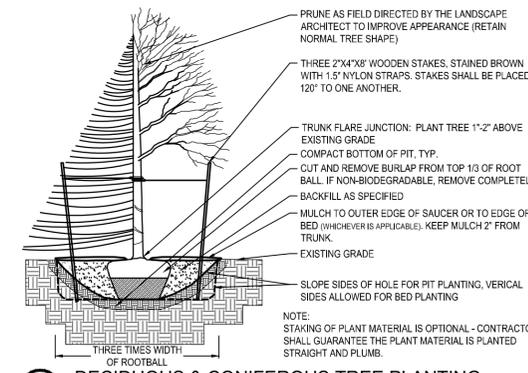
SEE PLANT SCHEDULE

1 SHRUB/100 SF OR "LANDSCAPE AREA"
6,620.2 SF / 100 = 66 SHRUBS REQUIRED

SEE PLANT SCHEDULE



1 SHRUB NTS



2 DECIDUOUS & CONIFEROUS TREE PLANTING NTS

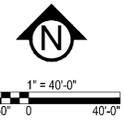
POLLINATOR SAFE PLANT MATERIAL:

- THE CONTRACTOR SHALL PROVIDE ONLY PLANT MATERIAL FREE OF NEONICOTINOID BASED INSECTICIDES AND/OR TREATMENTS OF ANY KIND, INCLUDING BUT NOT LIMITED TO IMIDACLOPRID (CONFIDOR, ADMIRE, GAUCHO, ADVOCATE), THIAMETHOXAM (ACTARA, PLATINUM, CRUISER), CLOTHIANIDIN (PONCHO, DANOTOSU, DANTOP), ACETAMIPRID (MOSPILAN, ASSAIL, NITENOTRISTAR), THIACTOPRID (CALYPSO), DINOTEFURAN (STARKLE, SAFARI, VENOM), AND NITENPYRAM (CAPSTAR, GUARDIAN).
- CONTRACTOR SHALL CERTIFY, THROUGH SUPPLIER'S POLICY STATEMENT OR AFFIDAVIT, THAT NO NEONICOTINOID BASED INSECTICIDES HAVE BEEN USED ON SITE OR DIRECTLY ADJACENT TO THE GROWING OR STORAGE PLOTS OF THE SUPPLIED PLANT MATERIAL, INCLUDING THE PLANTING OF AGRICULTURAL (OR OTHER) SEED TREATED WITH NEONICS.

LEGEND:

- SHRIPPED HARDWOOD MULCH (DK. BROWN), OVER FILTER FABRIC IN ALL PLANTING BEDS AND PLANTERS, TYP.
- SOD

GOPHER STATE ONE CALL
WWW.GOPHERSTATEONECALL.ORG
(800) 252-1166 TOLL FREE
(651) 454-0002 LOCAL



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Patrick J. Sarver
DATE 11/06/15 LICENSE NO. 24904

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DATE	DESCRIPTION

PROJECT NUMBER: 15033

LANDSCAPE PLAN

L1.0



- SWPPP NOTES:**
1. THIS PROJECT IS GREATER THAN ONE ACRE AND AN MPCA NPDES PERMIT IS REQUIRED. AN EROSION CONTROL PERMIT IS ALSO REQUIRED FROM THE CITY OF MINNEAPOLIS.
 2. SEE SHEETS SW1.0 - SW1.5 FOR ALL EROSION CONTROL NOTES, DESCRIPTIONS, AND PRACTICES.
 3. SEE GRADING PLAN FOR ADDITIONAL GRADING AND EROSION CONTROL NOTES.
 4. CONTRACTOR IS RESPONSIBLE FOR SWPPP IMPLEMENTATION, INSPECTIONS, AND COMPLIANCE WITH NPDES PERMIT.

PROJECT
ESSEX AND 27TH AVENUE SE REDEVELOPMENT
117 27TH AVENUE SE, MINNEAPOLIS, MN
ELSEY PARTNERS, LLC
1532 COLLEGE AVENUE, F-19, MANHATTAN, KS 66502

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

Matthew R. Pavak
Matthew R. Pavak
DATE 11/06/15 LICENSE NO. 44263

ISSUE/SUBMITTAL SUMMARY

DATE	DESCRIPTION
10/16/15	PDR SUBMITTAL
11/06/15	LAND USE SUBMITTAL

REVISION SUMMARY

DATE	DESCRIPTION

PROJECT NUMBER: 15033

SWPPP - EXISTING CONDITIONS

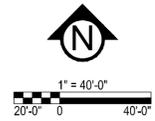
SW1.0

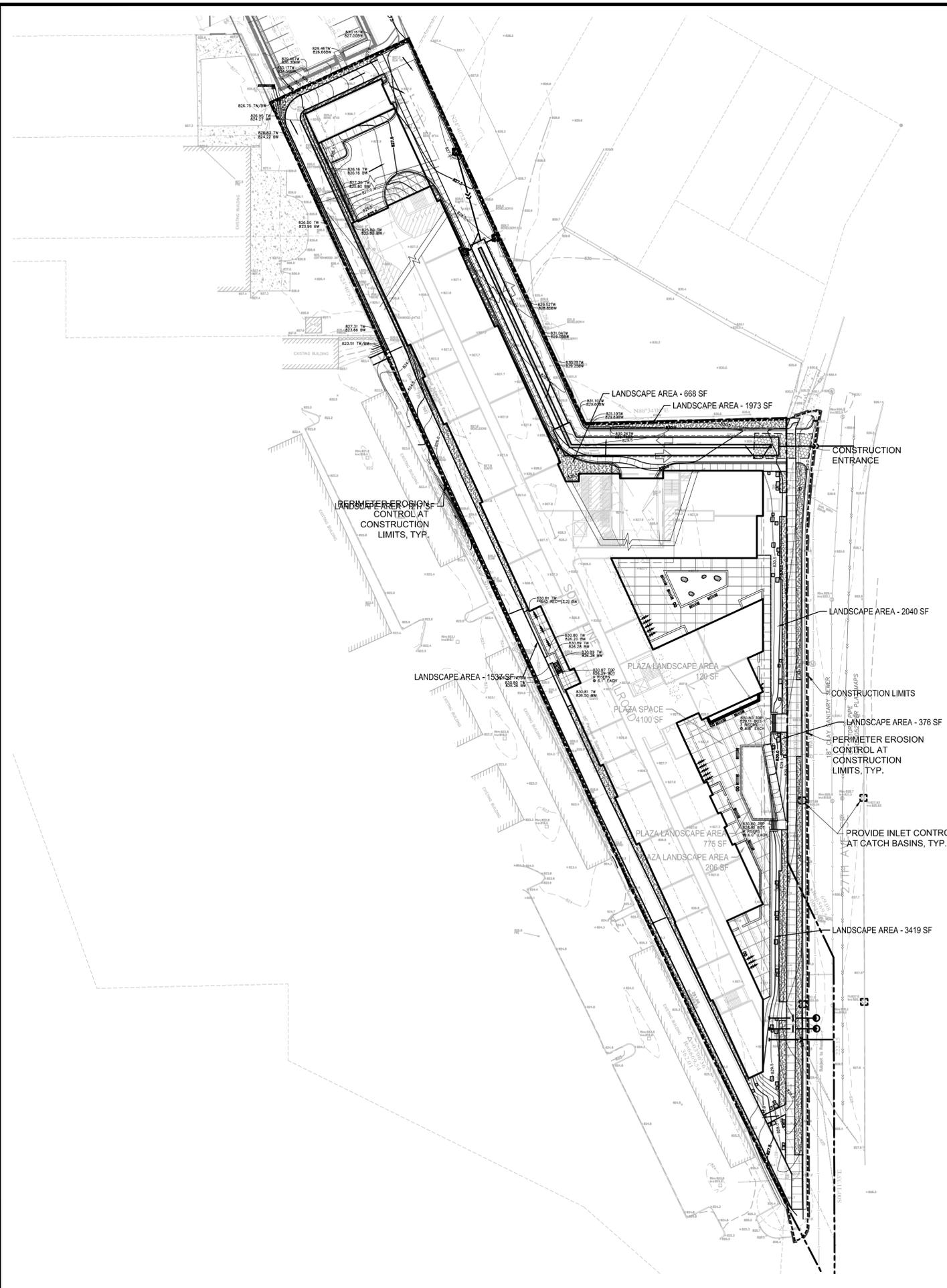
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- 932 --- EX. 1' CONTOUR ELEVATION INTERVAL
- 932 — 1' CONTOUR ELEVATION INTERVAL
- 932.0 • EXISTING SPOT GRADE ELEVATION
- 932.0 — PROPOSED SPOT GRADE ELEVATION
- DRAINAGE ARROW
- SILT FENCE / BIOROLL - GRADING LIMIT
- INLET PROTECTION
- ▨ STABILIZED CONSTRUCTION ENTRANCE
- PROPOSED MANHOLE OR CATCH BASIN

- ⊕ PROPOSED GATE VALVE
- ⊕ PROPOSED FIRE HYDRANT
- PROPOSED SANITARY SEWER
- PROPOSED STORM SEWER
- EXISTING STORM SEWER
- EXISTING WATER MAIN
- EXISTING GAS MAIN
- EXISTING UNDERGROUND ELECTRIC
- EXISTING UNDERGROUND CABLE
- EXISTING MANHOLE
- EXISTING CATCH BASIN
- ⊕ EXISTING HYDRANT
- EXISTING STOPBOX
- ⊗ EXISTING GATE VALVE
- ⊕ EXISTING ELECTRIC BOX
- ⊕ EXISTING LIGHT
- ⊕ EXISTING GAS METER
- EXISTING GAS VALVE

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(800) 252-1166 TOLL FREE
(651) 454-0002 LOCAL





- SWPPP NOTES:**
1. THIS PROJECT IS GREATER THAN ONE ACRE AND AN MPCA NPDES PERMIT IS REQUIRED. AN EROSION CONTROL PERMIT IS ALSO REQUIRED FROM THE CITY OF MINNEAPOLIS.
 2. SEE SHEETS SW1.0 - SW1.5 FOR ALL EROSION CONTROL NOTES, DESCRIPTIONS, AND PRACTICES.
 3. SEE GRADING PLAN FOR ADDITIONAL GRADING AND EROSION CONTROL NOTES.
 4. CONTRACTOR IS RESPONSIBLE FOR SWPPP IMPLEMENTATION, INSPECTIONS, AND COMPLIANCE WITH NPDES PERMIT.

PROJECT
ESSEX AND 27TH AVENUE SE REDEVELOPMENT
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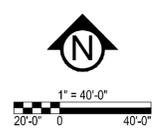
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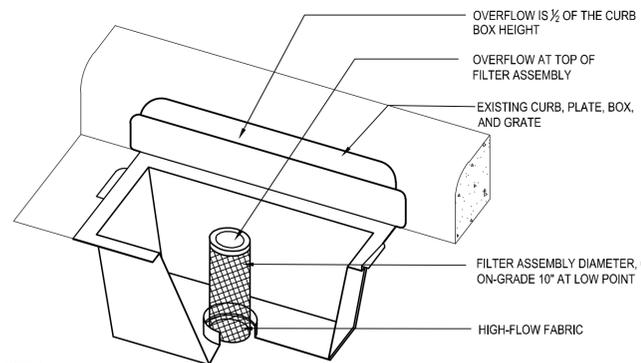
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| --- 932 --- | EX. 1' CONTOUR ELEVATION INTERVAL | | PROPOSED GATE VALVE |
| — 932 — | 1' CONTOUR ELEVATION INTERVAL | | PROPOSED FIRE HYDRANT |
| x 802.0 | EXISTING SPOT GRADE ELEVATION | | PROPOSED SANITARY SEWER |
| — 932.0 — | PROPOSED SPOT GRADE ELEVATION | | PROPOSED STORM SEWER |
| | DRAINAGE ARROW | | EXISTING STORM SEWER |
| | SILT FENCE / BIOROLL - GRADING LIMIT | | EXISTING WATER MAIN |
| | INLET PROTECTION | | EXISTING GAS MAIN |
| | STABILIZED CONSTRUCTION ENTRANCE | | EXISTING UNDERGROUND ELECTRIC |
| | PROPOSED MANHOLE OR CATCH BASIN | | EXISTING UNDERGROUND CABLE |
| | | | EXISTING MANHOLE |
| | | | EXISTING CATCH BASIN |
| | | | EXISTING HYDRANT |
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| | | | EXISTING GATE VALVE |
| | | | EXISTING ELECTRIC BOX |
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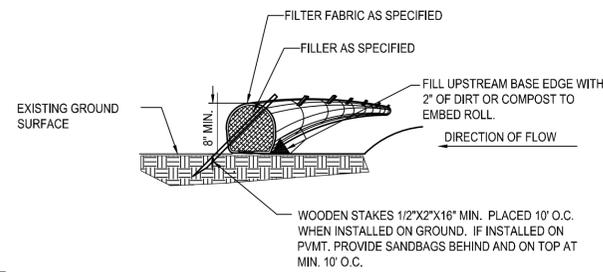
PROJECT NUMBER: 15033
 SWPPP - PROPOSED CONDITIONS

SW1.1



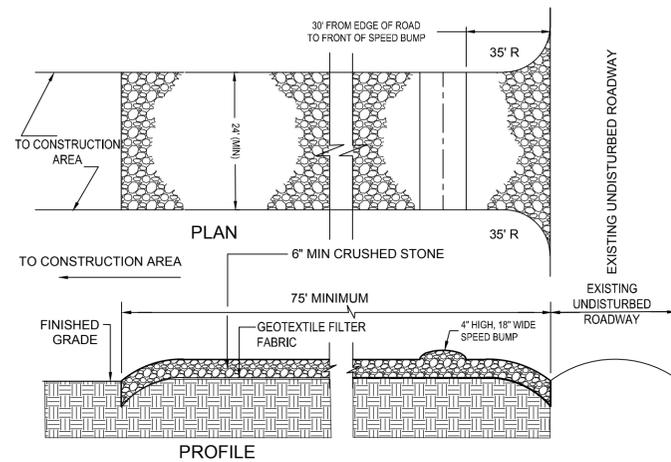
- NOTES:
1. REPLACE INLET GRATE UPON COMPLETE INSTALLATION OF INLET PROTECTION FABRIC.
2. CONTRACTOR SHALL REMOVE ALL ACCUMULATED SEDIMENT AND DEBRIS FROM THE SURFACE OF THE SYSTEM AFTER EACH STORM EVENT AND AT THE COMPLETION OF THE CONTRACT.
3. REFERENCE APPLE VALLEY STANDARD PLATE ERO-4C.

1 CURB INLET FILTER
N T S



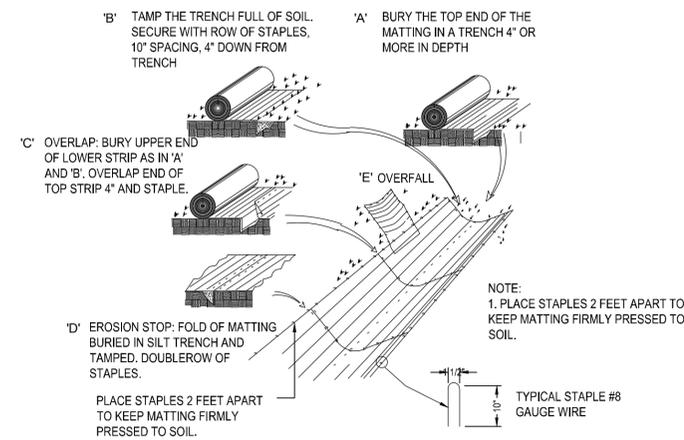
- NOTE:
1. COMPOST FILTER LOGS (BIO ROLLS) SHALL BE FILTREXX EROSION CONTROL SOXX OR APPROVED EQUAL.
2. COMPOST FILLER TO BE MADE FROM A COMPOST BLEND 30%-40% GRADE 2 (SPEC 3890) AND 60%-70% PARTIALLY DECOMPOSED WOOD CHIPS, PER MNDOT SPEC 3897.
3. FILTER FABRIC SHALL BE GEOTEXTILE KNITTED MATERIAL WITH MAX. OPENINGS OF 3/8\"/>
4. IF MULTIPLE ROLLS NEEDED, OVERLAP BY MIN. 12\"/>

4 SEDIMENT BIO-ROLL / COMPOST FILTER LOG
N T S

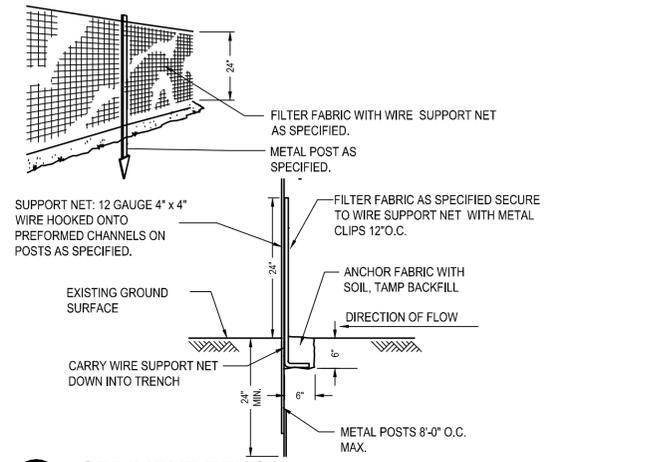


- NOTES:
1. PROVIDE APPROPRIATE TRANSITION BETWEEN STABILIZED CONSTRUCTION ENTRANCE AND UNDISTURBED ROADWAY.
2. THE ENTRANCE SHALL BE MAINTAINED IN A CONDITION WHICH WILL PREVENT TRACKING OR FLOWING OF SEDIMENT ONTO UNDISTURBED ROADWAY. THIS MAY REQUIRE PERIODIC TOP DRESSING WITH ADDITIONAL STONE OR ADDING STONE TO THE LENGTH OF THE ENTRANCE.
3. REPAIR AND CLEANOUT MEASURES USED TO TRAP SEDIMENT.
4. ALL SEDIMENT SPILLED, DROPPED, WASHED, OR TRACKED ONTO UNDISTURBED ROADWAY SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.
5. FINAL LOCATION AND INSTALLATION SHALL BE COORDINATED WITH THE CITY PRIOR TO CONSTRUCTION ACTIVITIES.
6. CRUSHED STONE SHALL BE 1-1/2\"/>

2 STABILIZED CONSTRUCTION ACCESS
N T S



3 EROSION BLANKET
N T S



5 SEDIMENT FENCE
N T S

PROJECT
ESSEX AND 27TH AVENUE SE REDEVELOPMENT

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PROJECT NUMBER: 15033

SWPPP - DETAILS

SW1.2



Attachment A: SWPPP Site Specific Document Stormwater Pollution Prevention Plan (SWPPP)

To comply with the General Stormwater Permit for Construction Activity

Construction Activity Information

Project name: 117 27th AVENUE REDEVELOPMENT
Project location: 117 27th AVENUE
City or Township: MINNEAPOLIS State: MN Zip code: 55414
Latitude/longitude of approximate centroid of project: LAT: 44.969476 LONG: -93.220786
Method of collection of latitude/longitude: GPS Online tool USGS Topographic map
All cities where construction will occur: MINNEAPOLIS
All counties where construction will occur: HENNEPIN
All townships where construction will occur:
Project size (number of acres to be disturbed): 2.2
Project type: Residential Commercial/Industrial Road construction
Cumulative impervious surface: Existing area of impervious surface: 1.1 (to the nearest tenth acre)
Post construction area of impervious surface: 1.9 (to the nearest tenth acre)
Total new area of impervious surface: 0.8 (to the nearest tenth acre)

Table with 5 columns: Water body ID, Name of water body, Type, Special water?, Impaired Water?*. Row 1: 07010206-503, MISSISSIPPI RIVER, RIVER, Yes No, Yes No

*Water body identification (ID) might not be available for all water bodies. Use the Special and Impaired Waters Search Tool at: www.pca.state.mn.us/water/stormwater/swppp-swc.html

RIP RAP WILL BE UTILIZED AT PIPE OUTLETS.
6. Describe Methods to be used to promote infiltration and sediment removal on the site prior to offsite discharge, unless infeasible (e.g., direct stormwater flow to vegetated areas): DISCONNECTED IMPERVIOUS AREA AND INFILTRATION AREAS WILL BE UTILIZED.
7. For drainage or diversion ditches, describe practices to stabilize the normal wetted perimeter within 200 lineal feet of the property edge or point of discharge to surface water. The last 200 lineal feet must be stabilized within 24 hours after connecting to surface waters and construction in that portion of the ditch has temporarily or permanently ceased for all discharges to Special, Impaired or work in water restrictions. All other remaining portions of the temporary or permanent ditches or swales within 14 calendar days after connecting to a surface water, property edge and construction in that area has temporarily or permanently ceased. N/A - NO DITCHES ON SITE
8. Describe additional erosion prevention measures that will be implemented at the site during construction (e.g., construction phasing, minimizing soil disturbance, vegetative buffers, horizontal slope grading, slope draining/terracing, etc.): OTHER EROSION CONTROL PRACTICES INCLUDE BUT NOT LIMITED TO: MINIMIZE SITE EXPOSURE WHEN POSSIBLE.
9. If applicable, include additional requirements in Appendix A Part C.3 regarding maintaining a 100-foot buffer zone or installing redundant BMPs for portions of the site that drain to special waters). N/A
10. If applicable, describe additional erosion prevention BMPs to be implemented at the site to protect planned infiltration areas MINIMIZE SITE EXPOSURE IN AREAS ADJACENT TO INFILTRATION AREA.

Sediment Control Practices (IV.C)

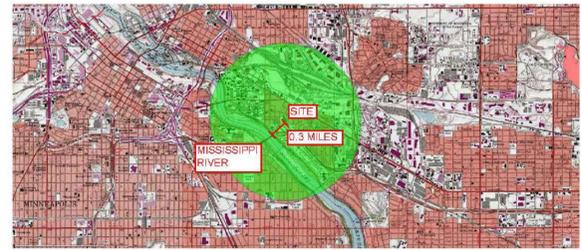
Describe the methods of sediment control BMPs to be implemented at this site during construction to minimize sediment impacts to surface waters, including curb and gutter systems.
1. Describe Methods to be used for down gradient perimeter control: SILT FENCE WILL BE INSTALLED AROUND THE ENTIRE PERIMETER OF THE SITE.
2. Describe Methods to be used to contain soil stockpiles: SEED AND MULCH AS WELL AS EROSION CONTROL BLANKETS WILL BE UTILIZED AS NECESSARY.
3. Describe Methods to be used for storm drain inlet protection: SEE INLET PROTECTION DETAILS.
4. Describe Methods to minimize vehicle tracking at construction exits and street sweeping activities: THE PROJECT WILL UTILIZE A ROCK CONSTRUCTION ENTRANCE.
5. Describe Methods, if applicable, additional sediment controls (e.g., diversion berms) to be installed to keep runoff away from planned infiltration areas when excavated prior to final stabilization of the contributing drainage area: SILT FENCE TO BE INSTALLED IMMEDIATELY AFTER GRADING TO PROTECT INFILTRATION AREAS.
6. Describe methods to be used to minimize soil compaction and preserve top soil (unless infeasible) at this site: LIGHT TRACKED EQUIPMENT WILL BE USED. TOPSOIL WILL BE STRIPPED AND STOCKPILED.
7. Describe plans to preserve a 50-foot natural buffer between the project's soil disturbance and a surface water or plans for redundant sediment controls if a buffer is infeasible: DOUBLE ROW OF SILT FENCE WILL BE INSTALLED ALONG WETLAND. PROJECT WILL NOT DISTURB WITHIN 200 FEET OF WETLAND.
8. Describe plans for use of sedimentation treatment chemicals (e.g., polymers, flocculants, etc.) see Part IV.C.10 of the permit: N/A

Dates of construction (Briefly describe where construction activity occurs. Include address if available.)
Construction start date: 5/16 Estimated completion date: 5/17

General Construction Project Information

Describe the construction activity (what will be built, general timeline, etc): THE PROJECT WILL BE THE CONSTRUCTION OF A MULTI UNIT APARTMENT BUILDING.
Describe soil types found at the project: THE EXISTING SOILS AT THE SITE ARE PRIMARILY SILTY SANDS.

Site Location map - Attach maps (U.S. Geologic Survey 7.5 minute quadrangle, National Wetland Inventory maps or equivalent) showing the location and type of all receiving waters, including wetlands, drainage ditches, stormwater ponds or basins, etc. that will receive runoff from the project. Use arrows showing the direction of flow and distance to the water body



9. Is the project required to install a temporary sediment basin due to 10 or more acres draining to a common location or 5 acres or more if the site is within 1 mile of a special or impaired water? Yes No
If yes, describe (or attach plans) showing how the basin will be designed and constructed in accordance with Part III.C of the permit. N/A

Dewatering and Basin Draining (IV.D)

1. Will the project include dewatering or basin draining? Yes No
If yes, describe measures to be used to treat/dispose of turbid or sediment-laden water and method to prevent erosion or scour of discharge points (see Part IV. D of the permit):
2. Will the project include use of filters for backwash water? Yes No
If yes, describe how filter backwash water will be managed on the site or properly disposed (see Part III.D.3. of the permit):

Additional BMPs for Special Waters and Discharges to Wetlands (Appendix A, Parts C and D)

1. Special Waters. Does your project discharge to special waters? Yes No
2. If proximity to bedrock or road projects where the lack of right of way precludes the installation of any of the permanent stormwater management practices, then other treatment such as grassed swales, smaller ponds, or grit chambers is required prior to discharge to surface waters. Describe what other treatment will be provided. N/A
3. Describe erosion and sediment controls for exposed soil areas with a continuous positive slope to a special waters, and temporary sediment basins for areas that drain five or more acres disturbed at one time. N/A
4. Describe the undisturbed buffer zone to be used (not less than 100 lineal feet from the special water). N/A
5. Describe how the permanent stormwater management system will ensure that the pre and post project runoff rate and volume from the 1, and 2-year 24-hour precipitation events remains the same. N/A
6. Describe how the permanent stormwater management system will minimize any increase in the temperature of trout stream receiving waters resulting in the 1, and 2-year 24-hour precipitation events. N/A
7. Wetlands. Does your project discharge stormwater with the potential for significant adverse impacts to a wetland (e.g., conversion of a natural wetland to a stormwater pond)? Yes No
If yes, describe the wetland mitigation sequence that will be followed in accordance with Part D of Appendix A. N/A

Inspections and Maintenance (IV.E)

Describe procedures to routinely inspect the construction site:
• Once every seven (7) days during active construction and
• Within 24 hours after a rainfall event greater than 0.5 inches in 24 hours, and within seven (7) days after that.
Inspections must include stabilized areas, erosion prevention and sediment control BMPs, and infiltration areas.
INSPECTOR WILL FOLLOW REQUIREMENTS SPECIFIED ABOVE AND FILL OUT ATTACHMENT B - CONSTRUCTION

General Site Information (III.A)

1. Describe the location and type of all temporary and permanent erosion prevention and sediment control Best Management Practices (BMPs). Include the timing for installation and procedures used to establish additional temporary BMPs as necessary. (III.A.4.a)
THE PROJECT WILL BE PROTECTED BY TWO (2) MAIN BMP'S, SILT FENCE AND INLET PROTECTION DEVICES. THE SILT FENCE WILL BE INSTALLED AT THE DOWNHILL LOCATIONS OF THE SITE AND MONITORED AS NECESSARY. INLET PROTECTION DEVICES WILL BE INSTALLED IN ALL CATCH BASINS ON THE SITE AND ANY OFF SITE THAT WILL RECEIVE STORMWATER RUNOFF FROM THIS SITE, AS THE PROJECT PROGRESSES. ADDITIONAL BMP'S SUCH AS EROSION CONTROL BLANKET MAY BE UTILIZED.
2. Attach to this SWPPP a table with the anticipated quantities for the life of the project for all erosion prevention and sediment control BMPs (III. A. 4.b)
SEE PAGE SW1.3
3. Attach to this SWPPP a site map that includes the following features (III.A.3.b - f):
• Existing and final grades, including dividing lines and direction of flow for all pre and post-construction stormwater runoff drainage areas located within the project limits.
• Locations of impervious surfaces and soil types.
• Locations of areas not to be disturbed.
• Location of areas of phased construction.
• All surface waters and existing wetlands within one mile from the project boundaries that will receive stormwater runoff from the site (identifiable on maps such as USGS 7.5 minute quadrangle maps or equivalent). Where surface waters receiving runoff associated with construction activity will not fit on the plan sheet, they must be identified with an arrow, indicating both direction and distance to the surface water.
Methods to be used for final stabilization of all exposed soil areas.
4. Were stormwater mitigation measures required as the result of an environmental, archaeological, or other required local, state, or federal review of the project? Yes No
If yes, describe how these measures were addressed in the SWPPP. (III.A.6)
N/A
5. Is the project located in a karst area such that additional measures would be necessary to protect drinking water supply management areas as described in Minn. R. chapters 7050 and 7060? Yes No
If yes, describe the additional measures to be used. (III.A.7)
N/A
6. Does the site discharge to a calcareous fen listed in Minn. R. 7050.0180, subp. 6.b.? Yes No
If yes, a letter of approval from the Minnesota Department of Natural Resources must be obtained prior to application for this permit. (Part I B.6 and Part III A.6)
7. Does the site discharge to a water that is listed as impaired for the following pollutant(s) or stressor(s): phosphorus, turbidity, dissolved oxygen or biotic impairment? Use the Special and Impaired Waters Search Tool at: www.pca.state.mn.us/water/stormwater/stormwater-c.html. Yes No
If no, skip to Training.
b. List the BMPs and any other specific construction stormwater related implementation activities identified in the TMDL.
If the site has a discharge point within one mile of the impaired water and the water flows to the impaired water but no specific BMPs for construction are identified in the TMDL, the additional BMPs in Appendix A (C.1, C.2, C.3 & (C.4-trout stream)) must be added to the SWPPP and implemented. (III.A.7). The additional BMPs only apply to those portions of the project that drain to one of the identified discharge points.

STORMWATER INSPECTION CHECKLIST.*

Pollution Prevention Management Measures (IV.F)

1. Describe practices for storage of building products with a potential to leach pollutants to minimize exposure to stormwater: ALL BUILDING PRODUCTS WILL BE SEALED AND STORED IN A MANNER TO MINIMIZE EXPOSURE.
2. Describe practices for storage of pesticides, herbicides, insecticides, fertilizers, treatment chemical, and landscape materials: ALL LANDSCAPE TREATMENT CHEMICALS WILL BE SEALED AND STORED IN A MANNER TO MINIMIZE EXPOSURE.
3. Describe practices for storage and disposal of hazardous materials or toxic waste (e.g., oil, fuel, hydraulic fluids, paint solvents, petroleum-based products, wood preservative, additives, curing compounds, and acids) according to Minn. R. ch. 7045, including restricted access and secondary containment: ALL HAZARDOUS WASTE WILL BE APPROPRIATELY DISPOSED OFF SITE ACCORDING TO LOCAL AND STATE LAWS.
4. Describe collection, storage and disposal of solid waste in compliance with Minn. R. ch. 7035: ALL CONSTRUCTION DEBRIS AND SOLID WASTE WILL BE APPROPRIATELY DISPOSED OFF SITE ACCORDING TO LOCAL AND STATE LAWS.
5. Describe management of portable toilets to prevent tipping and disposal of sanitary wastes in accordance with Minn. R. ch. 7040: SANITARY AND SEPTIC SERVICES WILL BE PROVIDED TO WORKERS WITH PORTABLE FACILITIES MAINTAINED AS NEEDED BY THE PROVIDER.
6. Describe spill prevention and response for fueling and equipment or vehicle maintenance: EMPLOYEES WILL BE TRAINED IN TECHNIQUES DESIGNED TO MINIMIZE SPILLS. VEHICLES AND EQUIPMENT SHALL BE CHECKED FOR LEAKS.
7. Describe containment and disposal of vehicle and equipment wash water and prohibiting engine degreasing on the site: ALL CONSTRUCTION VEHICLES SHALL BE WASHED OFF SITE.
8. Describe storage and disposal of concrete and other washout wastes so that wastes do not contact the ground: ALL CONCRETE WASH-OUT SHALL OCCUR OFF SITE.

Final Stabilization (IV.G)

1. Describe method of final stabilization (permanent cover) of all disturbed areas: FINAL STABILIZATION WILL BE ACCOMPLISHED WITH PAVEMENT, SOD AND LANDSCAPE MATERIALS.
2. Describe procedures for completing final stabilization and terminating permit coverage (see Part IV.G.1-9): UPON FINAL STABILIZATION DESCRIBED ABOVE, THE CONTRACTOR AND OWNER SHALL MUTUALLY TRANSFER THE NPDES PERMIT TO THE NEXT OWNER WITH DOCUMENTS DESCRIBING THE NATURE OF TERMINATION PROCEDURE.

Documentation of infeasibility: (If Applicable)

N/A
9. Identify adjacent public waters where the Minnesota Department of Natural Resources (DNR) has declared work in water restrictions* during fish spawning timeframes
N/A

Selection of a Permanent Stormwater Management System (III.D.)

1. Will the project create a new cumulative impervious surface greater than or equal to one acre? Yes No
If yes, a water quality volume of one inch of runoff from the cumulative new impervious surfaces must be retained on site (see Part III.D of the permit) through infiltration unless prohibited due to one of the reasons in Part III.D.1. If infiltration is prohibited identify other method of other volume reduction (e.g., filtration system, wet sedimentation basin, regional ponding or equivalent method)
2. Describe which method will be used to treat runoff from the new impervious surfaces created by the project (III.D):
• Wet sedimentation basin
• Infiltration/Filtration
• Regional ponds
• Combination of practices
Include all calculations and design information for the method selected. See Part III.D of the permit for specific requirements associated with each method.
INFILTRATION/FILTRATION
Calculations are within the Site Storm Water management Report and part of this SWPPP as Attachment D.
3. If it is not feasible to meet the treatment requirement for the water quality volume, describe why. This can include proximity to bedrock or road projects where the lack of right of way precludes the installation of any permanent stormwater management practices. Describe what other treatment, such as grasses swales, smaller ponds, or grit chambers, will be implemented to treat runoff prior to discharge to surface waters. (III.C)
IT IS FEASIBLE TO MEET REQUIREMENT FOR WATER QUALITY VOLUME.
4. For projects that discharge to trout streams, including tributaries to trout streams, identify method of incorporating temperature controls into the permanent stormwater management system.
N/A

Erosion Prevention Practices (IV.B)

Describe the types of temporary erosion prevention BMPs expected to be implemented on this site during construction:
1. Describe construction phasing, vegetative buffer strips, horizontal slope grading, and other construction practices to minimize erosion. Delineate areas not to be disturbed (e.g., with flags, stakes, signs, silt fence, etc.) before work begins. SILT FENCE WILL BE INSTALLED AT THE DOWNHILL LOCATIONS OF THE SITE.
2. Describe Methods of temporarily stabilizing soils and soil stockpiles (e.g., mulches, hydraulic tackifiers, erosion blankets, etc.): TEMPORARY EROSION PROTECTION WILL BE SEED AND MULCH AND EROSION BLANKETS WHERE REQUIRED, WITH PERMANENT COVER BEING EITHER SOD OR LANDSCAPE FEATURES.
3. Describe Methods of dissipating velocity along stormwater conveyance channels and at channel outlets (e.g., check dams, sediment traps, rip rap, etc.): SOD WILL BE UTILIZED ALONG CHANNELS AND RIP RAP AT CHANNEL OUTLETS.
4. Describe Methods to be used for stabilization of ditch and swale wetted perimeters (Note that mulch, hydraulic soil tackifiers, hydromulches, etc. are not acceptable soil stabilization methods for any part of a drainage ditch or swale) FINAL STABILIZATION OF SWALES WILL BE SOD.
5. Describe Methods to be used for energy dissipation at pipe outlets (e.g., rip rap, splash pads, gabions, etc.)

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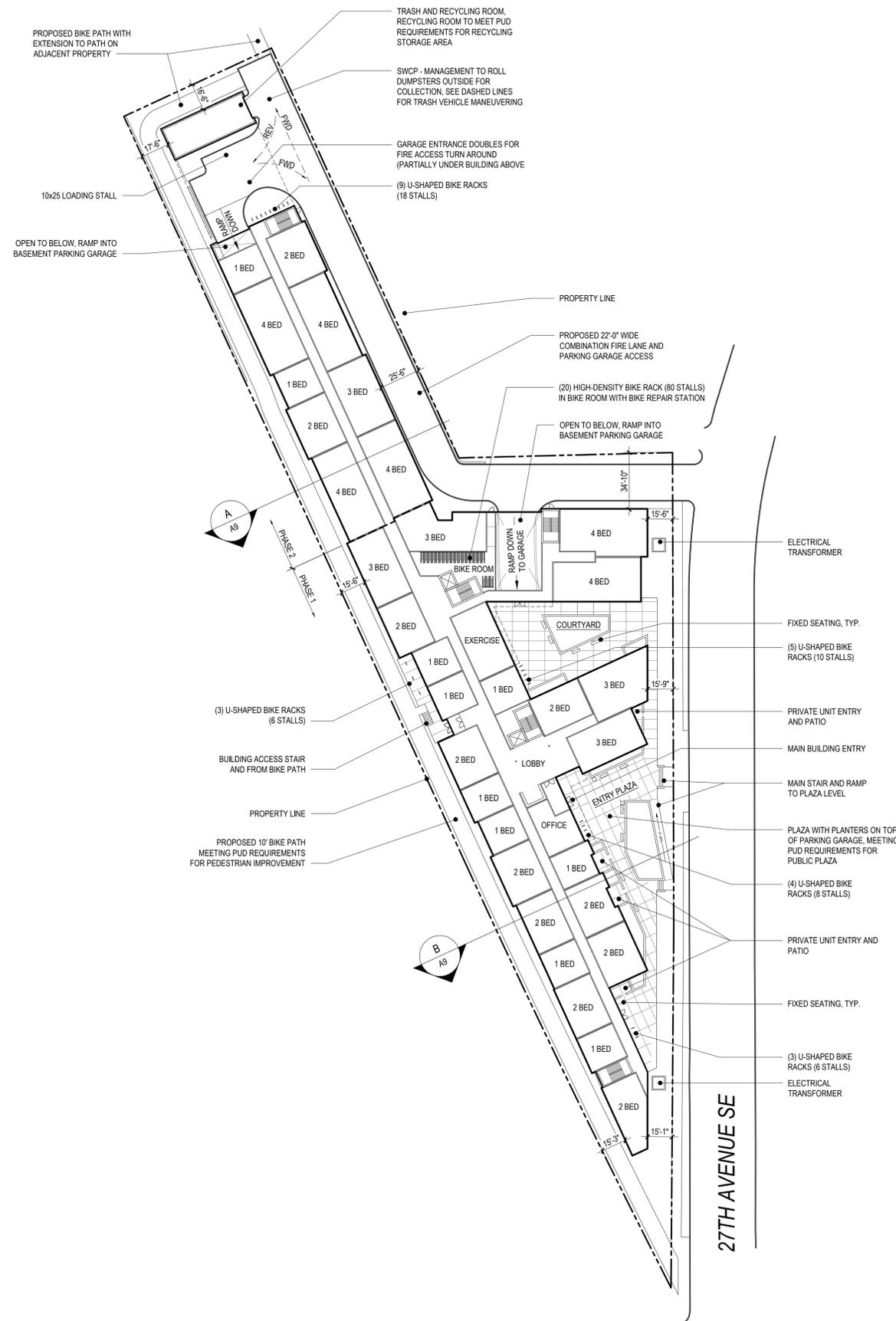
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ISSUE/SUBMITTAL SUMMARY table with columns DATE and DESCRIPTION. Row 1: 10/16/15 PDR SUBMITTAL. Row 2: 11/06/15 LAND USE SUBMITTAL.

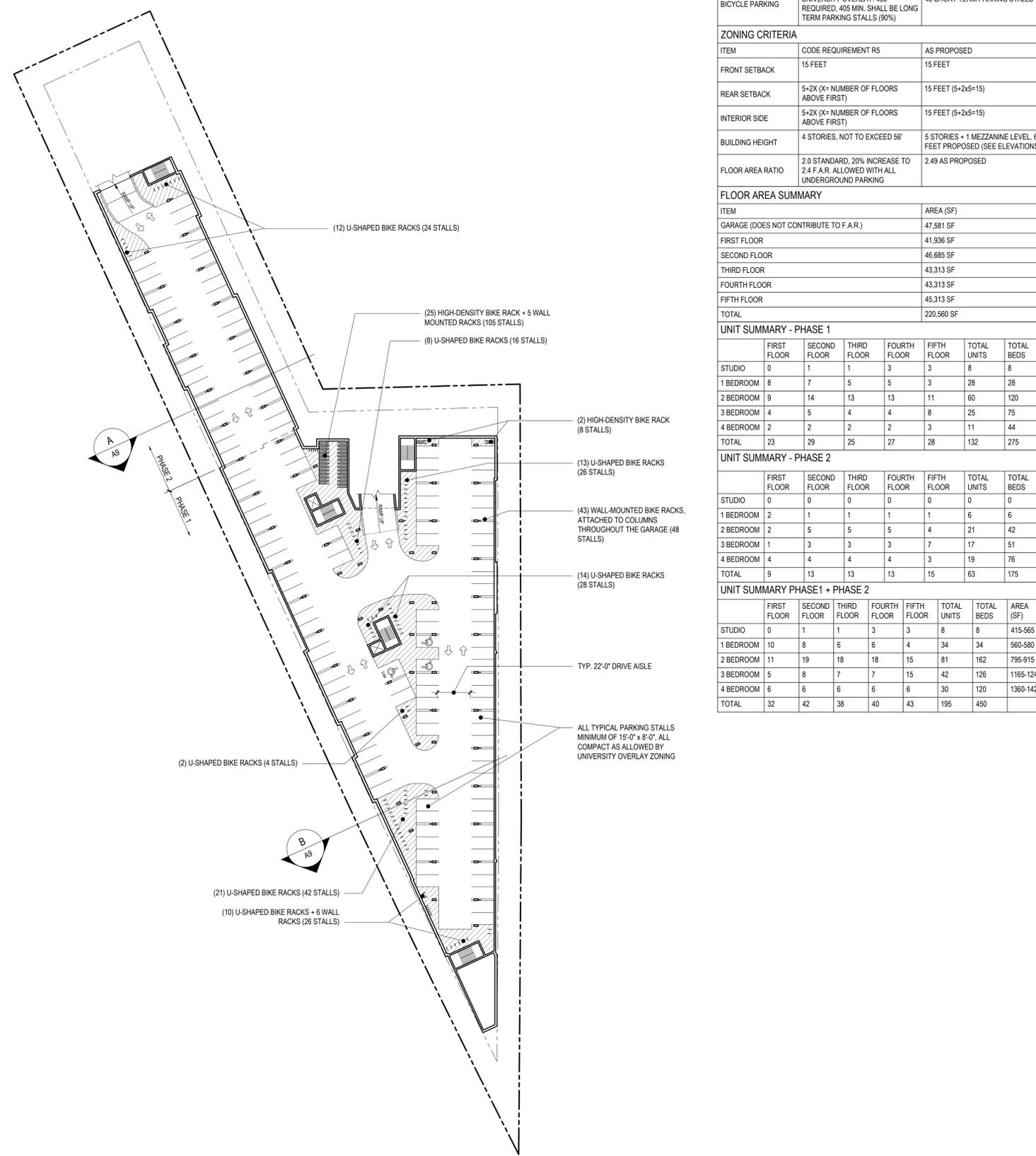
REVISION SUMMARY table with columns DATE and DESCRIPTION.

PROJECT NUMBER: 15033 SWPPP - ATTACHMENTS

SW1.4



FIRST FLOOR PLAN
1" = 40"



BASEMENT GARAGE FLOOR PLAN
1" = 40"

REQUIRED SITE FEATURES		
ITEM	CODE REQUIREMENT RS	AS PROPOSED
PARKING STALLS	0.5 STALLS PER BEDROOM, BUT NOT LESS THAN 1 STALL PER DWELLING UNIT PER UNIVERSITY OVERLAY= 246 STALLS REQUIRED	140 STALLS, (ALL COMPACT AS ALLOWED BY UNIVERSITY OVERLAY) 3 ACCESSIBLE STALL PROVIDED, 1 OF WHICH IS VAN
BICYCLE PARKING	1 SPACE PER BEDROOM PER UNIVERSITY OVERLAY: 450 REQUIRED, 405 MIN. SHALL BE LONG TERM PARKING STALLS (90%)	405 LONG TERM PARKING STALLS, 45 SHORT TERM PARKING STALLS

ZONING CRITERIA		
ITEM	CODE REQUIREMENT RS	AS PROPOSED
FRONT SETBACK	15 FEET	15 FEET
REAR SETBACK	5+2X (X= NUMBER OF FLOORS ABOVE FIRST)	15 FEET (5+2x5=15)
INTERIOR SIDE	5+2X (X= NUMBER OF FLOORS ABOVE FIRST)	15 FEET (5+2x5=15)
BUILDING HEIGHT	4 STORIES, NOT TO EXCEED 56'	5 STORIES + 1 MEZZANINE LEVEL, 64 FEET PROPOSED (SEE ELEVATIONS)
FLOOR AREA RATIO	2.0 STANDARD, 20% INCREASE TO 2.4 F.A.R. ALLOWED WITH ALL UNDERGROUND PARKING	2.49 AS PROPOSED

FLOOR AREA SUMMARY	
ITEM	AREA (SF)
GARAGE (DOES NOT CONTRIBUTE TO F.A.R.)	47,581 SF
FIRST FLOOR	41,936 SF
SECOND FLOOR	46,685 SF
THIRD FLOOR	43,313 SF
FOURTH FLOOR	43,313 SF
FIFTH FLOOR	45,313 SF
TOTAL	220,560 SF

UNIT SUMMARY - PHASE 1							
	FIRST FLOOR	SECOND FLOOR	THIRD FLOOR	FOURTH FLOOR	FIFTH FLOOR	TOTAL UNITS	TOTAL BEDS
STUDIO	0	1	1	3	3	8	8
1 BEDROOM	8	7	5	5	3	28	28
2 BEDROOM	9	14	13	13	11	60	120
3 BEDROOM	4	5	4	4	8	25	75
4 BEDROOM	2	2	2	2	3	11	44
TOTAL	23	29	25	27	28	132	275

UNIT SUMMARY - PHASE 2							
	FIRST FLOOR	SECOND FLOOR	THIRD FLOOR	FOURTH FLOOR	FIFTH FLOOR	TOTAL UNITS	TOTAL BEDS
STUDIO	0	0	0	0	0	0	0
1 BEDROOM	2	1	1	1	1	6	6
2 BEDROOM	2	5	5	5	4	21	42
3 BEDROOM	1	3	3	3	7	17	51
4 BEDROOM	4	4	4	4	3	19	76
TOTAL	9	13	13	13	15	63	175

UNIT SUMMARY PHASE1 + PHASE 2								
	FIRST FLOOR	SECOND FLOOR	THIRD FLOOR	FOURTH FLOOR	FIFTH FLOOR	TOTAL UNITS	TOTAL BEDS	AREA (SF)
STUDIO	0	1	1	3	3	8	8	415-565
1 BEDROOM	10	8	6	6	4	34	34	560-580
2 BEDROOM	11	19	18	18	15	81	162	795-915
3 BEDROOM	5	8	7	7	15	42	126	1165-1245
4 BEDROOM	6	6	6	6	30	120	480	1360-1420
TOTAL	32	42	38	40	43	195	450	

PRIME DESIGN
 MANHATTAN, KS 66502
 785.317.5725

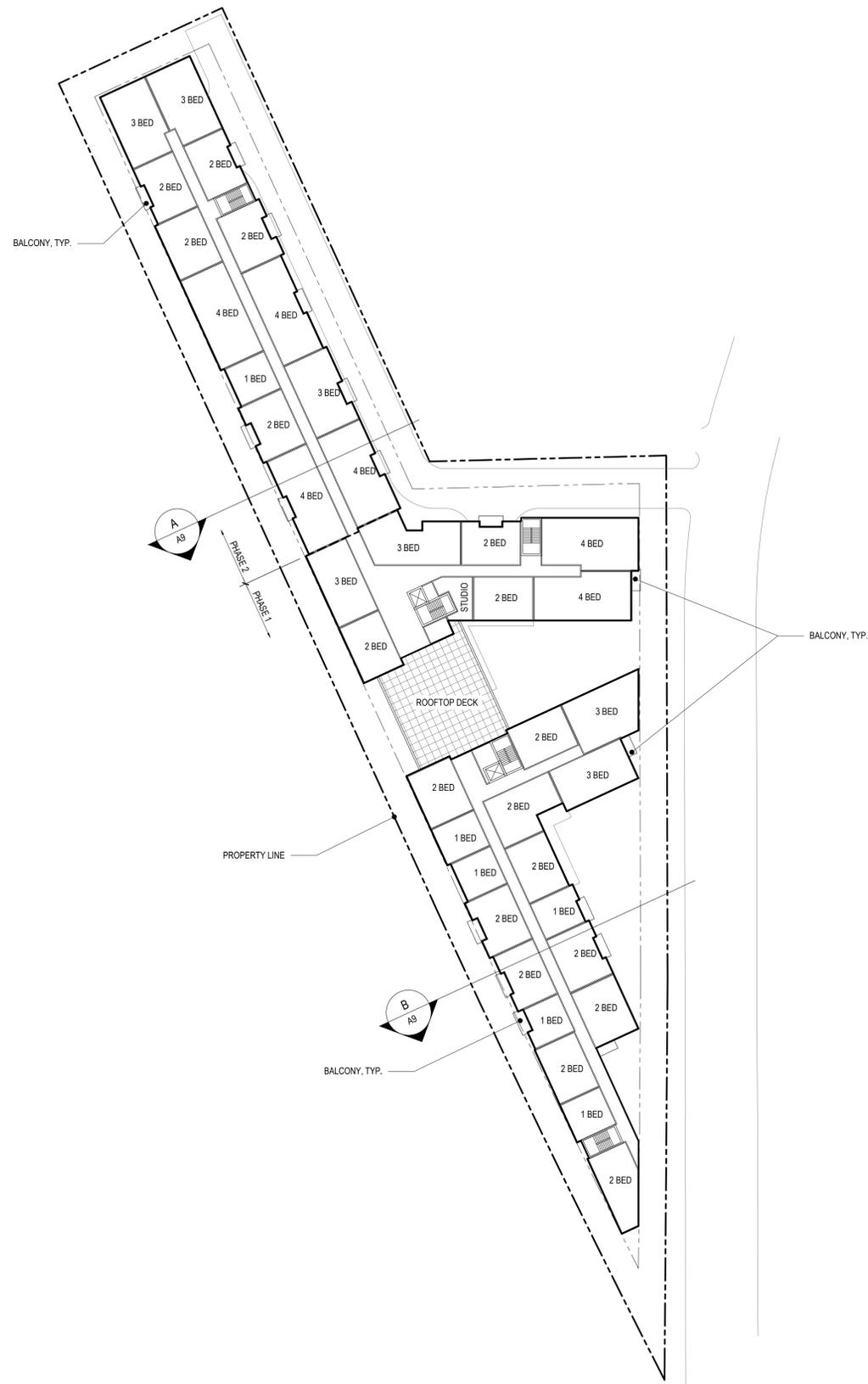
117 27TH AVE. SE APARTMENTS
 MINNEAPOLIS, MINNESOTA 55414
 ELSEY PARTNERS

Job No. 02-2015
 Sheet A1

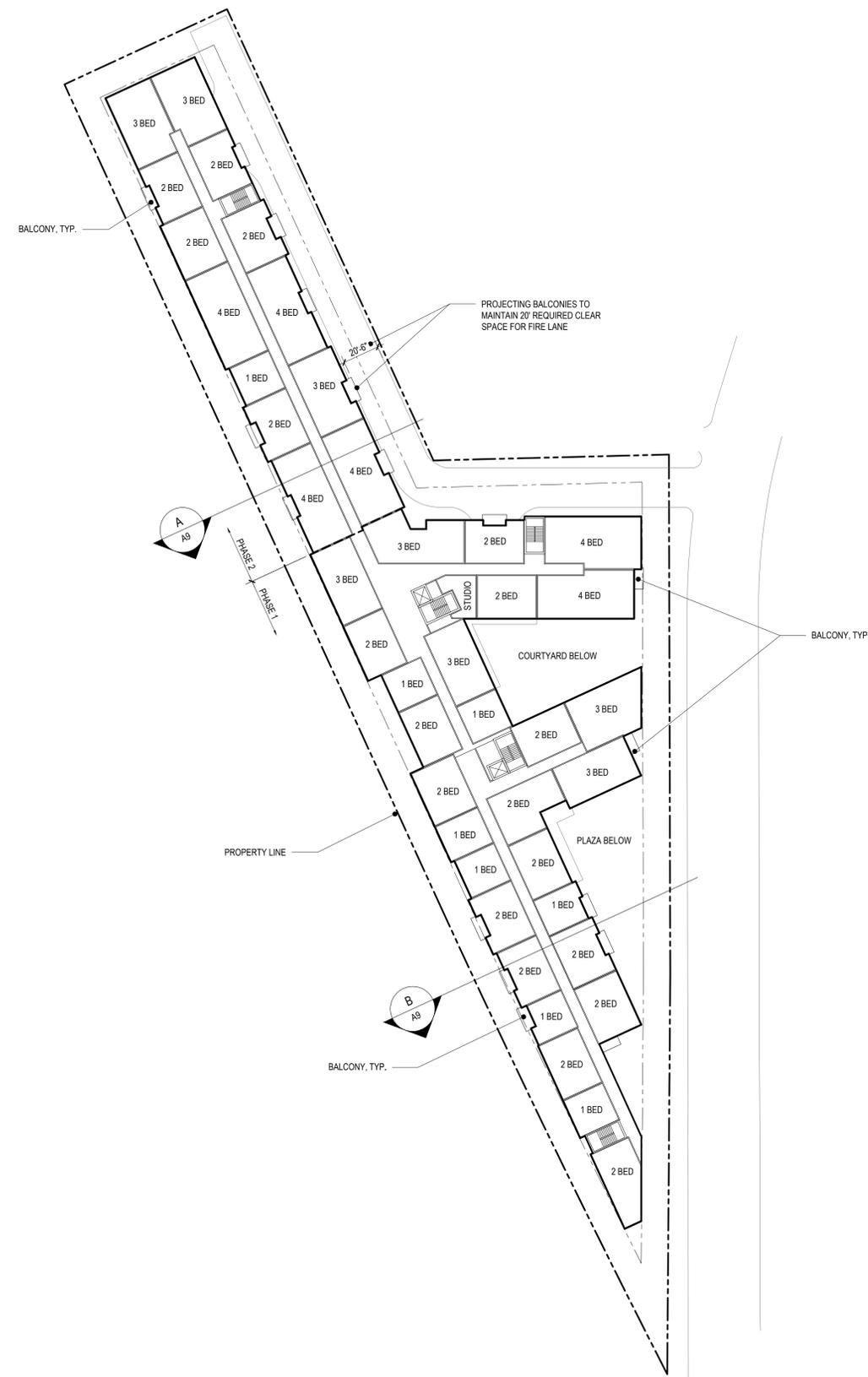
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

BRADLEY A. BUSER
 DATE: NOVEMBER 6, 2015
 LICENSE NUMBER: 52038

Date Issued: NOVEMBER 6, 2015
 Revisions:



B
A2
THIRD FLOOR PLAN
1" = 40"



A
A2
SECOND FLOOR PLAN
1" = 40"

HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

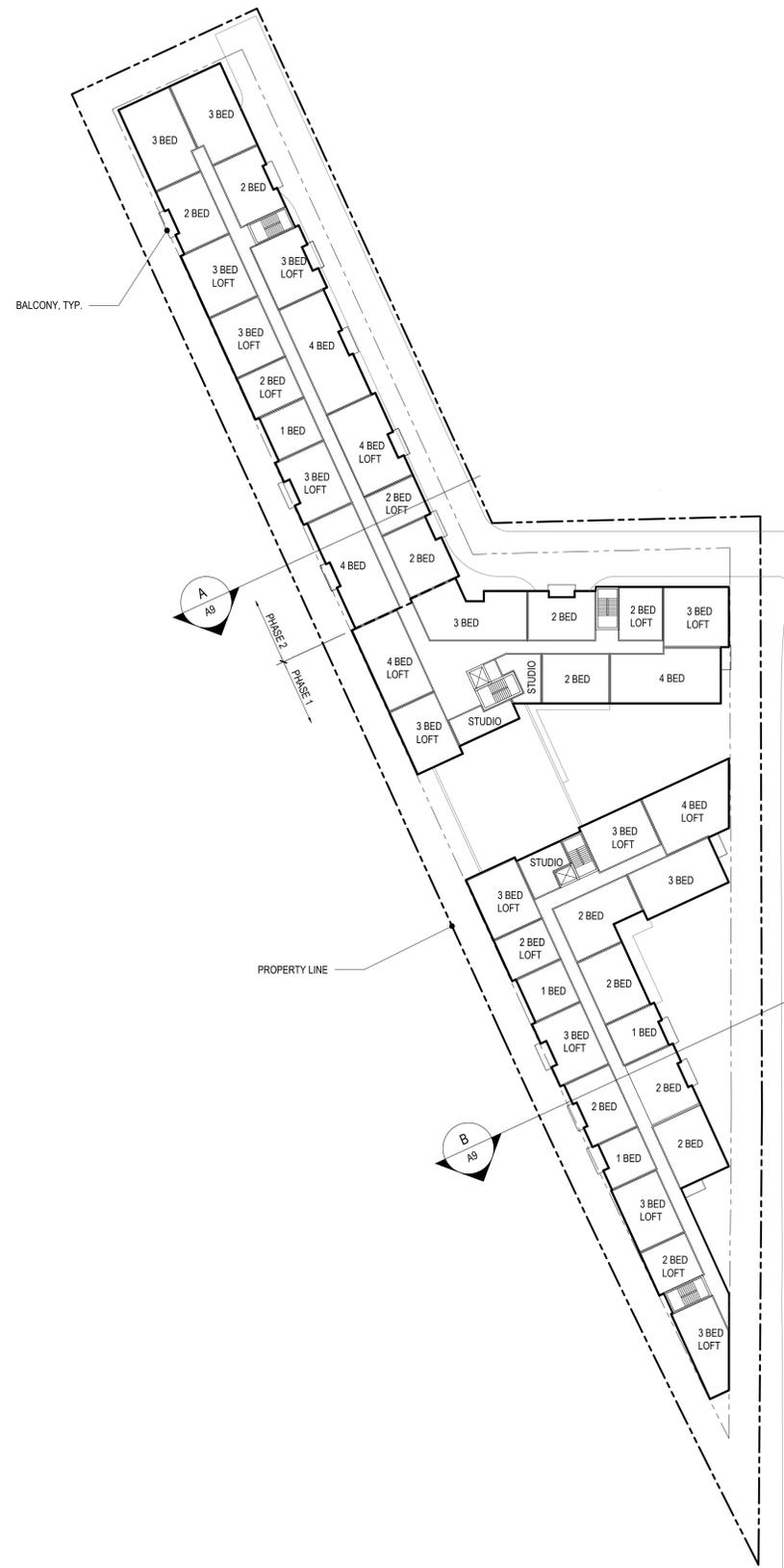
BRADLEY A. BUSER
DATE
LICENSE NUMBER - 52838

Date Issued:
NOVEMBER 8, 2015

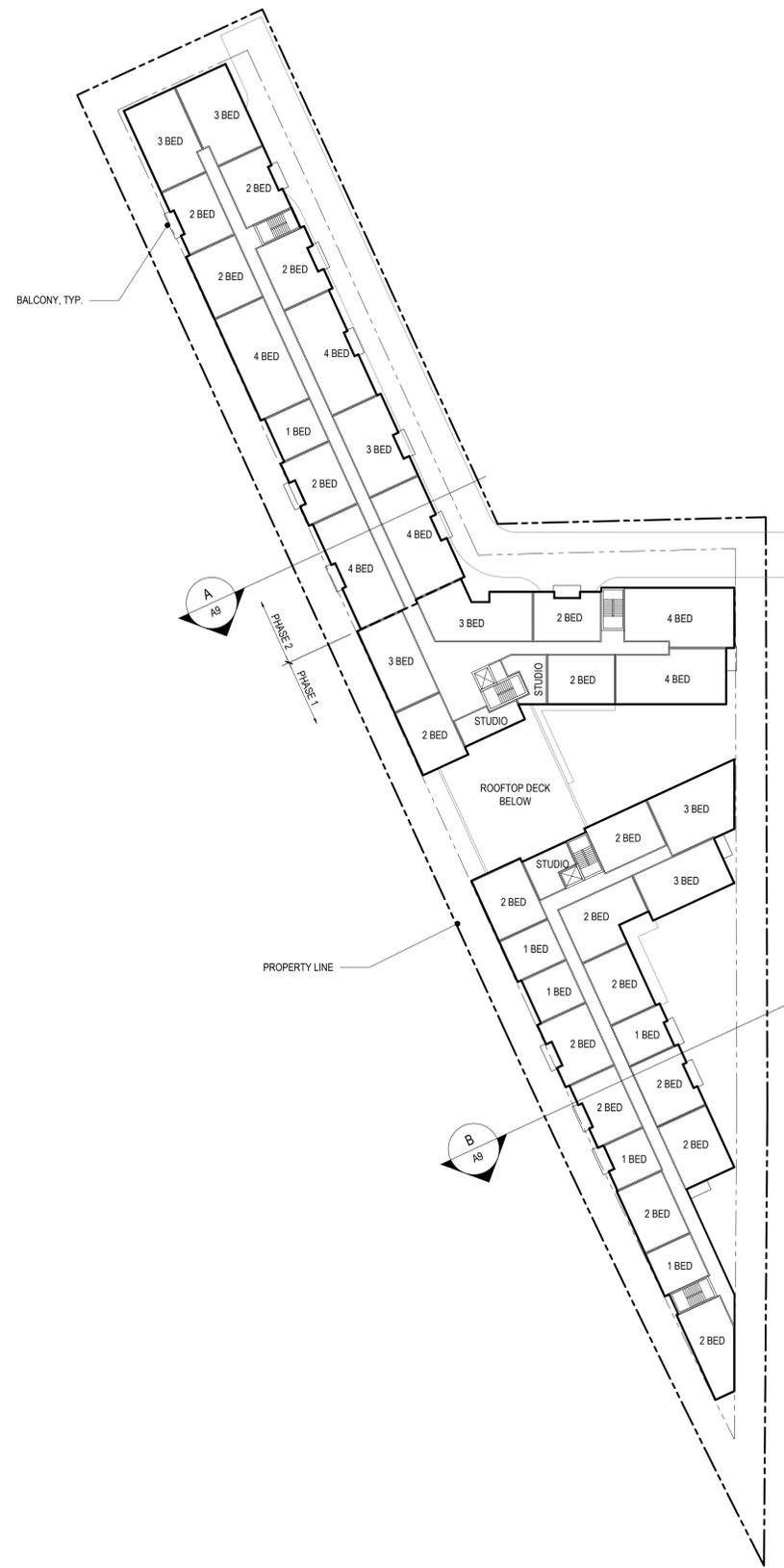
Revisions:

Job No. 02-2015

Sheet



 **FIFTH FLOOR PLAN**
1" = 40"



 **FOURTH FLOOR PLAN**
1" = 40"

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

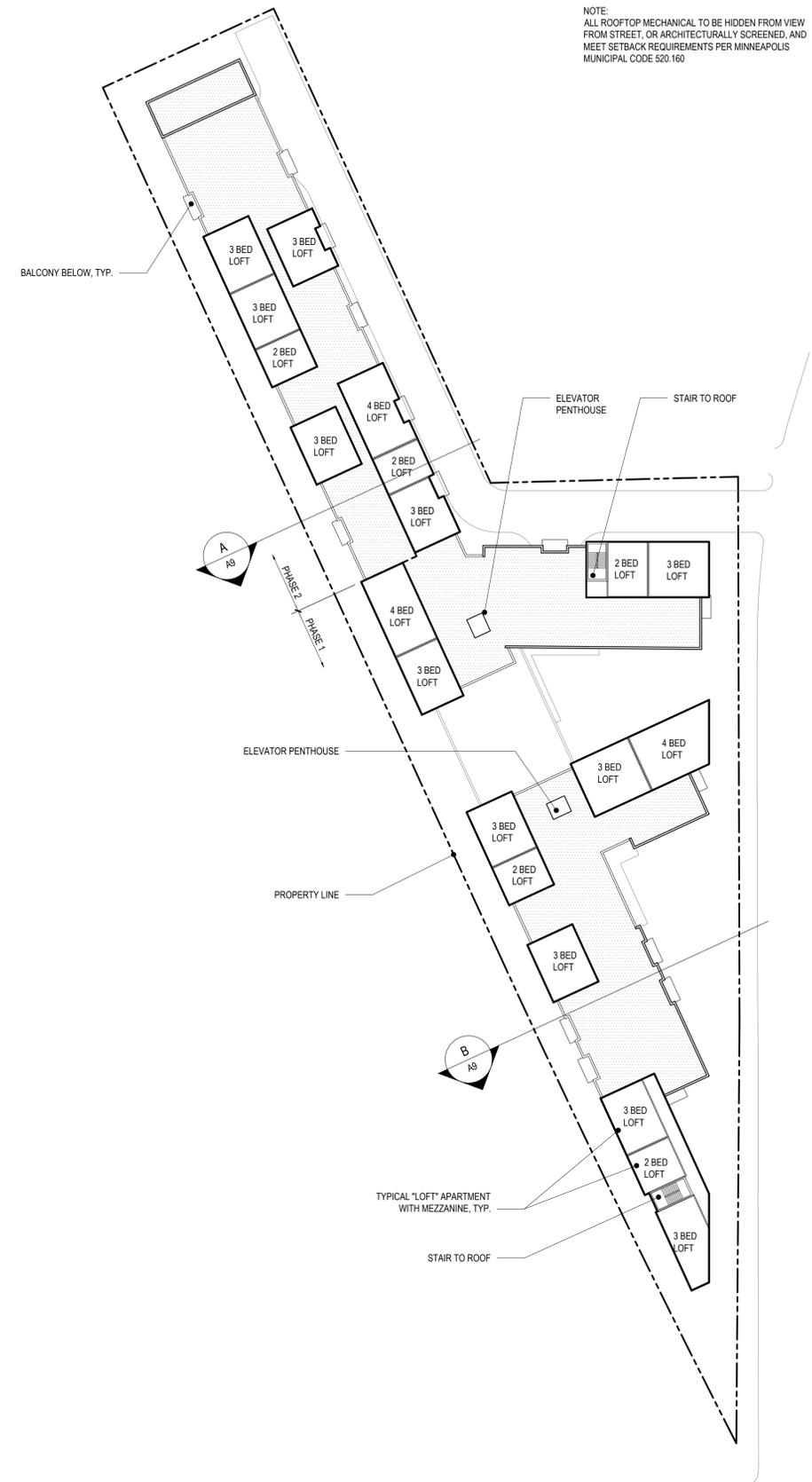
BRADLEY A. BUSER
DATE
LICENSE NUMBER - 52838

Date Issued:
NOVEMBER 8, 2015

Revisions:

Job No. 02-2015

Sheet



NOTE:
 ALL ROOFTOP MECHANICAL TO BE HIDDEN FROM VIEW FROM STREET, OR ARCHITECTURALLY SCREENED, AND MEET SETBACK REQUIREMENTS PER MINNEAPOLIS MUNICIPAL CODE 520.160

 **A**
A4 MEZZANINE AND ROOF PLAN
 1" = 40"

PRIME DESIGN
 MANHATTAN, KS 66502
 785.317.5725

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.

BRADLEY A. BUSER
 DATE
 LICENSE NUMBER - 52038

Date Issued:
 NOVEMBER 8, 2015

Revisions:
 .
 .
 .

117 27TH AVE. SE APARTMENTS
 MINNEAPOLIS, MINNESOTA 55414
 ELSEY PARTNERS

Job No. 02-2015

Sheet

A4



SOUTH ELEVATION - MATERIAL COVERAGE SUMMARY
 MATERIALS CALCULATED WITH EAST ELEVATION AS THE SOUTH ELEVATION ESSENTIALLY HAS NO PROFILE (I.E. "KNIFE" SHAPE)

C SOUTH ELEVATION
 A5 1/16" = 1'-0"



NORTHEAST ELEVATION MATERIAL COVERAGE SUMMARY
 MATERIAL 1 - BRICK = 15.3%
 MATERIAL 2 - STANDARD FIBER CEMENT = 9.1%
 MATERIAL 3 - PREMIUM FIBER CEMENT (+5/8" THICK) = 50.1%
 GLASS/WINDOWS = 25.4%

B NORTHEAST ELEVATION
 A5 1/16" = 1'-0"



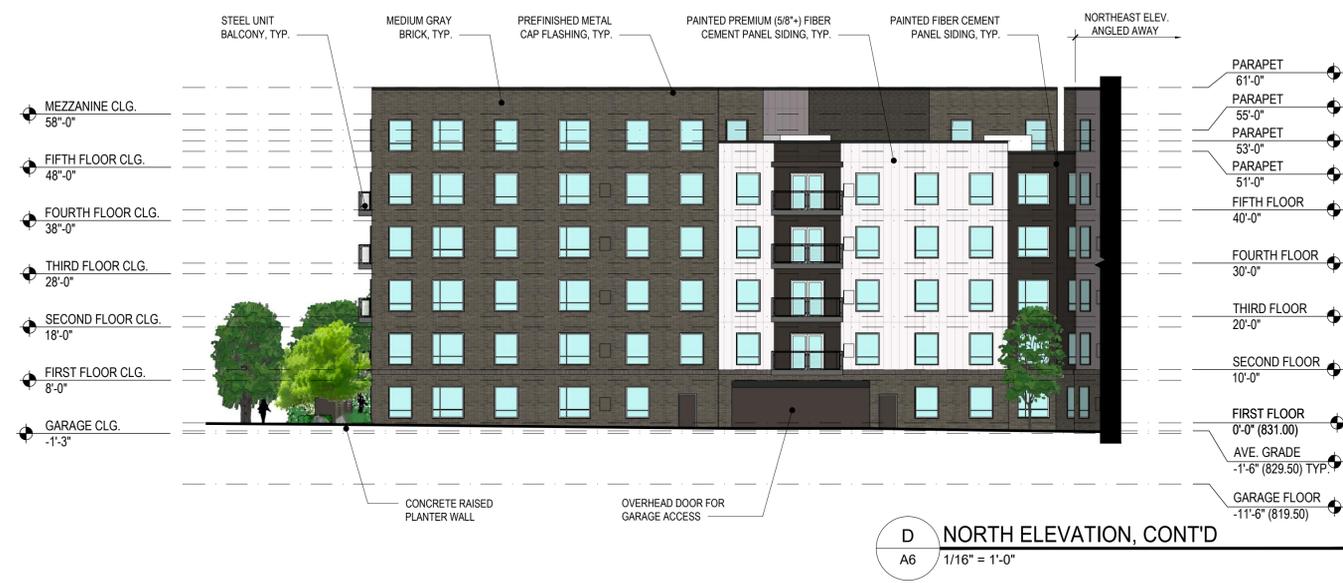
EAST ELEVATION - MATERIAL COVERAGE SUMMARY
 MATERIAL 1 - BRICK = 33.6%
 MATERIAL 2 - STANDARD FIBER CEMENT = 8.6%
 MATERIAL 3 - PREMIUM FIBER CEMENT (+5/8" THICK) = 19.6%
 GLASS/WINDOWS = 38.2%

A EAST ELEVATION
 A5 1/16" = 1'-0"

HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MISSOURI.
 BRUCE A. BLISS
 DATE: 11/3/15
 LICENSE NUMBER: 52038

Date Issued: **NOVEMBER 6, 2015**
 Revisions:

Job No: 02-2015
 Sheet



D NORTH ELEVATION, CONT'D
A6 1/16" = 1'-0"



C NORTH ELEVATION
A6 1/16" = 1'-0"

NORTH ELEVATION MATERIAL COVERAGE SUMMARY (ELEVATION C & D)
MATERIAL 1 - BRICK = 61.3%
MATERIAL 2 - PREMIUM FIBER CEMENT (+5/8\" THICK) = 23.2%
GLASS/WINDOWS - 15.5%



B WEST ELEVATION, CONT'D
A6 1/16" = 1'-0"



A WEST ELEVATION
A6 1/16" = 1'-0"

WEST ELEVATION MATERIAL COVERAGE SUMMARY (ELEV. A & B)
MATERIAL 1 - BRICK = 44.4%
MATERIAL 2 - STANDARD FIBER CEMENT = 5.9%
MATERIAL 3 - PREMIUM FIBER CEMENT (+5/8\" THICK) = 25.6%
GLASS/WINDOWS = 24.1%

HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MISSOURI.
[Signature]
DATE: 12/3/15
LICENSE NUMBER: 5208

Date Issued: NOVEMBER 6, 2015
Revisions:

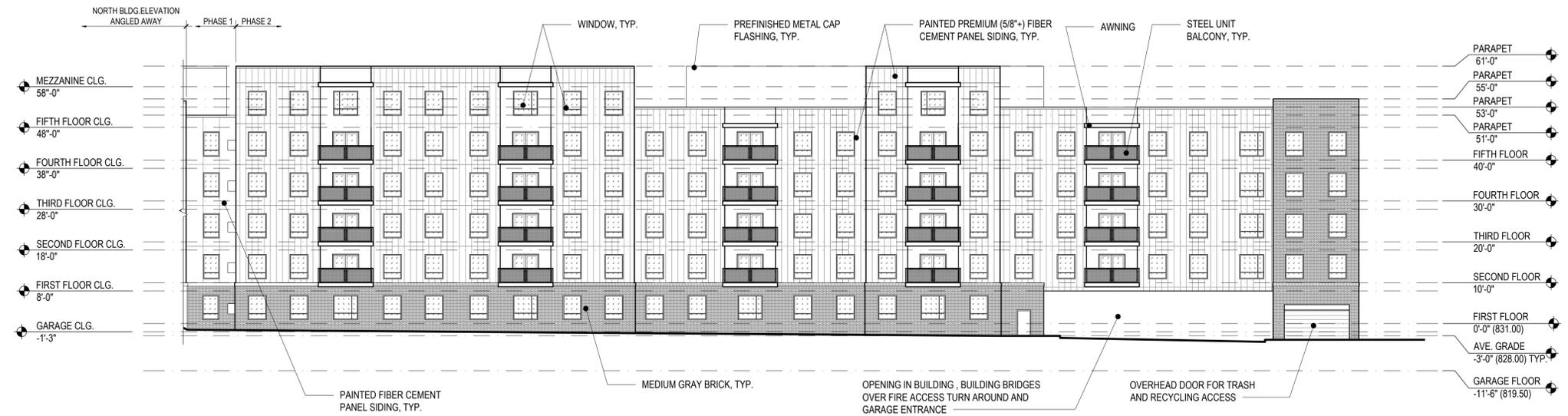
Job No: 02-2015

Sheet



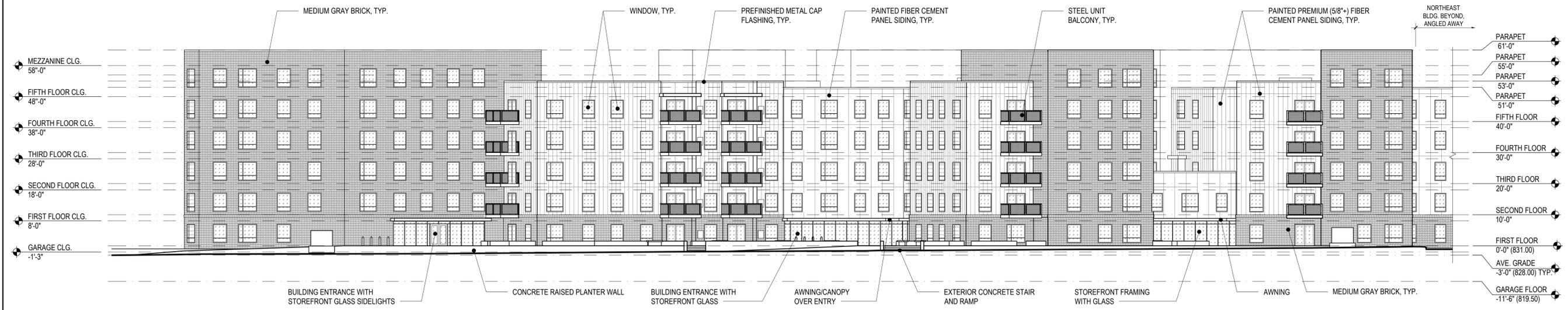
SOUTH ELEVATION - MATERIAL COVERAGE SUMMARY
 MATERIALS CALCULATED WITH EAST ELEVATION AS THE SOUTH ELEVATION ESSENTIALLY HAS NO PROFILE (I.E. "KNIFE" SHAPE)

C SOUTH ELEVATION
 A5 1/16" = 1'-0"



NORTHEAST ELEVATION MATERIAL COVERAGE SUMMARY
 MATERIAL 1 - BRICK = 15.3%
 MATERIAL 2 - STANDARD FIBER CEMENT = 9.1%
 MATERIAL 3 - PREMIUM FIBER CEMENT (+5/8" THICK) = 50.1%
 GLASS/WINDOWS = 25.4%

B NORTHEAST ELEVATION
 A5 1/16" = 1'-0"



EAST ELEVATION - MATERIAL COVERAGE SUMMARY
 MATERIAL 1 - BRICK = 33.6%
 MATERIAL 2 - STANDARD FIBER CEMENT = 8.6%
 MATERIAL 3 - PREMIUM FIBER CEMENT (+5/8" THICK) = 19.6%
 GLASS/WINDOWS = 38.2%

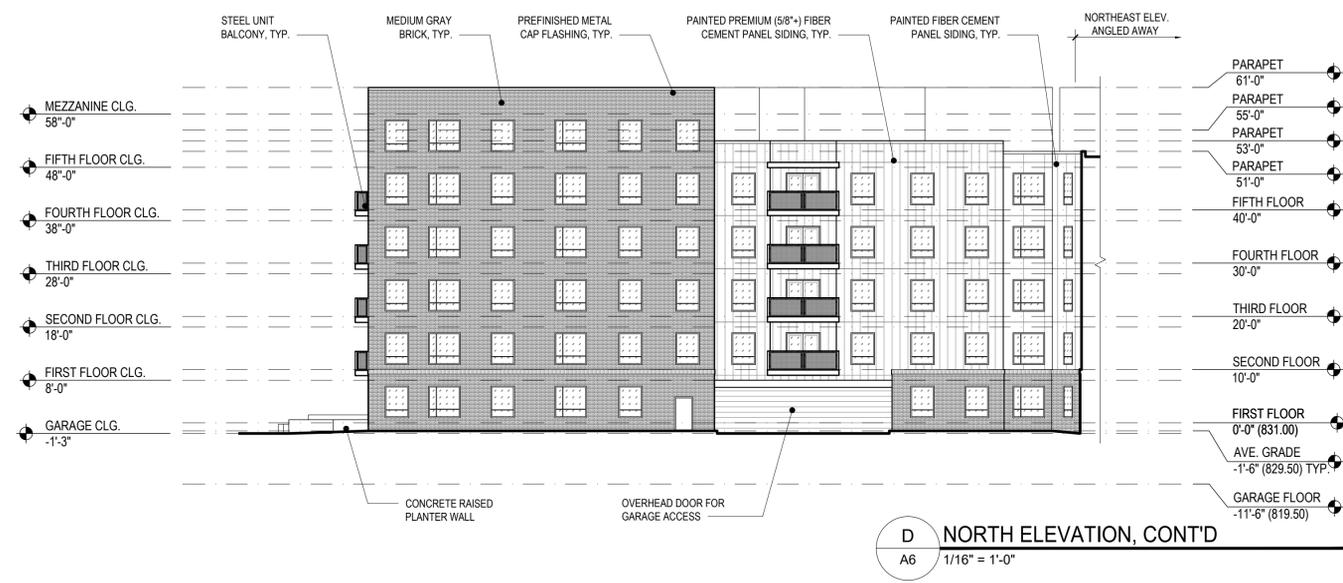
A EAST ELEVATION
 A5 1/16" = 1'-0"

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 DATE: 12/3/15
 LICENSE NUMBER: 52038

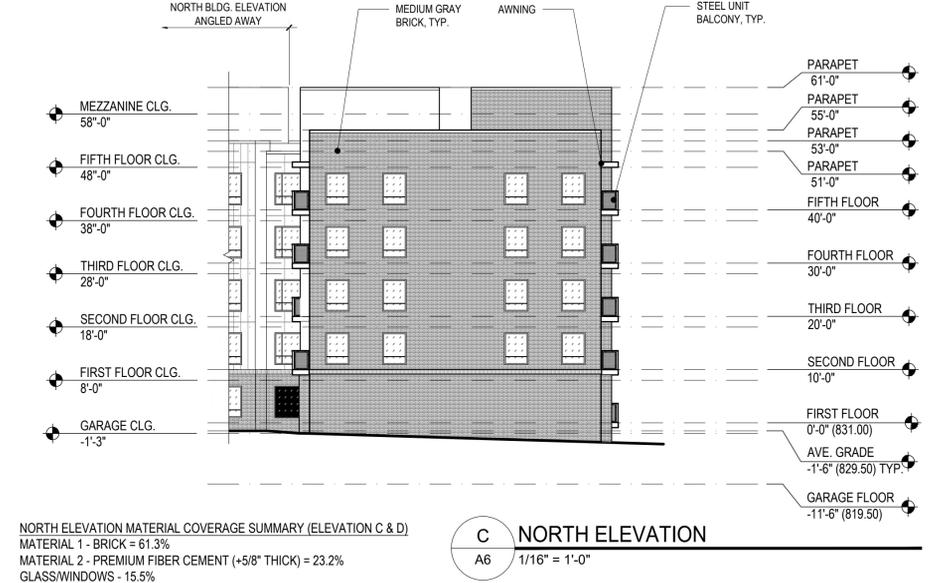
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Job No: 02-2015

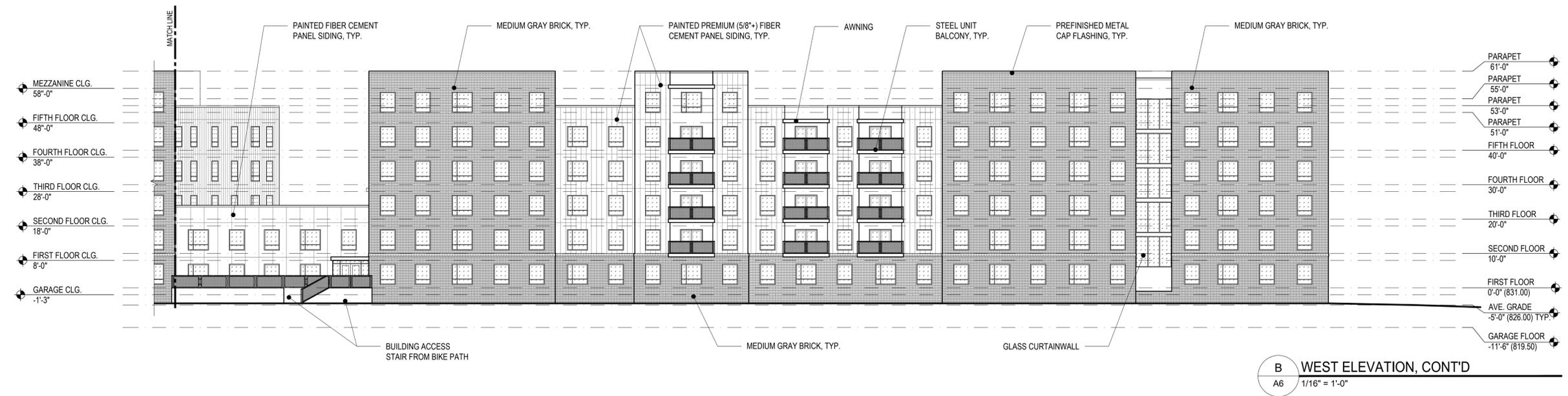
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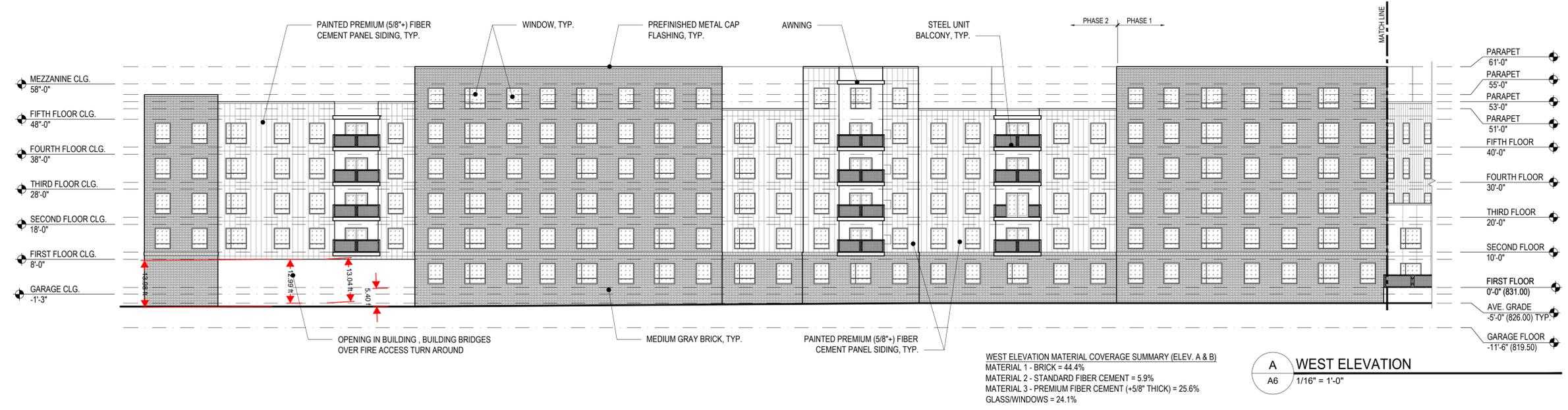
D NORTH ELEVATION, CONT'D
A6 1/16" = 1'-0"



C NORTH ELEVATION
A6 1/16" = 1'-0"



B WEST ELEVATION, CONT'D
A6 1/16" = 1'-0"

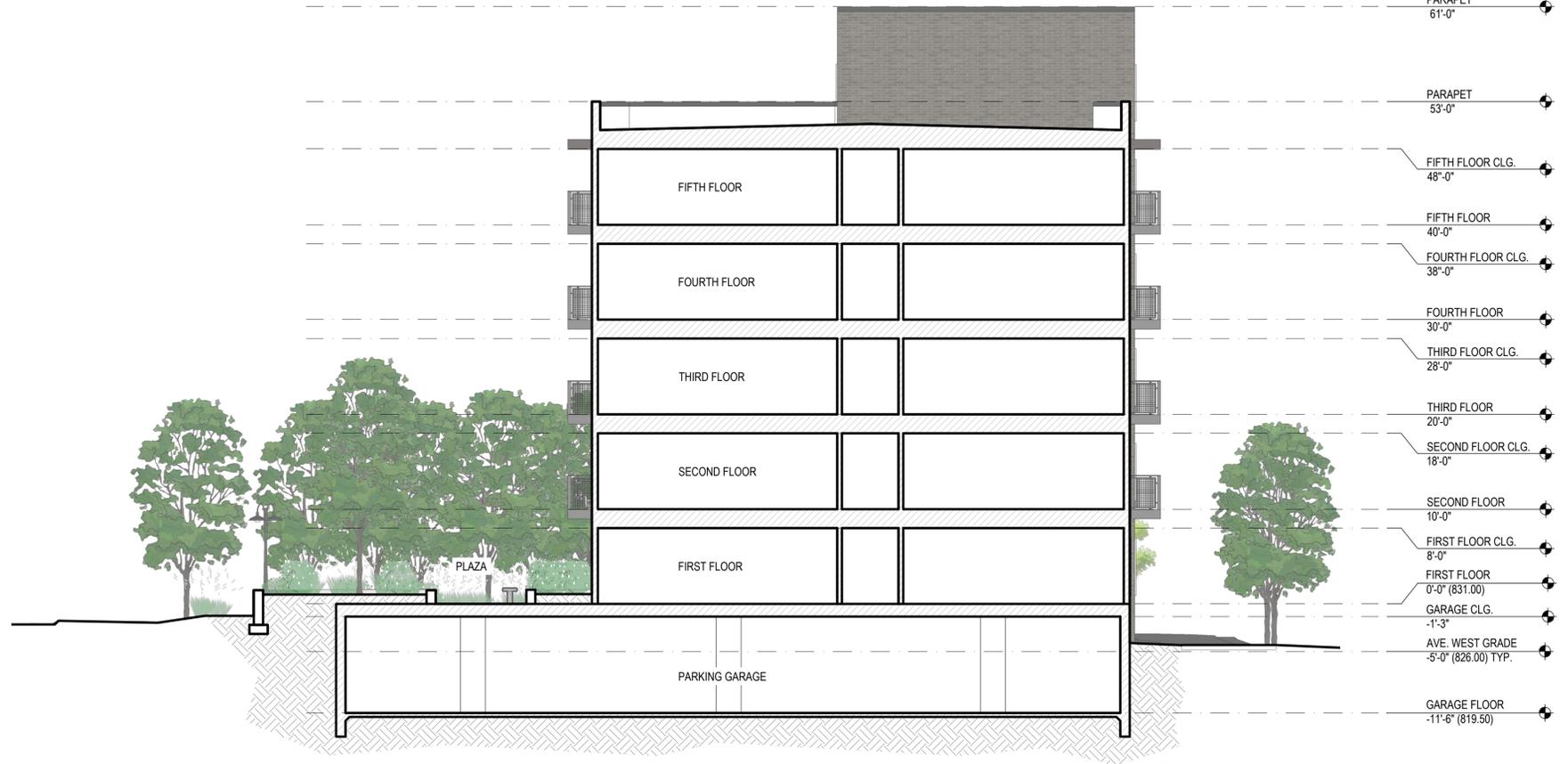


A WEST ELEVATION
A6 1/16" = 1'-0"

HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.
BRUCE A. BLISS
DATE 11/3/15
LICENSE NUMBER - 5208

Date Issued: NOVEMBER 6, 2015
Revisions:

Job No. 02-2015
Sheet



B SECTION THRU SOUTH BLDG.+PLAZA
 A9 1/8" = 1'-0"



A SECTION THRU NORTH BUILDING
 A9 1/8" = 1'-0"

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED ARCHITECT UNDER THE LAWS OF THE STATE OF MINNESOTA.
 [Signature]
 ARCHITECT & DESIGNER
 DATE 12/3/15
 LICENSE NUMBER - 52038

Date Issued:
 NOVEMBER 6, 2015
 Revisions:

Job No. 02-2015

Sheet

Shadow Study for 117 27th Ave SE | Minneapolis, MN

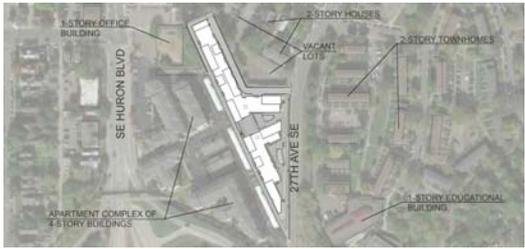
For General Land Use Application

Developer/Property Owner

Stacy Parkers, LLC
1522 College Ave, P-19
Minneapolis, MN 55452

Architect

Philo Design
1522 College Ave, P-19
Minneapolis, MN 55452



Context Map



SeptMar 20 | 1 hr after sunrise

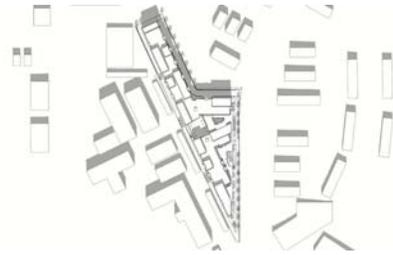
Shadow Study for 117 27th Ave SE | 217

Shadow Study for 117 27th Ave SE | 317



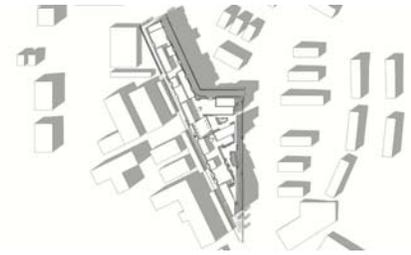
SeptMar 20 | 9:00 AM

Shadow Study for 117 27th Ave SE | 417



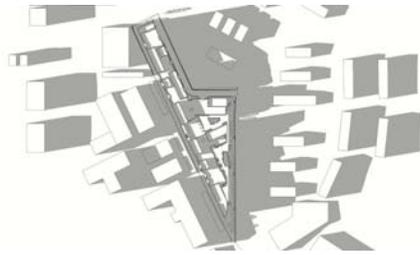
SeptMar 20 | Noon

Shadow Study for 117 27th Ave SE | 517



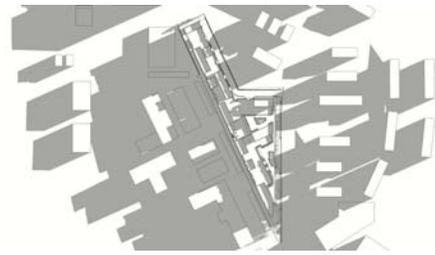
SeptMar 20 | 3:00 PM

Shadow Study for 117 27th Ave SE | 617



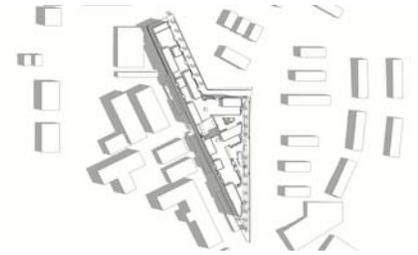
SeptMar 20 | 1 hr before sunset

Shadow Study for 117 27th Ave SE | 717



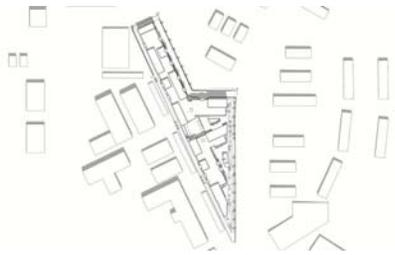
June 21 | 1 hr after sunrise

Shadow Study for 117 27th Ave SE | 817



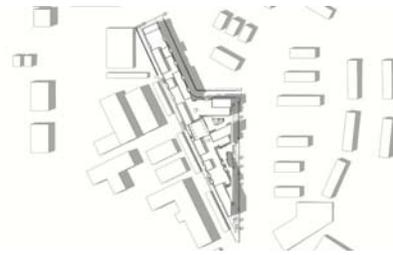
June 21 | 9:00 AM

Shadow Study for 117 27th Ave SE | 917



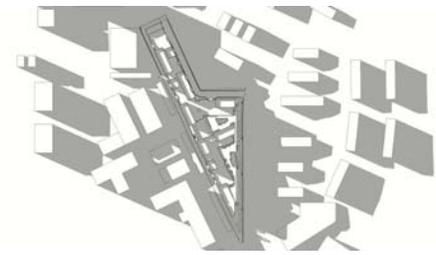
June 21 | Noon

Shadow Study for 117 27th Ave SE | 1017



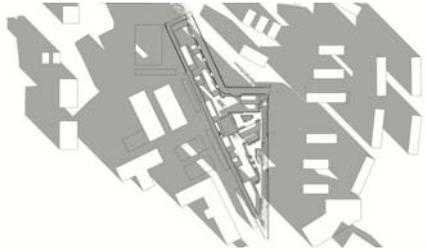
June 21 | 3:00 PM

Shadow Study for 117 27th Ave SE | 1117



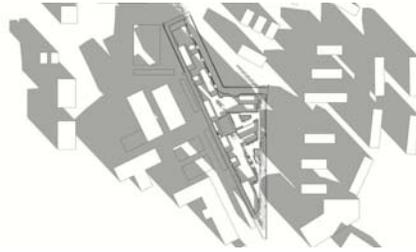
June 21 | 1 hr before sunset

Shadow Study for 117 27th Ave SE | 1217



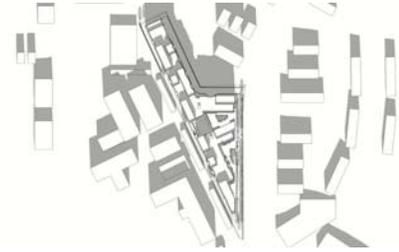
Dec 22 | 1 hr after sunrise

Shadow Study for 117 27th Ave SE | 1517



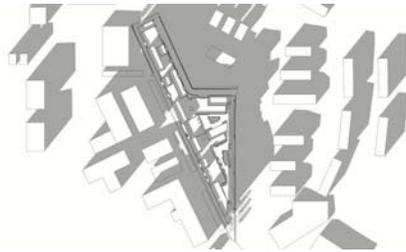
Dec 22 | 9:00 AM

Shadow Study for 117 27th Ave SE | 1517



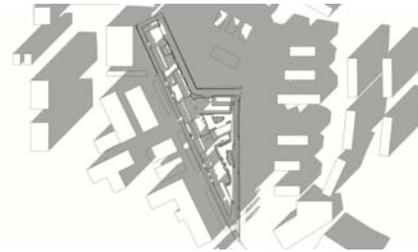
Dec 22 | Noon

Shadow Study for 117 27th Ave SE | 1517



Dec 22 | 3:00 PM

Shadow Study for 117 27th Ave SE | 1517



Dec 22 | 1 hr before sunset

Shadow Study for 117 27th Ave SE | 1517